



## CITY OF NEWPORT BEACH COMMUNITY DEVELOPMENT DEPARTMENT PLANNING DIVISION ACTION REPORT

TO: CITY COUNCIL, CITY MANAGER, AND PLANNING COMMISSION

FROM: Seimone Jurjis, Assistant City Manager/Community Development Director

SUBJECT: Report of actions taken by the Zoning Administrator, and/or Community Development Director for the week ending April 26, 2024.

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### ZONING ADMINISTRATOR PUBLIC HEARING ACTIONS APRIL 25, 2024

- Item 1: McBride Residential Condominiums Tentative Parcel Map and Coastal Development Permit (PA2024-0044)  
Site Address: 318 & 318 ½ Iris Avenue
- |   |                  |   |
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| Action: Approved by Resolution No. ZA2024-025 | Council District | 6 |
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- Item 2: Pirozzi Corona Del Mar (Foretti's) Limited Term Permit and Coastal Development Permit (PA2023-0231)  
Site Address: 2929 East Coast Highway
- |   |                  |   |
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| Action: Approved by Resolution No. ZA2024-026 | Council District | 6 |
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- Item 3: Beane Residence Coastal Development Permit (PA2023-0234)  
Site Address: 2632 Circle Drive
- |   |                  |   |
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| Action: Approved by Resolution No. ZA2024-027 | Council District | 2 |
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### COMMUNITY DEVELOPMENT DIRECTOR OR ZONING ADMINISTRATOR ACTIONS (Non-Hearing Items)

- Item 1: Starbucks Jamboree Road Staff Approval for Substantial Conformance (PA2024-0035)  
Site Address: 4501 Jamboree Road
- |                  |                  |   |
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| Action: Approved | Council District | 3 |
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**APPEAL PERIOD:** An appeal or call for review may be filed with the Director of Community Development or City Clerk, as applicable, within fourteen (14) days following the date the action or decision was rendered unless a different period is specified by the Municipal Code (e.g., Title 19 allows ten (10) day appeal period for tentative parcel and tract maps, lot line adjustments, or lot mergers). For additional information on filing an appeal, contact the Planning Division at 949 644-3200.

## **RESOLUTION NO. ZA2024-025**

### **A RESOLUTION OF THE ZONING ADMINISTRATOR OF THE CITY OF NEWPORT BEACH, APPROVING A TENTATIVE PARCEL MAP AND COASTAL DEVELOPMENT PERMIT FOR TWO-UNIT RESIDENTIAL CONDOMINIUM PURPOSES LOCATED AT 318 AND 318 ½ IRIS AVENUE (PA2024-0044)**

THE ZONING ADMINISTRATOR OF THE CITY OF NEWPORT BEACH HEREBY FINDS AS FOLLOWS:

#### **SECTION 1. STATEMENT OF FACTS.**

1. An application was filed by Matthew Watson (Applicant) on behalf of William McBride (Owner) with respect to property located at 318 and 318 ½ Iris Avenue and legally described as Lot 20 of Block 236 of the Corona Del Mar Tract (Property)
2. The Applicant requests a tentative parcel map and coastal development permit for a two-unit condominium. A duplex has been demolished, and a new duplex is currently under construction. The tentative parcel map will allow each unit to be sold individually. No waivers of Newport Beach Municipal Code (NBMC) Title 19 (Subdivisions) are proposed. A CDP is required because the property is located in the coastal zone (Project).
3. The Property is designated Two Unit Residential (RT) by the General Plan Land Use Plan and is located within the Two-Unit Residential (R-2) Zoning District.
4. The Property is located within the coastal zone. The Coastal Land Use Plan category is Two-Unit Residential (RT-D – 20.0-29.9 DU/AC) and it is located within the Two-Unit Residential (R-2) Coastal Zoning District.
5. A public hearing was held on April 25, 2024, online via Zoom. A notice of the time, place, and purpose of the hearing was given in accordance with the NBMC. Evidence, both written and oral, was presented to and considered by, the Zoning Administrator at this hearing.

#### **SECTION 2. CALIFORNIA ENVIRONMENTAL QUALITY ACT DETERMINATION.**

1. This Project is exempt from the California Environmental Quality Act (CEQA) pursuant to Section 15315 under Class 15 (Minor Land Divisions) of the CEQA Guidelines, California Code of Regulations, Title 14, Division 6, Chapter 3 because it has no potential to have a significant effect on the environment.
2. The Class 15 exemption allows the division of property in urbanized areas zoned for residential, commercial, or industrial use into four or fewer parcels when the division is in conformance with the General Plan and Zoning, no variances or exceptions are required, all services and access to the proposed parcels are available, the parcel was not involved in a division of a larger parcel within the previous two years, and the parcel does not have an average slope greater than 20%. The tentative parcel map is for a



two-unit condominium subdivision and will conform to all development standards. Therefore, the Project is eligible for the Class 15 exemption.

### SECTION 3. REQUIRED FINDINGS.

#### ***Tentative Parcel Map***

The Zoning Administrator determined in this case that the tentative parcel map is consistent with the legislative intent of the NBMC Title 19 (Subdivisions) and is approved based on the following findings per NBMC Section 19.12.070 (Required Findings for Action on Tentative Maps).

#### Finding:

- A. *That the proposed map and the design or improvements of the subdivision are consistent with the General Plan and any applicable specific plan, and with applicable provisions of the Subdivision Map Act and this Subdivision Code.*

#### Facts in Support of Finding:

1. The Tentative Parcel Map is for two-unit residential condominium purposes. The RT and R-2 designations apply to a range of two-family residential dwelling units such as duplexes and townhomes. The Property was previously developed as a duplex, which has been demolished. A new duplex is currently under construction. The proposed subdivision and improvements will continue to be consistent with the density of the RT and R-2 designations.
2. The Property is not located within a specific plan area.

#### Finding:

- B. *The site is physically suitable for the type and density of development.*

#### Facts in Support of Finding:

1. The Property is physically suitable for a duplex because it is regular in shape and size. An existing duplex has been demolished and a new duplex is currently under construction, which complies with all requirements of the NBMC Title 20 (Planning and Zoning) and Title 21 (Local Coastal Program Implementation Plan).
2. The Property is accessible from Iris Ave and the alley in the rear. Vehicular access is taken from the alley.
3. The Property is adequately served by all existing utilities to accommodate the two new dwelling units being constructed. The proposed subdivision does not alter the anticipated utility demand generated from the site.

Finding:

- C. *That the design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage nor substantially and avoidably injure fish or wildlife or their habitat. However, notwithstanding the foregoing, the decision-making body may nevertheless approve such a subdivision if an environmental impact report was prepared for the project and a finding was made pursuant to Section 21081 of the California Environmental Quality Act that specific economic, social, or other considerations make infeasible the mitigation measures or project alternatives identified in the environmental impact report.*

Facts in Support of Finding:

1. The Property is located within an existing residential neighborhood that does not contain any sensitive vegetation or habit on-site.
2. This Project is exempt from the California Environmental Quality Act (CEQA) pursuant to Section 15315 under Class 15 (Minor Land Divisions) of the CEQA Guidelines. Section 2 of this Resolution is hereby incorporated by reference.

Finding:

- D. *That the design of the subdivision or the type of improvements is not likely to cause serious public health problems.*

Facts in Support of Finding:

1. The Project has been conditioned to require public improvements including the reconstruction of sidewalks, curbs, and gutters along the Iris Avenue frontage and any damaged concrete panels along the alley frontage, as needed. New turf or drought tolerant landscaping along in the Iris Avenue public right-of-way shall be installed and the existing street tree on the Iris Avenue frontage shall be protected in place. The utilities will be undergrounded, consistent with Title 19 (Subdivisions) of the Newport Beach Municipal Code (NBMC).
2. The tentative parcel map is for two-unit condominium purposes. All improvements associated with the project will comply with all Building, Public Works, and Fire Codes, which are in place to prevent serious public health problems. Public improvements will be required of the developer per NBMC Section 19.28.010 (General Improvement Requirements) and Section 66411 (Local agencies to regulate and control design of subdivisions) of the Subdivision Map Act. All ordinances of the City and all Conditions of Approval will be complied with.

Finding:

- E. *That the design of the subdivision or the type of improvements will not conflict with easements, acquired by the public at large, for access through or use of property within*

*the proposed subdivision. In this connection, the decision-making body may approve a map if it finds that alternate easements, for access or use, will be provided and that these easements will be substantially equivalent to ones previously acquired by the public. This finding shall apply only to easements of record or to easements established by the judgment of a court of competent jurisdiction and no authority is hereby granted to the City Council to determine that the public at large has acquired easements for access through or use of property within a subdivision.*

Fact in Support of Finding:

1. The Public Works Department has reviewed the proposed tentative parcel map and determined that the design of the development will not conflict with easements acquired by the public at large, for access through, or use of the Property within the proposed development because no public easements are located on the Property.

Finding:

- F. That, subject to the detailed provisions of Section 66474.4 of the Subdivision Map Act, if the land is subject to a contract entered into pursuant to the California Land Conservation Act of 1965 (Williamson Act), the resulting parcels following a subdivision of the land would not be too small to sustain their agricultural use or the subdivision will result in residential development incidental to the commercial agricultural use of the land.*

Facts in Support of Finding:

1. The Property is not subject to the Williamson Act because the subject property is not designated as an agricultural preserve and is less than 100 acres in area.
2. The Property was previously developed for residential use and is located in a Zoning District that permits residential uses.

Finding:

- G. That, in the case of a "land project" as defined in Section 11000.5 of the California Business and Professions Code: (1) there is an adopted specific plan for the area to be included within the land project, and (2) the decision-making body finds that the proposed land project is consistent with the specific plan for the area.*

Facts in Support of Finding:

1. California Business and Professions Code Section 11000.5 has been repealed by the Legislature. However, this project site is not considered a "land project" as previously defined in Section 11000.5 of the California Business and Professions Code because the project site does not contain 50 or more parcels of land.
2. The Project is not located within a specific plan area.

Finding:

- H. That, solar access, and passive heating and cooling design requirements have been satisfied in accordance with Sections 66473.1 and 66475.3 of the Subdivision Map Act.*

Fact in Support of Finding:

1. The tentative parcel map and any future improvements are subject to Title 24 of the California Building Code, which requires new construction to meet minimum heating and cooling efficiency standards depending on location and climate. The City's Building Division enforces Title 24 compliance through the plan check and inspection process.

Finding:

- I. That the subdivision is consistent with Section 66412.3 of the Subdivision Map Act and Section 65584 of the California Government Code regarding the City's share of the regional housing need and that it balances the housing needs of the region against the public service needs of the City's residents and available fiscal and environmental resources.*

Fact in Support of Finding:

1. The proposed two-unit condominiums will replace an existing duplex, which is consistent with the R-2 Zoning District and does not result in a net decrease in residential density. Therefore, the tentative parcel map for condominium purposes will not negatively affect the City in meeting its regional housing need.

Finding:

- J. That the discharge of waste from the proposed subdivision into the existing sewer system will not result in a violation of existing requirements prescribed by the Regional Water Quality Control Board.*

Fact in Support of Finding:

1. The Project wastewater is designed to discharge into the existing sewer system and complies with the Regional Water Quality Control Board (RWQCB) requirements.

Finding:

- K. For subdivisions lying partly or wholly within the Coastal Zone, the subdivision conforms with the certified Local Coastal Program and, where applicable, with public access and recreation policies of Chapter 3 of the Coastal Act.*

Fact in Support of Finding:

1. The Property is located within the coastal zone and a CDP is therefore requested in conjunction with the proposed tentative parcel map. The Project complies with the

certified Local Coastal Program (LCP) and public access and recreation policies of Chapter Three of the Coastal Act. The Facts in Support of Findings L and M for the Coastal Development Permit (below) are hereby incorporated by reference.

### ***Coastal Development Permit***

In accordance with Section 21.52.015(F) (Coastal Development Permits - Findings, and Decision) of the Newport Beach Municipal Code, the following findings, and facts in support of such findings are set forth:

#### Finding:

*L. Conforms to all applicable sections of the certified Local Coastal Program.*

#### Facts in Support of Finding:

1. The Project is for a tentative parcel map for two-unit residential condominium purposes. A duplex has been demolished and a new duplex is under construction. The duplex conforms to all applicable development standards, including floor area limit, setbacks, height, and off-street parking. The proposed subdivision and improvements are consistent with the density of the R-2 Coastal Zoning District.
2. The Property is not located adjacent to a coastal view road, or public viewpoint as identified in the Coastal Land Use Plan (CLUP). The Project does not have the potential to degrade the visual quality of the Coastal Zone or to result insignificant adverse impacts to public view. The Property is located approximately 1,600 feet from the Begonia Park which is identified by the CLUP as a public viewpoint; however, due to the distance to the park and intervening structures, the Project does not negatively impact the public views of the coast from the park.
3. The Property is located in an area known for the potential for seismic activity. All projects are required to comply with the California Building Code and Building Division standards and policies, which includes all required mitigation for seismic hazards.
4. The Project is located approximately 1,600 feet from Corona del Mar State Beach and is not near any natural landforms or environmentally sensitive areas.

#### Finding:

*M. Conforms to the public access and public recreation policies of Chapter 3 of the Coastal Act if the project is located between the nearest public road and the sea or shoreline of any body of water located within the coastal zone.*

#### Facts in Support of Finding:

1. The Property is not located between the nearest public road and the sea or shoreline. Implementation Plan Section 21.30A.040 (Determination of Public Access/Recreation

Impacts) requires that the provision of public access bear a reasonable relationship between the requirement and the project's impact and be proportional to the impact. In this case, the Project is a tentative parcel map for two-unit condominium purposes within the R-2 Coastal Zoning District. The Project does not involve a change in land use, density, or intensity that will result in increased demand for public access and recreation opportunities. Furthermore, the Project is designed and sited (appropriate height, setbacks, etc.) so as not to block or impede existing public access opportunities.


2. The Property is approximately 1,600 feet from the Corona del Mar State Beach where lateral coastal access is provided along Ocean Boulevard. Additionally, vertical access to the beach is provided nearby on the street ends of Goldenrod and Heliotrope Avenues, per the Coastal Land Use Plan (CLUP). Approval of the parcel map will not affect public recreation, access, or views.

#### SECTION 4. DECISION.

#### **NOW, THEREFORE, BE IT RESOLVED:**

1. The Zoning Administrator of the City of Newport Beach hereby finds this project is categorically exempt from the California Environmental Quality Act pursuant to Sections 15315 under Class 15 (Minor Land Divisions) of the CEQA Guidelines, California Code of Regulations, Title 14, Division 6, Chapter 3 because it has no potential to have a significant effect on the environment.
2. The Zoning Administrator of the City of Newport Beach hereby approves the Tentative Parcel Map and Coastal Development Permit filed as PA2024-0044, subject to the conditions set forth in Exhibit A, which is attached hereto and incorporated by reference.
3. This action shall become final and effective 14 days following the date this Resolution was adopted unless within such time an appeal or call for review is filed with the Director of Community Development in accordance with the provisions of NBMC Title 19 (Subdivisions) and Title 21 (Local Coastal Program Implementation Plan).

**PASSED, APPROVED, AND ADOPTED THIS 25<sup>TH</sup> DAY OF APRIL 2024.**



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Benjamin M. Zueba, AICP, Zoning Administrator

**EXHIBIT “A”****CONDITIONS OF APPROVAL****Planning Division**

1. The Project is subject to all applicable City ordinances, policies, and standards unless specifically waived or modified by the Conditions of Approval.
2. After the recordation of the Parcel Map and prior to building permit final inspection, the Applicant shall apply for a building permit for a description change of the subject project development from “duplex” to “condominium.” The development will not be condominiums until this description change permit is final.
3. The Applicant shall comply with all federal, state, and local laws. A material violation of any of those laws in connection with the use may be cause for the revocation of this tentative parcel map.
4. This approval shall expire and become void unless exercised within 24 months from the actual date of review authority approval, except where an extension of time is approved in compliance with the provisions of Title 19 (Subdivisions) and Title 21 of the Newport Beach Municipal Code.
5. Prior to the approval of the final map, a “Purpose Statement” shall be added to the map to clarify that the map is for two residential condominium units and how common areas will be owned.
6. To the fullest extent permitted by law, the applicant shall indemnify, defend and hold harmless the City, its City Council, its boards and commissions, officials, officers, employees, and agents from and against any claims, demands, obligations, damages, actions, causes of action, suits, losses, judgments, fines, penalties, liabilities, costs, and expenses (including without limitation, attorney’s fees, disbursements, and court costs) of every kind and nature whatsoever which may arise from or in any manner relate (directly or indirectly) to City’s approval of **McBride Residence Condominium Parcel Map, but not limited to, Tentative Parcel Map and Coastal Development Permit (PA2024-0044)**. This indemnification shall include, but not be limited to, damages awarded against the City, if any, costs of suit, attorney’s fees, and other expenses incurred in connection with such claim, action, causes of action, suit, or proceeding whether incurred by the applicant, City, and/or the parties initiating or bringing such proceeding. The applicant shall indemnify the City for all of the City’s costs, attorney’s fees, and damages that which City incurs in enforcing the indemnification provisions set forth in this condition. The applicant shall pay to the City upon demand any amount owed to the City pursuant to the indemnification requirements prescribed in this condition.

**Public Works Department**

7. Prior to final building permit inspection, a Parcel Map shall be recorded. The Map shall be prepared on the California coordinate system (NAD83). Prior to recordation of the Map, the surveyor/engineer preparing the Map shall submit to the County Surveyor and the City of Newport Beach a digital-graphic file of said map in a manner described in Section 7-9-330 and 7-9-337 of the Orange County Subdivision Code and Orange County Subdivision Manual, Subarticle 18. The Map to be submitted to the City of Newport Beach shall comply with the City's CADD Standards. Scanned images will not be accepted.
8. Prior to recordation of the Parcel Map, the surveyor/engineer preparing the map shall tie the boundary of the map into the Horizontal Control System established by the County Surveyor in a manner described in Section 7-9-330 and 7-9-337 of the Orange County Subdivision Code and Orange County Subdivision Manual, Subarticle 18. Monuments (one inch iron pipe with tag) shall be set on each lot corner unless otherwise approved by the Subdivision Engineer. Monuments shall be protected in place if installed prior to completion of the construction project.
9. All improvements shall be constructed as required by Ordinance and the Public Works Department.
10. An encroachment permit shall be required for all work activities within the public right-of-way.
11. Existing broken and/or otherwise damaged concrete curb, gutter and sidewalk panels along the Iris Avenue frontage and any damaged concrete panels along the alley frontage shall be reconstructed.
12. All existing overhead utilities shall be undergrounded.
13. Install turf or drought tolerant landscaping of the type approved the City throughout the Iris Avenue parkway fronting the Property.
14. Existing street tree along Iris Avenue shall be protected-in-place.
15. Existing non-standard drain within parkway area along the Iris Avenue frontage shall be removed and replaced with a private drain through curb per City Standard.
16. Each unit shall be served by its individual water service/meter and sewer lateral/cleanout.
17. In case of damage done to public improvements surrounding the Property by the private construction, additional reconstruction within the public right-of-way could be required at the discretion of the Public Works Inspector.



**Fire Department**

18. Prior to the recordation of the Parcel Map, a NFPA 13D fire sprinkler system shall be provided for each unit.
19. Independent fire sprinkler systems shall be installed for each unit and separately connect to the water meter that serves each unit.

**Building Division**

20. Prior to the recordation of the Parcel Map, separate utility services are required for each unit.

## **RESOLUTION NO. ZA2024-026**

### **A RESOLUTION OF THE ZONING ADMINISTRATOR OF THE CITY OF NEWPORT BEACH, CALIFORNIA, APPROVING A LIMITED TERM PERMIT AND COASTAL DEVELOPMENT PERMIT TO ALLOW A TEMPORARY EXPANSION OF THE OUTDOOR DINING AREA LOCATED AT 2929 COAST HIGHWAY EAST (PA2023-0231)**

THE ZONING ADMINISTRATOR OF THE CITY OF NEWPORT BEACH HEREBY FINDS AS FOLLOWS:

#### **SECTION 1. STATEMENT OF FACTS.**

1. An application was filed by Edmond Bourke, representing Pirozzi Corona del Mar (Foretti's), with respect to property located at 2929 East Coast Highway, and legally described as Parcel 1 of Resubdivision 179 being Lots 3, 4, and 5 and a portion of Lot 2, of Block G, Tract 323, M.M. 14-40, 41 in the City of Newport Beach, Orange County, California ("Property").
2. The Applicant requests approval of a limited term and a coastal development permit to allow a 375 square foot maximum expanded dining area for up to a one-year term (January 1, 2024 through December 31, 2024) ("Project").
3. The subject property is designated CC (Corridor Commercial) by the General Plan Land Use Element and is located within the CC (Commercial Corridor) Zoning District.
4. The subject property is located within the coastal zone. The Coastal Land Use Plan category is CC-B (Corridor Commercial – 0.0 - 0.75 FAR) and it is located within the CC (Commercial Corridor) Coastal Zoning district.
5. Pursuant to Assembly Bill No. 1217, a local jurisdiction that has not adopted an ordinance that provides relief from parking restrictions for expanded outdoor dining areas is authorized to reduce the number of required parking spaces for existing uses by the number of spaces that the local jurisdiction determines are needed to accommodate an expanded outdoor dining area.
6. A public hearing was held on April 25, 2024, online via Zoom. A notice of the time, place, and purpose of the hearing was given by the Newport Beach Municipal Code (NBMC). Evidence, both written and oral, was presented to and considered by the Zoning Administrator at this hearing.

#### **SECTION 2. CALIFORNIA ENVIRONMENTAL QUALITY ACT DETERMINATION.**

1. This Project is categorically exempt pursuant to Title 14 of the California Code of Regulations pursuant to Section 15301, Division 6, Chapter 3, Guidelines for Implementation of the California Environmental Quality Act (CEQA) under Class 1 (Existing Facilities) and Section 15303 under Class 3 (New Construction or Conversion of

Small Structures) because it has no potential to have a significant effect on the environment.

2. The Class 1 exemption includes the operation, repair, maintenance, permitting, leasing, licensing, or minor alteration of existing public or private structures, facilities, mechanical equipment, or topographical features, involving negligible or no expansion of an existing or former use. The Class 3 exemption includes a store, motel, office, restaurant, or similar structure not involving the use of significant amounts of hazardous substances, not exceeding 2,500 square feet in floor area or 10,000 square feet in floor area in urbanized areas zoned for such use. The project authorizes a temporary 375-square-foot outdoor dining area within the parking lot of an existing restaurant and qualifies under the parameters of the Class 1 and Class 3 exemptions.
3. The exceptions to the categorical exemptions under Section 15300.2 are not applicable. The project location does not impact an environmental resource of hazardous or critical concern, does not result in cumulative impacts, does not have a significant effect on the environment due to unusual circumstances, does not damage scenic resources within a state scenic highway, is not a hazardous waste site, and is not identified as a historical resource.

### SECTION 3. REQUIRED FINDINGS.

#### ***Limited Term Permit***

In accordance with Section 20.52.040(G) (Limited Term Permits – Finding and Decision) of the Newport Beach Municipal Code (“NBMC”), the findings and facts in support of such findings are set forth as follows:

#### Finding:

- A. *The operation of the limited duration use at the location proposed and within the time period specified would not be detrimental to the harmonious and orderly growth of the City, nor endanger, jeopardize, or otherwise constitute a hazard to the public convenience, health, interest, safety, or general welfare of persons residing or working in the neighborhood of the requested limited duration use.*

#### Facts in Support of Finding:

1. The Limited Term Permit will allow a temporary outdoor dining area within the parking lot of Foretti’s restaurant. The temporary outdoor dining area would serve as additional outdoor seating for guests and is fully located on private property.
2. A dining area barrier is installed within the existing parking lot. The barrier delineates the area dedicated for outdoor dining use and alcohol service from the parking lot and drive aisle. There are no existing City utilities within the expanded dining area.
3. Condition of approval no. 3 limits the operation of the expanded dining area for up to a one (1)-year term, terminating on December 31, 2024.

4. Outdoor dining areas are common in the Corona del Mar commercial corridor, have been used at the subject property during similar hours in the past and the use has not proven detrimental. Condition of approval no. 6 limits the hours of operation for the restaurant and patio from 12:00 p.m. to 9:00 p.m., daily.
5. The expanded dining area has not posed a hazard to the general welfare of persons residing in the area since it was placed during the COVID-19 pandemic in 2020 through an Emergency Temporary Use Permit.
6. The proposed operation is conditioned to be accessible to all persons, including those with disabilities, in accordance with the Americans with Disabilities Act (ADA), as required by Condition No. 26.
7. The permitted use shall adhere to applicable State of California and Orange County Health Care Agency guidelines for the safe operation of the use. It is the responsibility of the permittee to implement and follow industry-specific guidance of the State of California and the Orange County Health Care Agency guidelines.
8. The permitted use must be operated in compliance with applicable State Department of Alcoholic Beverage Control (ABC) requirements.
9. The patio was originally authorized on June 12, 2020 by the Community Development Director through Emergency Temporary Use Permit No. UP2020-018 and Emergency Coastal Development Permit No. CD2020-034 (PA2020-089). The patio was later incorporated into Emergency Temporary Use Permit No. UP2020-111 and Coastal Development Permit no. CD2020-107 (PA2020-201), which authorized multiple patios for the entire multi-tenant, commercial, development which the restaurant is located within.
10. The restaurant operates with Use Minor Permit No. 2014-001. Condition of approval no. 1 requires all conditions of approval from Minor Use Permit No. 2014-001 be adhered to for this temporary patio, unless otherwise modified by the conditions of approval contained herein. No live entertainment, exterior amplified music, sound systems, televisions, paging systems, etc. shall be permitted within the temporary patio.

Finding:

- B. The subject lot is adequate in size and shape to accommodate the limited duration use without material detriment to the use and enjoyment of other properties located adjacent to and in the vicinity of the lot;*

Facts in Support of Finding:

1. The subject property is approximately 0.54 acres in size and gently slopes toward East Coast Highway. The existing food service use with outdoor patio dining has operated at this location since its original approval in 2014.

2. The subject property is bounded by East Coast Highway, Iris Avenue, an adjacent retail property and the Port Theater (a landmark theater), and residential properties to the rear. The subject property is within the commercial corridor of Corona del Mar where food service uses with outdoor dining are common. The expanded outdoor dining use, which is accessory to an existing restaurant, is not anticipated to impede the use or enjoyment of the properties within the area and will instead add to the ambiance and quaint character of Corona del Mar.
3. The expanded dining area will occupy a three (3) standard parking spaces. One (1) accessible parking space shall be maintained clear and accessible for use at all times. No traffic or site circulation issues were experienced during the previous operation of the expanded outdoor dining area and therefore, are not anticipated.
4. Given the expanded patio is located entirely on private property, impacts to pedestrian circulation are not anticipated.

Finding:

- C. *The subject lot is adequately served by streets or highways having sufficient width and improvements to accommodate the kind and quantity of traffic that the limited duration use would or could reasonably be expected to generate.*

Facts in Support of Finding:

1. The subject property is irregularly shaped and developed with a multi-tenant commercial center. Primary vehicular access is taken from East Coast Highway with additional vehicular access provided via rear alley and through an additional driveway on Iris Avenue. The patio will not block vehicular access to the site.
2. Peak hours for the restaurant are anticipated to be in the evening, when other retail and office uses on the property are typically closed. No traffic issues are anticipated with the continued use of the expanded dining area.

Finding:

- D. *Adequate temporary parking to accommodate vehicular traffic to be generated by the limited duration use would be available either on-site or at alternate locations acceptable to the Zoning Administrator; and*

Facts in Support of Finding:

1. The existing food service use with outdoor dining has operated at the subject property since 2014 and the expanded outdoor dining area has operated at the subject site since 2020. The existing surface parking lot has historically served as the primary parking supply for patrons. There are additional off-street parking spaces available onsite and on-street parking is available in proximity to the restaurant, along East Coast highway.
2. Fact 2 in Support of Finding C incorporated by reference.

Finding:

- E. *The limited duration use is consistent with all applicable provisions of the General Plan, any applicable specific plan, the Municipal Code, and other City regulations.*

Facts in Support of Finding:

1. The project site is categorized as Corridor Commercial (CC) by the Land Use Element of the General Plan. The CC category is intended to provide for a range of neighborhood-serving retail and service uses along street frontages that are located and designed to foster pedestrian activity. The expanded outdoor dining use is accessory to the existing food service use with outdoor dining, will be utilized for a limited duration on-site, and will not impede use of the site. Additionally, outdoor dining is a use that tends to foster additional pedestrian activity.
2. The site is within the Commercial Corridor (CC) Zoning District. The CC district is intended to provide a range of neighborhood-serving retail and service uses along street frontages that are located and designed to foster pedestrian activity. The CC zoning district allows food service uses and the expanded dining area is a temporary use, authorized with a limited term permit in the CC zoning district.
3. The Limited Term Permit for expanded outdoor dining would complement and be consistent with the other commercial uses permitted within the CC Zoning District of Corona del Mar in that it provides amenities that support visitors to the area and provides a social gathering place for those who live and work in the neighborhood, consistent with General Plan Land Use Element Goal LU2, below. Additional benefits include providing opportunities for the continuation of local businesses that generate sales tax and provide opportunities for employment, which is consistent with General Plan Land Use Element Policy LU 2.4 (Economic Development), also copied below:

**Goal LU 2** *A living, active, and diverse environment that complements all lifestyles and enhances neighborhoods, without compromising the valued resources that make Newport Beach unique. It contains a diversity of uses that support the needs of residents, sustain and enhance the economy, provide job opportunities, serve visitors that enjoy the City's diverse recreational amenities, and protect its important environmental setting, resources, and quality of life.*

**Policy LU 2.4 Economic Development** *Accommodate uses that maintain or enhance Newport Beach's fiscal health and account for market demands, while maintaining and improving the quality of life for current and future residents.*  
(Imp 1.1, 24.1)

4. Council Policy D-9 recognizes the need to balance economic development objectives with protection of the environment and the health and safety of the community. The policy recognizes the need to provide effective and efficient structures for implementing economic programs, utilizing staffing to provide healthy, thriving businesses, and maintain a healthy economy while preserving the unique commercial villages in Newport

Beach. The proposed limited term permit would support a local business and economic prosperity while maintaining the unique character of the Corona del Mar community.

5. The Property is not located within a specific plan area.

***Coastal Development Permit***

In accordance with Section 21.52.015(F) (Coastal Development Permits - Findings and Decision) of the NBMC, the following findings and facts in support of such findings are set forth:

Finding:

- F. Conforms to all applicable sections of the certified Local Coastal Program.*

Facts in Support of Finding:

1. The Property is located in the coastal zone and the proposed improvements require a coastal development permit in accordance with Newport Beach Municipal Code (NBMC) Section 21.52.035(C)(2) (Projects Exempt from Coastal Development Permit Requirements). The improvements constitute an increase of ten (10) percent or more of the internal floor area of an existing structure or a lesser improvement that has previously been undertaken pursuant to California Public Resources Code Section 30610(a). The expanded outdoor dining area and barrier within the parking lot are minor detached structures. The location of these improvements does not pose a conflict to coastal resources, coastal access, or other adverse environmental effects.
2. An initial evaluation was conducted of the project site in accordance with NBMC Section 21.30.100 (Scenic and Visual Quality Protection). The project site is not located between the first public roadway and the sea, the project site is not located on a coastal bluff or canyon, and the project site is not adjacent to a coastal view road, public access way, or Coastal Viewpoint as identified in the Coastal Land Use Plan. Furthermore, the development site contains no natural landforms or vegetation. The initial evaluation did not indicate that the project has the potential to significantly impact a public view or viewshed or the scenic and visual qualities of the coastal zone.
3. The nearest coastal viewpoint is Begonia Park, over 1,500 feet northwest of the Property. The subject property is situated further inland than Begonia Park, at a higher elevation, and not within the viewshed of the park. Due to the distance of the expanded outdoor dining area from the public viewpoint, the project will not impact coastal views. The expanded outdoor dining area complies with all applicable Local Coastal Program (LCP) development standards and maintains an area consistent with the existing pattern of development in Corona del Mar Village. Additionally, the project does not contain any unique features that could degrade the visual quality of the coastal zone.
4. The proposed outdoor dining area is located completely within private property.

5. Improvements are complementary to the area; the subject restaurant and adjacent neighbors have similar outdoor dining improvements within the parking areas and walkways of Corona del Mar.
6. Development authorized by this permit is not located in any environmentally sensitive habitat area and public access to the coast will not be blocked. Coastal access is increased by allowing commercial establishments to re-open allowing public to visit coastal areas and provides an added amenity for visitors. The proposed operation does not contain ESHA, wetlands, or sandy beach area;
7. Development authorized is not located in an area in which the California Coastal Commission retains direct permit review authority.

Finding:

- G. *Conforms with the public access and public recreation policies of Chapter 3 of the Coastal Act if the project is located between the nearest public road and the sea of shoreline of any body of water located within the coastal zone.*

Facts in Support of Finding:

1. The project site is not located between the nearest public road and the sea or shoreline. Implementation Plan Section 21.30A.040 (Determination of Public Access/Recreation Impacts) requires that the provision of public access bear a reasonable relationship between the requirement and the project's impact and be proportional to the impact. In this case, the project is not located by the sea where lateral and vertical coastal access would be needed.

SECTION 4. DECISION.

**NOW, THEREFORE, BE IT RESOLVED:**

1. The Zoning Administrator of the City of Newport Beach hereby finds this project is categorically exempt from the California Environmental Quality Act under Section 15301 under Class 1 (Existing Facilities) and Class 3 (New Construction or Conversion of Small Structures) of the CEQA Guidelines, California Code of Regulations, Title 14, Division 6, Chapter 3 because it has no potential to have a significant effect on the environment.
2. The Zoning Administrator of the City of Newport Beach hereby approves the Limited Term Permit and Coastal Development Permit filed as PA2023-0231, subject to the conditions set forth in Exhibit "A," which is attached hereto and incorporated by reference.
3. This action shall become final and effective 14 days following the date this Resolution was adopted unless within such time an appeal or call for review is filed with the Community Development Director by the provisions of Title 20 Planning and Zoning and Title 21 Local Coastal Implementation Plan, of the Newport Beach Municipal Code



**PASSED, APPROVED, AND ADOPTED THIS 25<sup>TH</sup> DAY OF APRIL 2024.**



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Benjamin M. Zueba, AICP, Zoning Administrator

**EXHIBIT “A”**

**CONDITIONS OF APPROVAL**

*(Project-specific conditions are in italics)*

**Planning Division**

1. *All conditions of approval from Minor Use Permit No. 2014-001 shall be adhered to unless specifically modified by the following set of conditions.*
2. The expanded outdoor dining area location shall be in substantial conformance with the approved site plan stamped and dated with the date of this approval, except as modified by applicable conditions of approval.
3. *The approval of this Limited Term Permit and Coastal Development Permit shall be effective from the effective date of this approval until December 31, 2024. The applicant shall be required to cease all permitted operations and remove any temporary improvements made to the outdoor spaces as part of this Limited Term Permit on or before December 31, 2024. No extension of time shall be authorized under this Limited Term Permit and Coastal Development Permit.*
4. *The outdoor dining area shall not exceed 375 square feet and shall occupy no more than three (3) on-site parking spaces. One (1) accessible parking space shall be maintained clear and accessible for use at all times.*
5. *The remaining on-site parking spaces shall remain free of obstructions and available for vehicle parking.*
6. *The existing allowed hours of operation of the establishment shall not be extended. The hours of operation of the temporary outdoor dining area as part of this approval shall not extend beyond 12 p.m. to 9 p.m., daily.*
7. The project is subject to all applicable City ordinances, policies, and standards, unless specifically waived or modified by the conditions of approval.
8. The Applicant shall adhere to all applicable State of California and Orange County Health Care Agency guidelines for the safe operation of the expanded outdoor dining area.
9. The Applicant shall comply with all federal, state, and local laws, and all conditions of the Alcoholic Beverage License. Material violation of any of those laws or conditions in connection with the use may be cause for revocation of the Limited Term Permit.
10. The Applicant is required to obtain all applicable permits from the City’s Building Division and Fire Department. The construction plans must comply with the most recent, City-adopted version of the California Building Code. The construction plans must meet all applicable State Disabilities Access requirements. Approval from the

Orange County Health Care Agency is required prior to the issuance of a building permit.

11. All noise generated by the proposed use shall comply with the provisions of Chapter 10.26 (Community Noise Control) and other applicable noise control requirements of the NBMC. The maximum noise shall be limited to no more than depicted below for the specified time periods unless the ambient noise level is higher:

	Between the hours of 7:00AM and 10:00PM		Between the hours of 10:00PM and 7:00AM	
Location	Interior	Exterior	Interior	Exterior
Single-, two- or multiple-family residential	45dBA	55dBA	40dBA	50dBA
Residential portions of mixed-use properties	45dBA	60dBA	40dBA	50dBA
Commercial	N/A	65dBA	N/A	60dBA
Industrial or manufacturing	N/A	70dBA	N/A	70dBA

12. *No outside paging system shall be utilized in conjunction with the expanded outdoor dining area.*
13. *There shall be no use of amplified sound or live entertainment in the expanded outdoor dining area.*
14. The expanded outdoor dining area shall be always maintained free of litter and graffiti. The owner or operator shall provide for daily removal of trash, litter debris, and graffiti from the premises and on all abutting sidewalks within 20 feet of the premises.
15. Deliveries and refuse collection for the facility shall be prohibited between the hours of 10:00 p.m. and 7:00 a.m. on weekdays and Saturdays and between the hours of 10:00 p.m. and 9:00 a.m. on Sundays and Federal holidays unless otherwise approved by the Director of Community Development and may require an amendment to this Limited Term Permit and Coastal Development Permit.
16. A Special Events Permit is required for any event or promotional activity outside the normal operating characteristics of the approved use, as conditioned, or that would attract large crowds, involve the sale of alcoholic beverages, include any form of on-site media broadcast, or any other activities as specified in the Newport Beach Municipal Code to require such permits.
17. The site shall not be excessively illuminated based on the luminance recommendations of the Illuminating Engineering Society of North America, or, in the opinion of the Director of Community Development, the illumination creates an unacceptable negative impact on surrounding land uses or environmental resources. The Director may order the dimming of light sources or other remediation upon finding that the site is excessively illuminated.

18. The Applicant shall obtain and maintain authorization from the State Department of Alcoholic Beverage Control (ABC) for all areas where the sale, service or consumption of alcohol is under the control of the applicant. The establishment shall abide by all applicable regulations of the State Department of Alcoholic Beverage Control.
19. All trash shall be stored within a building or within dumpsters stored in a trash enclosure or otherwise screened from view of neighboring properties, except when placed for pick-up by refuse collection agencies.
20. The Applicant shall ensure that the trash dumpsters and/or receptacles are maintained to control odors. This may include the provision of either fully self-contained dumpsters or periodic steam cleaning of the dumpsters, if deemed necessary by the Planning Division. Cleaning and maintenance of trash dumpsters shall be done in compliance with the provisions of Title 14, including all future amendments (including Water Quality related requirements).
21. Before the issuance of a building permit, the applicant shall pay any unpaid administrative costs associated with the processing of this application to the Planning Division.
22. This Limited Term Permit and Coastal Development Permit be modified or revoked by the Zoning Administrator if determined that the proposed uses or conditions under which it is being operated or maintained is detrimental to the public health, welfare or materially injurious to property or improvements in the vicinity or if the property is operated or maintained so as to constitute a public nuisance.
23. The Community Development Director or designee may inspect the modified area at any time during normal business hours.
24. Any change in operational characteristics, expansion in area, or other modification to the approved plans, shall require an amendment to this Limited Term Permit and Coastal Development Permit.
25. To the fullest extent permitted by law, Applicant shall indemnify, defend and hold harmless City, its City Council, its boards and commissions, officials, officers, employees, and agents from and against any and all claims, demands, obligations, damages, actions, causes of action, suits, losses, judgments, fines, penalties, liabilities, costs and expenses (including without limitation, attorney's fees, disbursements and court costs) of every kind and nature whatsoever which may arise from or in any manner relate (directly or indirectly) to City's **approval of the Pirozzi Corona del Mar Limited Term Permit and Coastal Development Permit including, but not limited to, PA2023-0231**. This indemnification shall include, but not be limited to, damages awarded against the City, if any, costs of suit, attorneys' fees, and other expenses incurred in connection with such claim, action, causes of action, suit or proceeding whether incurred by Applicant, City, and/or the parties initiating or bringing such proceeding. The Applicant shall indemnify the City for all of City's costs, attorneys' fees,

and damages which City incurs in enforcing the indemnification provisions set forth in this condition. The Applicant shall pay to the City upon demand any amount owed to the City pursuant to the indemnification requirements prescribed in this condition.

### **Building Department**

26. Any areas used for temporary commercial or institutional use shall be accessible to disabled persons.
  - a. A minimum 4-ft wide accessible path to all functional area shall be provided.
  - b. Access to restrooms shall be provided at all times.
  - c. Accessible parking stalls shall not be used for seating areas when onsite parking is provided.
  - d. At least one (1) accessible seating area shall be provided.
  - e. Detectable warnings are required when pedestrian paths cross or are adjacent to a vehicular way where no physical barrier are provided to separate the two (2).
27. All exiting paths shall be a minimum 36 inches free and clear. All public walks and sidewalks shall be minimum 48 inches free and clear.

### **Fire Department**

28. No heating equipment to be used in the tent unless approved by the Newport Beach Fire Department.
29. All exits from the existing building shall remain unobstructed and cannot exit into the tent.
30. Provide documents from the tent vendor indicating that the tent meets all smoke and flame spreading rates as required by the 2022 California Fire Code.
31. Fire extinguishers shall be provided in the tent.
32. Provide exit signs and no smoking signs in the tent.

### **Public Works Department**

33. The applicant shall install and maintain a substantial physical barrier (K-rail, water-filled traffic barrier or other barrier approved by the Public Works Department) between the proposed outdoor dining area and parking spaces and parking aisles in the parking lot. The physical barrier shall not encroach into the parking aisle or any required parking space.

## **RESOLUTION NO. ZA2024-027**

### **A RESOLUTION OF THE ZONING ADMINISTRATOR OF THE CITY OF NEWPORT BEACH APPROVING COASTAL DEVELOPMENT PERMIT TO DEMOLISH AN EXISTING SINGLE-FAMILY RESIDENCE AND CONSTRUCT A NEW TWO-STORY SINGLE-FAMILY RESIDENCE AND ATTACHED TWO-CAR GARAGE LOCATED AT 2632 CIRCLE DRIVE (PA2023-0234)**

THE ZONING ADMINISTRATOR OF THE CITY OF NEWPORT BEACH HEREBY FINDS AS FOLLOWS:

#### **SECTION 1. STATEMENT OF FACTS.**

1. An application was filed by Craig Hampton (Applicant), concerning property located at 2632 Circle Drive, requesting approval of a coastal development permit.
2. The lot at 2632 Circle Drive is legally described as Lot 22 of Tract 1102 (Property).
3. The applicant proposes the demolition of an existing 3,307 square-foot, single-unit residence with a two-car garage, and the construction of a new 3,784 square foot two-story single-unit residence and an attached 627 square-foot two-car garage (Project).
4. The Property is designated RS-D (Single Unit Residential Detached) by the General Plan Land Use Element and is located within the R-1 (Single Unit Residential) Zoning District.
5. The Property is located within the coastal zone. The Coastal Land Use Plan category is RSD-B (Single Unit Residential Detached – 6.0 – 9.9 DU/AC) and is located within the R-1 (Single-Unit Residential) Coastal Zoning District.
6. A public hearing was held on April 25, 2024, online via Zoom. A notice of the time, place, and purpose of the hearing was given by the Newport Beach Municipal Code (NBMC). Evidence, both written and oral, was presented to and considered by, the Zoning Administrator at this hearing.

#### **SECTION 2. CALIFORNIA ENVIRONMENTAL QUALITY ACT DETERMINATION.**

1. This Project is categorically exempt pursuant to Title 14 of the California Code of Regulations Section 15303, Division 6, Chapter 3, Guidelines for Implementation of the California Environmental Quality Act (CEQA) under Class 3 (New Construction or Conversion of Small Structures) because it has no potential to have a significant effect on the environment.
2. Class 3 exempts the demolition of up to three single-family residences and new construction of a single-family residence on any legal parcel. The proposed Project consists of the demolition of one single-family residence and the construction of a new 3,784 square-foot single-family residence and an attached 627 square-foot, two-car garage and therefore qualifies within this exemption.

3. The exceptions to this categorical exemption under Section 15300.2 are not applicable. The Project location does not impact an environmental resource of hazardous or critical concern, does not result in cumulative impacts, does not have a significant effect on the environment due to unusual circumstances, does not damage scenic resources within a state scenic highway, is not a hazardous waste site, and is not identified as a historical resource.

### SECTION 3. REQUIRED FINDINGS.

By Section 21.52.015F (Coastal Development Permits - Findings, and Decision) of the Newport Beach Municipal Code (NBMC), the following findings, and facts in support of such findings are set forth:

#### Finding:

- A. *Conforms to all applicable sections of the certified Local Coastal Program.*

#### Facts in Support of Finding:

1. The proposed development complies with applicable residential development standards including, but not limited to, floor area limitation, setbacks, height, and parking.
  - a. The maximum floor area limitation is 5,440 square feet and the proposed floor area is 4,411 square feet.
  - b. The proposed development provides the minimum required setbacks, which are 10 feet along the front property line abutting Circle Drive, 3 feet along each side property line, and 10 feet along the rear property line.
  - c. The highest guardrail is less than 24 feet from the established grade and the highest ridge is no more than 29 feet from the established grade, which complies with the maximum height requirements.
  - d. The Project includes garage parking for a total of two vehicles, complying with the minimum two-car garage parking requirement for single-family residences with less than 4,000 square feet of habitable floor area.
2. The neighborhood is predominantly developed with one- and two-story, single-family residences. The proposed design, bulk, and scale of the development are consistent with the existing neighborhood pattern of development.
3. The Property is approximately 300 feet from the bay and is separated from the water by a row of residences and Bay Shore Drive. The finished floor elevation of the first floor of the proposed structure is 13.30 feet based on the North American Vertical Datum of 1988 (NAVD88), which complies with the minimum 9.0-foot (NAVD88) elevation standard for new structures. The identified distances from the coastal hazard areas

coupled with the 13.30-foot NAVD88 finish floor elevation will help to ensure the Project is reasonably safe for the economic life of the structure.

4. The Property is in an area known for the potential of seismic activity and liquefaction. All projects are required to comply with the California Building Code (CBC) and Building Division standards and policies. Geotechnical investigations specifically addressing liquefaction are required to be reviewed and approved before the issuance of building permits. Permit issuance is also contingent on the inclusion of design mitigation identified in the investigations. Construction plans are reviewed for compliance with approved investigations and CBC before building permit issuance.
5. The Property is not located on the shoreline, nor is it located within 100 feet of coastal waters of coastal waters. A Construction Erosion Control Plan was provided to implement temporary Best Management Practices (BMPs) during construction to minimize erosion and sedimentation and to minimize pollution of runoff and coastal waters derived from construction chemicals and materials. The Project design also addresses water quality through the inclusion of a post-construction drainage system that includes drainage and percolation features designed to retain dry weather and minor rain event runoff on-site. Any water not retained on-site is directed to the City's storm drain system.
6. Proposed landscaping complies with Implementation Plan Section 21.30.075. A condition of approval is included that requires drought-tolerant species. Before the issuance of building permits, the final landscape plans will be reviewed to verify invasive species are not planted.
7. The Property is within a neighborhood that is adjacent to Coast Highway, a designated coastal view road. However, the segment of Coast Highway abutting the Bayshores Community is excluded from this designation. Therefore, the Property is not located adjacent to a coastal view road, public access way, or Coastal Viewpoint as identified in the Coastal Land Use Plan. The nearest coastal viewpoint is Castaways Park, which is not visible from the site. The Project site may be located within the viewshed of distant public viewing areas. However, the Project is located on a coastal lot and will replace an existing single-unit dwelling with a new single-unit dwelling that complies with all applicable Local Coastal Program development standards and maintains a building envelope consistent with the existing neighborhood pattern of development. Therefore, the Project does not have the potential to degrade the visual quality of the Coastal Zone or result in significant adverse impacts to public views.

Finding:

- B. Conforms with the public access and public recreation policies of Chapter 3 of the Coastal Act if the project is located between the nearest public road and the sea or shoreline of any body of water located within the coastal zone.*

Fact in Support of Finding:

1. The Property is located between the nearest public road and the sea or shoreline in the private community of Bayshores. Developed in 1941, Bayshores is a 258-lot, single-family



gated community located on the Lido Channel, southwest of Coast Highway and Newport Bay Bridge. The 39-unit Anchorage Apartment complex is also located within the community. The community is accessible from West Coast Highway via Bay Shore Drive, a private street. The shoreline is on the south and east sides of the community and consists of bulkheads and two small sandy beaches.

2. Coastal Land Use Plan Policy 3.1.5-3 requires public access consistent with public access policies for any new development in private/gated communities causing or contributing to adverse public access impacts. Implementation Plan Section 21.30A.040 (Determination of Public Access/Recreation Impacts) requires that the provision of public access bear a reasonable relationship between the requirement and the project's impact and be proportional to the impact. In this case, the Project replaces an existing single-unit residence located on a coastal lot with a new single-unit residence. Therefore, the Project does not involve a change in land use, density or intensity that will result in increased demand on public access and recreation opportunities. Furthermore, the project is designed and sited so as not to block or impede existing public access opportunities.
3. The Property does not currently provide, nor does it inhibit public coastal access. Vertical and lateral access to the bay front is available adjacent to the Bay Shores community at the Balboa Bay Club, immediately west of the Bayshores community.

#### SECTION 4. DECISION.

#### **NOW, THEREFORE, BE IT RESOLVED:**

1. The Zoning Administrator of the City of Newport Beach hereby finds this project is categorically exempt from the California Environmental Quality Act under Section 15303 under Class 3 (New Construction or Conversion of Small Structures) of the CEQA Guidelines, California Code of Regulations, Title 14, Division 6, Chapter 3 because it has no potential to have a significant effect on the environment.
2. The Zoning Administrator of the City of Newport Beach hereby approves Coastal Development Permit No. PA2023-0234, subject to the conditions outlined in Exhibit "A," which is attached hereto and incorporated by reference.
3. This action shall become final and effective 14 days following the date this Resolution was adopted unless within such time an appeal or call for review is filed with the Community Development Director by the provisions of Title 21 Local Coastal Program (LCP) Implementation Plan, of the Newport Beach Municipal Code. Final action taken by the City may be appealed to the Coastal Commission in compliance with Section 21.64.035 (Appeal to the Coastal Commission) of the City's certified LCP and Title 14 California Code of Regulations, Sections 13111 through 13120, and Section 30603 of the Coastal Act.

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**PASSED, APPROVED, AND ADOPTED THIS 25<sup>TH</sup> DAY OF APRIL, 2024.**



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Benjamin M. Zueba, AICP, Zoning Administrator

**EXHIBIT “A”****CONDITIONS OF APPROVAL**

*(Project-specific conditions are in italics)*

**Planning Division**

1. The development shall be in substantial conformance with the approved site plan, floor plans, and building elevations stamped and dated with the date of this approval (except as modified by applicable conditions of approval).
2. No demolition or construction materials, equipment debris, or waste, shall be placed or stored in a location that would enter the sensitive habitat, receiving waters, or storm drains or result in impacts to environmentally sensitive habitat areas, streams, the beach, wetlands, or their buffers. No demolition or construction materials shall be stored on public property.
3. The Applicant is responsible for compliance with the Migratory Bird Treaty Act (MBTA). In compliance with the MBTA, grading, brush removal, building demolition, tree trimming, and similar construction activities shall occur between August 16 and January 31, outside of the peak nesting period. If such activities must occur inside the peak nesting season from February 1 to August 15, compliance with the following is required to prevent the taking of native birds under MBTA:
  - A. The construction area shall be inspected for active nests. If birds are observed flying from a nest or sitting on a nest, it can be assumed that the nest is active. Construction activity within 300 feet of an active nest shall be delayed until the nest is no longer active. Continue to observe the nest until the chicks have left the nest and activity is no longer observed. When the nest is no longer active, construction activity can continue in the nest area.
  - B. It is a violation of state and federal law to kill or harm a native bird. To ensure compliance, consider hiring a biologist to assist with the survey for nesting birds, and to determine when it is safe to commence construction activities. If an active nest is found, one or two short follow-up surveys will be necessary to check on the nest and determine when the nest is no longer active.
4. Best Management Practices (BMPs) and Good Housekeeping Practices (GHPs) shall be implemented before and throughout the duration of construction activity as designated in the Construction Erosion Control Plan.
5. The discharge of any hazardous materials into storm sewer systems or receiving waters shall be prohibited. Machinery and equipment shall be maintained and washed in confined areas specifically designed to control runoff. A designated fueling and vehicle maintenance area with appropriate berms and protection to prevent spillage shall be provided as far away from storm drain systems or receiving waters as possible.

6. Debris from demolition shall be removed from work areas each day and removed from the Project site within 24 hours of the completion of the Project. Stockpiles and construction materials shall be covered, enclosed on all sites, not stored in contact with the soil, and located as far away as possible from drain inlets and any waterway.
7. Trash and debris shall be disposed of in proper trash and recycling receptacles at the end of each construction day. Solid waste, including excess concrete, shall be disposed of in adequate disposal facilities at a legal disposal site or recycled at a recycling facility.
8. Revisions to the approved plans may require an amendment to this Coastal Development Permit or the processing of a new coastal development permit.
9. The Project is subject to all applicable City ordinances, policies, and standards unless specifically waived or modified by the conditions of approval.
10. The Applicant shall comply with all federal, state, and local laws. A material violation of any of those laws in connection with the use may be caused by the revocation of this Coastal Development Permit.
11. This Coastal Development Permit may be modified or revoked by the Zoning Administrator if determined that the proposed uses or conditions under which it is being operated or maintained are detrimental to the public health, and welfare or materially injurious to Property or improvements in the vicinity or if the Property is operated or maintained to constitute a public nuisance.
12. *Before the issuance of building permits, the Applicant shall submit a final construction erosion control plan. The plan shall be subject to review and approval by the Building Division.*
13. *Before the issuance of building permits, the Applicant shall submit a final drainage and grading plan. The plan shall be subject to review and approval by the Building Division.*
14. *Before issuance of a building permit, a copy of the Resolution, including conditions of approval Exhibit "A" shall be incorporated into the Building Division and field sets of plans.*
15. *Before the issuance of a building permit, the Applicant shall submit a final landscape and irrigation plan. These plans shall incorporate drought-tolerant plantings, non-invasive plant species, and water-efficient irrigation design. The plans shall be approved by the Planning Division.*
16. All landscape materials and irrigation systems shall be maintained by the approved landscape plan. All landscaped areas shall be maintained in a healthy and growing condition and shall receive regular pruning, fertilizing, mowing, and trimming. All landscaped areas shall be kept free of weeds and debris. All irrigation systems shall be kept operable, including adjustments, replacements, repairs, and cleaning as part of regular maintenance.

17. Construction activities shall comply with Section 10.28.040 (Construction Activity – Noise Regulations) of the Newport Beach Municipal Code, which restricts hours of noise-generating construction activities that produce noise to between the hours of 7:00 a.m. and 6:30 p.m., Monday through Friday. Noise-generating construction activities are not allowed on Saturdays, Sundays, or Holidays.
18. All noise generated by the proposed use shall comply with the provisions of Chapter 10.26 (Community Noise Control) and other applicable noise control requirements of the Newport Beach Municipal Code. The maximum noise shall be limited to no more than depicted below for the specified periods unless the ambient noise level is higher:

	Between the hours of 7:00 AM and 10:00 PM		Between the hours of 10:00 PM and 7:00 AM	
Location	Interior	Exterior	Interior	Exterior
Residential Property	45dBA	55dBA	40dBA	50dBA
Residential Property located within 100 feet of a commercial property	45dBA	60dBA	45dBA	50dBA
Mixed Use Property	45dBA	60dBA	45dBA	50dBA
Commercial Property	N/A	65dBA	N/A	60dBA

19. Before the issuance of the building permit, the Applicant shall pay any unpaid administrative costs associated with the processing of this application to the Planning Division.
20. Should the Property be sold or otherwise come under different ownership, any future owners or assignees shall be notified of the conditions of this approval by the current property owner or agent.
21. This Coastal Development Permit No. PA2023-0234 shall expire unless exercised within 24 months from the date of approval as specified in Section 21.54.060 (Time Limits and Extensions) of the Newport Beach Municipal Code, unless an extension is otherwise granted.
22. To the fullest extent permitted by law, the Applicant shall indemnify, defend and hold harmless the City, its City Council, its boards and commissions, officials, officers, employees, and agents from and against any claims, demands, obligations, damages, actions, causes of action, suits, losses, judgments, fines, penalties, liabilities, costs, and expenses (including without limitation, attorney's fees, disbursements, and court costs) of every kind and nature whatsoever which may arise from or in any manner relate (directly or indirectly) to City's approval of **Beane Residence including but not limited to, Coastal Development Permit No. PA2023-0234**. This indemnification shall include, but not be limited to, damages awarded against the City, if any, costs of suit, attorney's fees, and other expenses incurred in connection with such claim, action, causes of action, suit, or proceeding whether incurred by the Applicant, City, and/or the parties initiating or bringing such proceeding. The Applicant shall indemnify the City for all the City's costs, attorneys' fees, and damages, which the City incurs in enforcing the indemnification provisions outlined in this condition. The Applicant shall pay to the City upon demand any amount owed to the City under the indemnification requirements prescribed in this condition.

### **Building Division**

23. Foundation shall be design for liquefaction mitigation per policy CBC 1803.5.11-12.
24. The applicant is required to obtain all applicable permits from the City's Building Division and Fire Department. The construction plans must comply with the most recent, City-adopted version of the California Building Code. The construction plans must meet all applicable State Disabilities Access requirements.
25. The Applicant shall employ the following best available control measures (BACMs) to reduce construction-related air quality impacts:

#### **Dust Control**

- Water all active construction areas at least twice daily.
- Cover all haul trucks or maintain at least two feet of freeboard.
- Pave or apply water four times daily to all unpaved parking or staging areas.
- Sweep or wash any site access points within two hours of any visible dirt deposits on any public roadway.
- Cover or water twice daily any on-site stockpiles of debris, dirt, or other dusty material.
- Suspend all operations on any unpaved surface if winds exceed 25 mph.

#### **Emissions**

- Require 90-day low-NOx tune-ups for off-road equipment.
- Limit allowable idling to 30 minutes for trucks and heavy equipment.

#### **Off-Site Impacts**

- Encourage carpooling for construction workers.
- Limit lane closures to off-peak travel periods.
- Park construction vehicles off traveled roadways.
- Wet down or cover dirt hauled off-site.
- Sweep access points daily.
- Encourage receipt of materials during non-peak traffic hours.
- Sandbag construction sites for erosion control.

#### **Fill Placement**

- The number and type of equipment for dirt pushing will be limited on any day to ensure that SCAQMD significance thresholds are not exceeded.
- Maintain and utilize a continuous water application system during earth placement and compaction to achieve a 10% soil moisture content in the top six-inch surface layer, subject to review/discretion of the geotechnical engineer.

### **Fire Division**

26. NFPA 13D fire sprinkler system required to be installed.



## COMMUNITY DEVELOPMENT DEPARTMENT PLANNING DIVISION

100 Civic Center Drive, P.O. Box 1768, Newport Beach, CA 92658-8915  
949-644-3200

[www.newportbeachca.gov](http://www.newportbeachca.gov)

### **COMMUNITY DEVELOPMENT DIRECTOR** **ACTION LETTER**

**Subject:** Starbucks Jamboree Road (PA2024-0035)  
▪ Staff Approval for Substantial Conformance

**Site Location** 4501 Jamboree Road

**Applicant** Starbucks Coffee Company

**Legal Description** Parcel 2 of Resubdivision No. 0557 in Book 108, Page 27 of Parcel Maps

On April 26, 2024, the Community Development Director found substantial conformance and approved Staff Approval (PA2024-0035) to allow the conversion of a Wienerschnitzel drive-thru restaurant into a Starbucks drive-thru café located at 4501 Jamboree Road. The proposed changes do not intensify the existing use and are in substantial conformance with Use Permit No. UP1797C. This approval is based on the following analysis and is subject to all Conditions and Assumptions thereafter in Section V.

#### **LAND USE AND ZONING**

- **General Plan Land Use Plan Category:** Mixed Use Horizontal 2 (MU-H2)
- **Zoning District:** PC-15 (Koll Center Office Site G)

#### **I. PREVIOUS APPROVALS FOR THE EXISTING USE**

On July 15, 1976, the Planning Commission approved Use Permit No. UP1797 to construct and operate a Wienerschnitzel drive-thru restaurant within Office Site G of the Koll Center Planned Community (PC-15). The restaurant proposed a drive-thru aisle with a take-out window, a 630-square-foot dining room, a kitchen, service counter, restroom facilities, office, and back-of-house storage for a total of approximately 2,100-square-feet of gross floor area. The original proposed hours of operation were from 10:30 a.m. to 11 p.m. While the Newport Beach Municipal Code (NBMC) required 42 parking spaces, the Planning Commission approved the applicant's proposal to provide only 32 on-site parking spaces with the potential for eight additional cars queued in the drive-thru aisle.

On February 10, 1983, the Planning Commission approved an amendment to UP1797 (UP1797A) to add 850 square feet of outdoor dining space. As a part of the amendment, the applicant submitted a parking study demonstrating that the provided on-site parking was adequate. The number of required on-site parking spaces was further reduced to 31 with capacity for eight cars queued in the drive-thru aisle.

On September 6, 1984, the Planning Commission approved a second amendment to UP1797 (UP1797B) to allow the onsite consumption of beer and wine. No further changes were made to parking requirements.

On June 20, 1985, the Planning Commission approved a third and final amendment to UP1797 (UP1797C) to allow a portion of the outdoor dining area to be enclosed for a total gross floor area of roughly 2,300 square feet. No changes were made to parking requirements.

## **II. CURRENT OPERATIONAL CHARACTERISTICS**

The restaurant currently operates pursuant to UP1797 and its amendments. Current hours of operation are from 8 a.m. to 11 p.m., Monday through Friday, with an earlier closing time of 10 p.m. on Saturday and Sunday. The restaurant provides 47 indoor seats and 32 outdoor seats.

In 2016, the operator received a building permit to add a storage room at the rear of the building, bringing the total gross floor area of the restaurant to approximately 2,668 square feet. The accessibility upgrades required by the California Building Code as part of the storage room addition reduced the total number of onsite parking spaces to 30. A reduction in required onsite parking spaces provided to accommodate an accessibility upgrade associated with tenant improvements is allowed administratively by the Director pursuant to NBMC 20.40.110 (Adjustments to Off-Street Parking Requirements). The drive-thru currently provides stacking for nine cars.

In late 2022, Starbucks expressed interest in converting the restaurant into a Starbucks drive-thru Café and applied for a Staff Approval to find the conversion in substantial conformance with UP1797C. On March 2, 2023, the Community Development Director approved the request filed as PA2022-0165. The approval required a reconfiguration of the parking lot and drive-thru layout to provide adequate queuing capacity without implementing queuing management measures. The Applicant was ultimately unable to implement the project as approved due to a private, covenants, conditions, and restrictions (CC&Rs) issue prohibiting modification to the drive-thru layout. As such, the Applicant is abandoning all entitlement granted by PA2022-0165.

## **III. PROPOSED CHANGES**

The applicant has modified the project and requests a new staff approval for substantial conformance with UP1797 and its amendments to convert the existing restaurant into a Starbucks drive-thru café.

Table 1 below summarizes the proposed changes to the existing restaurant:



<b>Table 1 – Existing Condition versus Proposed Condition</b>		
	<b>Existing</b>	<b>Proposed</b>
Gross Floor Area	2,668 square feet	2,117 square feet
Number of Indoor Seats	47 seats	25 seats
Number of Outdoor Seats	32 seats	16 seats
Number of Parking Spaces	30 parking spaces	30 parking spaces
Drive-thru Capacity	9 cars	9 cars
Hours of Operation	9 a.m. to 11 p.m.	5 a.m. to 11 p.m., daily

At the direction of the City Traffic Engineer, a Drive-Thru Queuing, Site Access, and Parking Evaluation was prepared by Linscott Law & Greenspan, Engineers, dated March 27, 2024 (LLG Report), and is available as Attachment No. CD 3. The LLG Report found that the current site design and layout is adequate to serve drive-thru queues generated by the project with the implementation of a management measure. The LLG Report requires a Starbucks team member to be dispatched to the drive-thru entrance to facilitate drive-thru orders and prevent spill over into the drive aisle whenever the drive-thru queue reaches nine vehicles. This management measure has been incorporated into the Staff Approval as Condition and Assumption No. 10.

As shown above in Table 1, the project will reduce the overall size of the building and number of seats. Other building improvements include a refinish of the exterior façade to be consistent with Starbucks branded paint colors, materials, and signage. A new approximately 145-square-foot trash enclosure is proposed along the rear property line and new landscape will be installed along the front property line, interior property lines, and adjacent to the drive-thru lane. While the project will restripe a portion of the parking lot to meet current accessibility standards, the overall number of onsite parking spaces will not change. The project plans are available as Attachment No. CD 4.

#### **IV. FINDINGS**

Pursuant to Section 20.54.070 (Changes to an Approved Project) of the NBMC, the Community Development Director may authorize minor changes to an approved site plan, architecture, or the nature of the approved use without a public hearing where the Director first finds that the changes:

##### **Finding:**

A. *The changes are consistent with all applicable provisions of this Zoning Code.*

##### **Facts in Support of Finding:**

1. The project site is located within the PC-15 (Koll Center Office Site G) Zoning District. Restaurants are an allowed use within the designated commercial areas of PC-15. Specifically, Office Site G is allowed three (3) restaurant, bar, or theater/nightclub

uses. The project site is one of three sites within Office Site G dedicated to restaurant uses.

2. Part II (Commercial), Section I (Site Area and Building Area) of PC-15 provides allowable building area for sites located within Office Site G. The project results in a reduction of the existing gross floor area.
3. Part II (Commercial), Section III (General Development Standards for Commercial Land), subsection C (Setbacks) requires a 30-foot front setback and no side or rear setback. The remodeled building is set back approximately 80 feet from the front property line.
4. Part II (Commercial), Group V (Restaurants), subsection E (Building Height) of PC-15 suggests a maximum height limit of 35 feet. The remodeled building will measure only 16 feet, 5 inches from the existing grade to the highest parapet and is within the suggested height limit.

Finding:

- B. The changes do not involve a feature of the project that was a basis for or subject of finding or exemptions in a negative declaration or Environmental Impact Report for the project.*

Facts in Support of Finding:

1. The project was originally found to be categorically exempt from the requirements of CEQA under Section 15301, Class 1 (Existing Facilities). Class 1 consists of the operation, repair, maintenance, permitting, leasing, licensing, or minor alteration of existing public or private structures, facilities, mechanical equipment, or topographical features, involving negligible or no expansion of existing or former use.
2. The proposed changes will alter an existing fast-food restaurant and will ultimately result in a reduction to both gross floor area of the restaurant and total onsite seating. Operational changes to the existing restaurant, such as the hours, will not result in an intensification of use or an expansion of use beyond the existing drive-thru use. The proposed changes are categorically exempt under Section 15301.

Finding:

- C. The changes do not involve a feature of the project that was specifically addressed or was the subject of a condition(s) of approval for the project or that was a specific consideration by the applicable review authority in the project approval.*

Facts in Support of Finding:

1. The property is within a developed neighborhood and the existing use of the building as a drive-thru restaurant will remain.
2. The proposed project was reviewed by relevant City departments including the Building Division, Public Works Department, and Fire/Life Safety Services. It was determined that adequate public and emergency vehicle access, public services, and utilities are provided to the subject property.
3. The proposed changes do not involve a feature that was specifically addressed in the approval for Use Permit No. UP1797 and its amendments.
4. In no past approvals were any limitations placed on the hours of operation for the restaurant nor were the hours used to make any required findings.
5. The project will not result in a reduction to the number of provided on-site parking spaces.
6. Project assumptions have been included as a part of this Staff Approval to provide zoning clearance for the use. For example, Assumption No. 7 assumes the hours of operation for drive-thru service and dining areas to be from 5 a.m. to 11 p.m., daily, and that all customers shall vacate the specified premises after these designated closing times.
7. Given the operational characteristics of the restaurant, noise from music or voices is unlikely to be generated at a level loud enough to impact surrounding uses. The largest stationary source of noise will be the speaker from the ordering system. Assumption No. 9 assumes the ordering system will be equipped with automatic volume control (AVC) that allows for adjustments if noise is found to be problematic.

Finding:

- D. The changes do not result in an expansion or change in operational characteristics of the use.*

Facts in Support of Finding:

1. The project consists of a minor alteration to an existing fast-food restaurant and a modification to hours of operation that do not constitute late hours or beyond 11 p.m. The project does not incorporate a new use or intensify the existing use.
2. All Facts in Support of Finding C incorporated by reference.

**V. CONDITIONS AND ASSUMPTIONS**

1. All previous conditions of approval for Conditional Use Permit No. UP1979 and subsequent amendments shall remain in full force and effect, as stated in Attachment No. CD 2.
2. Prior to Issuance of a Building Permit, a copy of this Staff Approval and the Conditions of Approval from UP1979C shall be incorporated into the Building Division and field sets of plans.
3. Upon implementation of PA2024-0035, PA2022-0165 shall be considered abandoned and will become null and void.
4. To the fullest extent permitted by law, the applicant shall indemnify, defend and hold harmless City, its City Council, its boards and commissions, officials, officers, employees, and agents from and against any claims, demands, obligations, damages, actions, causes of action, suits, losses, judgments, fines, penalties, liabilities, costs, and expenses (including without limitation, attorney's fees, disbursements, and court costs) of every kind and nature whatsoever which may arise from or in any manner relate (directly or indirectly) to City's approval of Starbucks Jamboree Road including, but not limited to, Staff Approval (PA2024-0035). This indemnification shall include, but not be limited to, damages awarded against the City, if any, costs of suit, attorney's fees, and other expenses incurred in connection with such claim, action, causes of action, suit, or proceeding whether incurred by the applicant, City, and/or the parties initiating or bringing such proceeding. The applicant shall indemnify the City for all the City's costs, attorneys' fees, and damages that which City incurs in enforcing the indemnification provisions outlined in this condition. The applicant shall pay to the City upon demand any amount owed to the City pursuant to the indemnification requirements prescribed in this condition.

The following assumptions are specific to providing zoning clearance through this Staff Approval and do not serve to modify or amend those conditions of the original permit. If any of these assumptions change, the project may no longer be in substantial conformance and may require the processing of a new use permit. In particular, this determination of substantial conformance is based on the following:

5. The project shall be substantially similar to the approved site plan and floor plan (Attachment No. CD 4).
6. No alcohol is proposed to be served onsite.
7. The hours of operation for drive-thru service and dining areas shall be limited to 5 a.m. to 11 p.m., daily. All customers shall vacate the specified premises after these designated closing times.

8. The gross floor area shall be limited to 2,117 square feet with a maximum of 25 seats. The outdoor dining area shall be limited to 322 square feet with a maximum of 16 seats.
9. The drive-thru order speaker shall be equipped with automatic volume control (AVC) and the drive-thru menu board shall be equipped with automatic brightness control to automatically adjust to the brightness level in connection with the brightness of the sun. The menu board shall turn off during closing hours.
10. When drive-thru queuing exceeds nine cars, an employee shall be deployed to the drive-thru entrance to facilitate drive-thru orders.

**APPEAL PERIOD:** An appeal or call for review may be filed with the Director of Community Development or City Clerk, as applicable, within fourteen (14) days following the date the action or decision was rendered unless a different period is specified by the NBMC. For additional information on filing an appeal, contact the Planning Division at 949-644-3200.

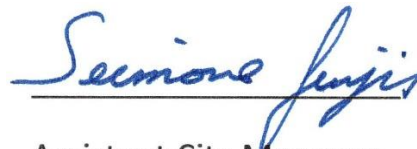
Prepared by:



Joselyn Perez  
Associate Planner

BMZ/jp

Approved by:



Assistant City Manager

Attachments:

CD 1 Vicinity Map  
CD 2 Use Permit No. UP1797C  
CD 3 Drive-Thru Queuing, Site Access, and Parking Evaluation  
CD 4 Project Plans

# **Attachment No. CD 1**

Vicinity Map

# VICINITY MAP



Staff Approval  
(PA2024-0035)

**4501 Jamboree Road**

# **Attachment No. CD 2**

Use Permit No. UP1797C



USE PERMIT APPLICATION  
CITY OF NEWPORT BEACH

PLANNING DEPARTMENT  
3300 Newport Boulevard  
P. O. Box 1768  
Newport Beach, CA 92658-8915  
(714) 644-3200

No. 1797A  
Application Rec'd by Shirley Brown  
Fee: \$ 688<sup>00</sup>

Applicant (Print) DER WIENERSCHNITZEL INTERNATIONAL, INC. Phone 714/752/5800

Mailing Address P. O. Box 7460, Newport Beach, CA 92658-7460

Property Owner Thomas and Lynn Wooldridge Phone 805/434-1657

Mailing Address 4 Santa Rita Ranch Road, Tempton, CA 93465

Address of Property Involved 4501 Jamboree Road, Newport Beach, California

Purpose of Application (describe fully) The applicant would like to add an ice cream sales area to the existing fast food restaurant by deleting some of the existing outdoor dining areas. This will decrease the seating capacity for indoor and outdoor dining by approx. 24 seats.

Zone P-C Present Use fast-food restaurant

Legal Description of Property Involved (if too long, attach separate sheet)  
Parcel 2 of Lot 1 of Tract No. 7953, City of Newport Beach, County of Orange, State of California, commonly known as 4501 Jamboree Road, Newport Beach, California, as per Maps recorded in Book 108 Pages 27 and 28 of Parcel Maps in the office of the County Recorder of Orange County, California.

OWNER'S AFFIDAVIT

I (We) Thomas H. And Lynn K. Wooldridge depose  
and say that ~~I am~~ (we are) the owner(s) of the property(ies) involved in this  
application. I (We) further certify, under penalty of perjury, that the fore-  
going statements and answers herein contained and the information herewith submitted  
are in all respects true and correct to the best of ~~my~~ (our) knowledge and belief.

Signature(s) Thomas H. Wooldridge 5/20/85  
Lynn K. Wooldridge 5-20-85

NOTE: An agent may sign for the owner if written authorization from the record owner is filed with the application.

DO NOT COMPLETE APPLICATION BELOW THIS LINE

Date Filed 5-24-85 Fee Pd. 688<sup>00</sup> Receipt No. \_\_\_\_\_

Hearing Date 6-20-85

Posting Date \_\_\_\_\_ Mail Date \_\_\_\_\_

P. C. Action \_\_\_\_\_ Date \_\_\_\_\_

Appeal \_\_\_\_\_ C. C. Hearing \_\_\_\_\_

C. C. Action \_\_\_\_\_ Date \_\_\_\_\_

EICHENHOFER  
GOFF  
KOPPELMAN  
KURLANDER  
PERSON  
TURNER  
WINBURN

## City of Newport Beach

ROLL CALL

INDEX

~~60,000 sq. ft. of Administrative, Professional, Financial Commercial uses on the vacant site north of the Post Office. Said General Plan Amendment will also look at development potential on underdeveloped sites in the immediate area.~~

3. The subject property has been developed in accordance with the City's General Plan and zoning. No further development rights exist on the subject site.
4. The subject property is currently leased to A.T.T. by The Irvine Company. There are approximately ten years remaining on the lease. The Irvine Company has indicated that the uses requested would not be permitted under the existing lease, and the Company has no current plans for a change in use.
5. The Planning Department has sufficient workload and projects of higher priority assigned to it, as noted in the staff report.
6. This request raises an issue as to the intent of the City Council under Council Policy Q-1 and the right of citizens to make requests for General Plan Amendments which specifically affect individual properties in which they have no right, title or interest.

\* \* \*

Use Permit No. 1797 (Amended) (Public Hearing)

Request to amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

LOCATION: Parcel No. 2 of Parcel Map 108-27 (Resubdivision No. 577), located at 4501 Jamboree Road, between Birch Street and MacArthur Boulevard, in the Koll Center Newport Planned Community.

ZONE: P-C

Item No. 4

UP1797AApproved

EICHENHOFER  
GOFF  
KOPPELMAN  
KURLANDER  
PERSON  
TURNER  
WINBURN

## City of Newport Beach

ROLL CALL

INDEX

APPLICANT: Der Wienerschnitzel International, Inc.,  
Newport Beach

OWNERS: Thomas H. and Lynn K. Wooldridge,  
Templeton

The public hearing was opened at this time, and Eva Sloan, architect, appeared before the Planning Commission on behalf of the applicant. Ms. Sloan stated that the applicant concurs with the findings and conditions in Exhibit "A".

The public hearing was closed at this time.

Commissioner Person made a motion to approve Use Permit No. 1797 (Amended) subject to the findings and conditions in Exhibit "A". Motion voted on, MOTION CARRIED.

FINDINGS:

1. That the subject restaurant and proposed ice cream parlor is consistent with surrounding land uses.
2. The project will not have any significant environmental impact.
3. That the proposed change in the operational characteristics of the existing restaurant so as to include the proposed ice cream facility, will not increase the parking demand of the restaurant inasmuch as said expansion will occupy space previously used for outdoor eating purposes.
4. Adequate off-street parking spaces and traffic circulation are being provided for the proposed development.
5. The Police Department has indicated that they do not contemplate any problems.
6. The approval of Use Permit No. 1797 (Amended) will not, under the circumstances of this case be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing and working in the neighborhood or be detrimental or injurious to property and improvements in the neighborhood or the general welfare of the City.

Motion  
All Ayes

x

EICHENHOFEN  
GOFF  
KOPPELMAN  
KURLANDER  
PERSON  
TURNER  
WINBURN

## City of Newport Beach

ROLL CALL

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CONDITIONS:

1. That development shall be in substantial conformance with the approved plot plan, floor plan and elevations.
2. That all applicable conditions of approval of the original Use Permit No. 1797 and subsequent amendments shall be fulfilled.

\* \* \*

Site Plan Review No. 39 (Public Hearing)

Item No. 5

Request to permit the construction of a retail commercial Parcel 1 of Parcel Map No. 84-726, (Resubdivision No. 796), located at 3100 East Coast Highway, on the northeasterly corner of East Coast Highway and Jasmine Avenue, in Corona del Mar.

SPR 39

Approved

ZONE: C-1

APPLICANT: Ernest George, Corona del Mar

OWNER: Same as applicant

Planning Director James Hewicker stated that the service station storage tanks on the site were removed in July, 1984, at which time an inspector from the Fire Department was present, wherein there was no determination or observation that there was any storage tank leakage or contamination.

In response to a question posed by Commissioner Kurlander, Mr. William Laycock, Current Planning Administrator, advised that the parking requirements are determined by the gross floor area of a building, exclusive of exterior walls.

In reference to Condition No. 28, Commissioner Goff inquired how to revoke the approval of a Site Plan Review? Mr. Hewicker replied that language could be added that if there are situations that occur in the future, wherein the Planning Commission has determined that a problem has been created, then the Planning Commission could have the situations corrected rather than revoke the Site Plan Review. Chairman Winburn concurred to correct Condition No. 28 to state that the condition should read "Site Plan Review" instead of

CITY OF NEWPORT BEACH

TO: Planning Commission  
FROM: Planning Department  
SUBJECT: Use Permit No. 1797 (Amended) (Public Hearing)

Request to amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

LOCATION: Parcel No. 2 of Parcel Map 108-27 (Resubdivision No. 577), located at 4501 Jamboree Road, between Birch Street and MacArthur Boulevard, in the Koll Center Newport Planned Community.

ZONE: P-C

APPLICANT: Der Wienerschnitzel International, Inc., Newport Beach

OWNERS: Thomas H. and Lynn K. Wooldridge, Templeton

Application

This application is a request to amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant. In accordance with Section 20.30.035 of the Newport Beach Municipal Code, changes in the operational and physical design characteristics of a restaurant require the securing of a use permit in each case. Use Permit procedures are outlined in Chapter 20.80 of the Newport Beach Municipal Code.

Environmental Significance

This project has been reviewed and it has been determined to be categorically exempt under Class 1 (Existing Facilities) from the requirements of the California Environmental Quality Act.

Conformance with the General Plan

The Land Use Element of the General Plan designates the site for "Administrative, Professional, and Financial Commercial" and "Retail and Service Commercial" uses. The subject restaurant conforms with the uses permitted.

TO:

Planning Commission -2.

### Subject Property and Surrounding Land Use

The "Der Wienerschnitzel" take-out restaurant, related drive-through lane and parking areas are located on the subject property. To the north, is Baxter's Restaurant and related parking area; to the east, across Jamboree Road is a portion of the University of California, Irvine, property; and to the south and west are commercial office and industrial uses and related parking areas.

### Background

At its meeting of July 15, 1976, the Planning Commission approved Use Permit No. 1797 which involved a request to establish the Der Wienerschnitzel take-out and drive-through restaurant on the subject property. Said action was subject to 5 findings and 7 conditions of approval as outlined in the attached excerpt of the Planning Commission minutes dated July 15, 1976.

At its meeting of February 10, 1983, the Planning Commission approved Use Permit No. 1797 (Amended), a request to allow the addition of an outdoor eating area to the existing restaurant and a request to waive a portion of the required off-street parking spaces for said expansion. Said action was subject to 5 findings and 3 conditions of approval as outlined in the attached excerpt of the Planning Commission minutes dated February 10, 1983.

At its meeting of September 6, 1984, the Planning Commission approved a second amendment to Use Permit No. 1797 so as to allow a change in the operational characteristics of the subject restaurant so as to permit on-sale beer and wine. Said approval was subject to 6 findings and 6 conditions of approval as set forth in the attached excerpt of the September 6, 1984 Planning Commission minutes.

### Analysis

The applicant is now proposing to enclose a portion of the existing outdoor eating area so as to include an ice cream sales facility in conjunction with the subject take-out restaurant. The new ice cream sales area will include approximately 475± sq.ft. of gross floor area and will have the same hours of operation as the Der Wienerschnitzel which currently operates from 6:00 a.m. to 10:00 p.m. Monday through Saturday and 9:00 a.m. to 10:00 p.m. on Sunday. There are currently 8 employees on-site during peak hours of operation and the applicant has indicated that no additional employees will be hired as a result of adding the ice cream service area.

### Required Off-Street Parking

The existing take-out restaurant includes 2,995 sq.ft. of gross floor area (including the outdoor eating areas). Based on the take-out restaurant parking requirement of one parking space for each 50 sq.ft.

TO:

Planning Commission -3.

of gross floor area, plus one space for each employee on the maximum work shift, the existing restaurant requires 68 parking spaces ( $2,995 \text{ sq.ft.} \div 50 \text{ sq.ft.} = 59.9$  or 60 spaces + 8 = 68 spaces). In conjunction with the February 10, 1983 approval of Use Permit No. 1797 (Amended), the Planning Commission waived 29 off-street parking spaces, thereby reducing the parking requirement to 39 spaces. There are currently 31 parking spaces located on the subject property and the "stacking lane" for the drive through windows accommodates a minimum of 8 additional spaces for a total of 39 parking spaces.

As indicated in the attached letter from the applicant, the number of peak hour employees will remain at eight persons; therefore, no additional parking spaces will be required inasmuch as the proposed ice cream service area will occupy space that was previously used for outdoor dining purposes and was included in the previous off-street parking calculation.

#### Signs

The existing restaurant currently has one 23± sq.ft., illuminated wall sign on the front elevation of the building and a total of four freestanding signs at various locations on the site. The largest freestanding sign is a 53± sq.ft. monument identification sign adjacent to Jamboree Road. Other freestanding signs include a 1.5 sq.ft. "drive through" sign; a 16.5 sq.ft. "order board"; and a 1.5 sq.ft. "do not enter" sign adjacent to the building. All of these signs were approved in conjunction with the original approval of Use Permit No. 1797.

As shown on the attached elevations, the applicant is proposing to install an additional wall sign on the northeast elevation of the building for the proposed Baskin-Robbins facility. Said sign will contain approximately 26.5± sq.ft. and will increase the total sign area on the property to 122± sq.ft.

Section 20.06.050 D of the Municipal Code provides that the total area of all signs on the site shall not exceed 2 sq.ft. for each lineal foot of street frontage. The width of the subject property is 150 feet; therefore, a maximum of 300 sq.ft. of sign area is permitted on the subject property.

#### Specific Findings and Recommendation

Section 20.80.060 of the Newport Beach Municipal Code provides that in order to grant any use permit, the Planning Commission shall find that the establishment, maintenance or operation of the use or building applied for will not, under the circumstances of the particular case, be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood of such proposed use or be detrimental or injurious to property and improvements in the neighborhood or the general welfare of the City.

TO:

Planning Commission -4.

Staff recommends approval of Use Permit No. 1797 (Amended) and suggests that the Planning Commission take such action subject to the findings and conditions as set forth in Exhibit "A", attached.

PLANNING DEPARTMENT

JAMES D. HEWICKER, Director

By W. William Ward  
W. William Ward  
Senior Planner

WWW:1a

UPA9

Attachments: Exhibit "A"  
Vicinity Map  
Excerpt of the Planning Commission minutes dated  
July 15, 1976, February 10, 1983 and  
September 6, 1984  
Letter from Applicant's Architect  
Plot plan, floor plan and elevations

City of Newport Beach



EXHIBIT "A"  
FINDINGS AND CONDITIONS OF APPROVAL  
USE PERMIT NO. 1797 (AMENDED)  
JUNE 20, 1985

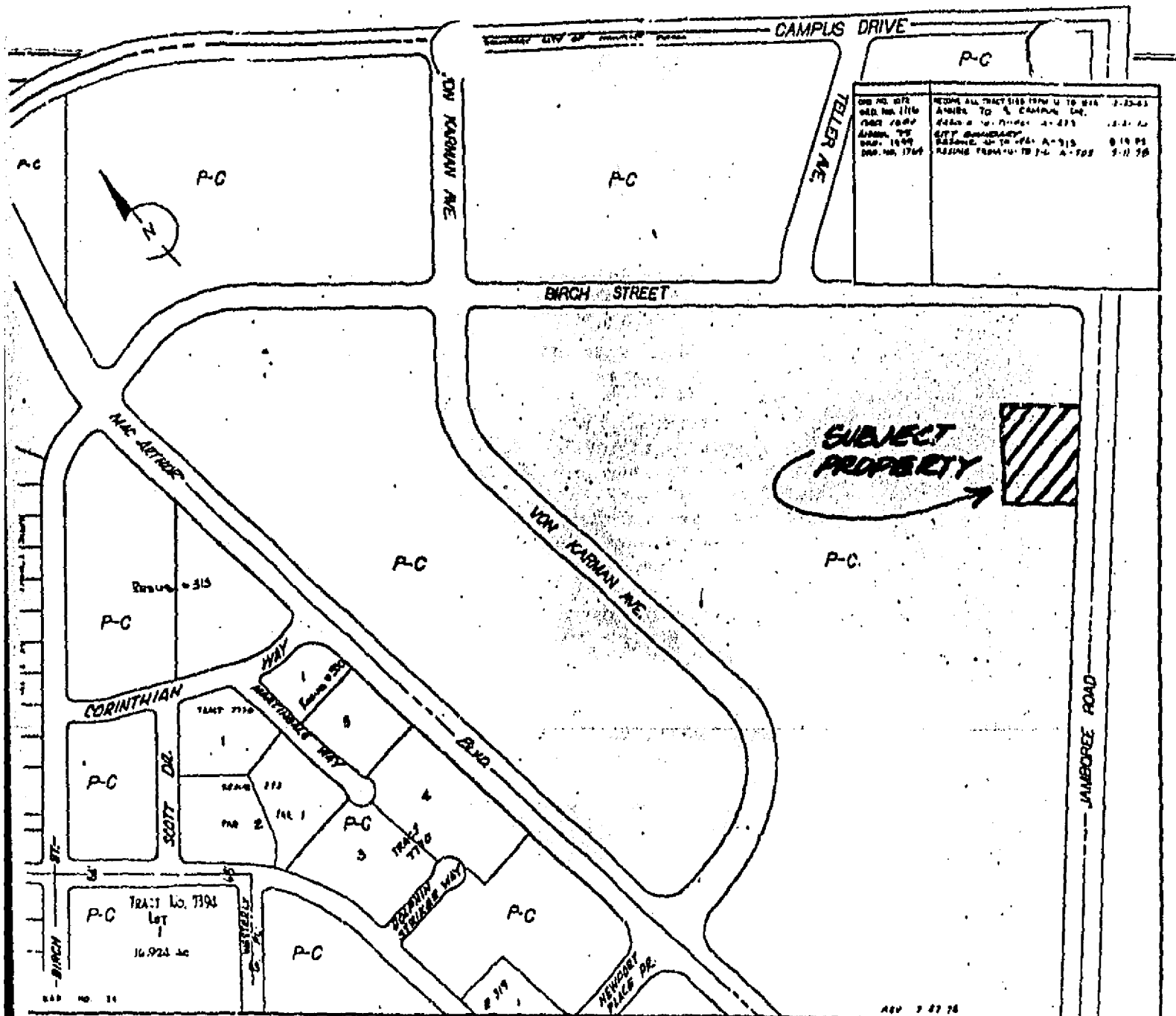
FINDINGS:

1. That the subject restaurant and proposed ice cream parlor is consistent with surrounding land uses.
2. The project will not have any significant environmental impact.
3. That the proposed change in the operational characteristics of the existing restaurant so as to include the proposed ice cream facility, will not increase the parking demand of the restaurant inasmuch as said expansion will occupy space previously used for outdoor eating purposes.
4. Adequate off-street parking spaces and traffic circulation are being provided for the proposed development.
5. The Police Department has indicated that they do not contemplate any problems.
6. The approval of Use Permit No. 1797 (Amended) will not, under the circumstances of this case be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing and working in the neighborhood or be detrimental or injurious to property and improvements in the neighborhood or the general welfare of the City.

CONDITIONS:

1. That development shall be in substantial conformance with the approved plot plan, floor plan and elevations.
2. That all applicable conditions of approval of the original Use Permit No. 1797 and subsequent amendments shall be fulfilled.

# VICINITY MAP



## MAP

NEWPORT — BEACH — CALIFORNIA

R-1	AGRICULTURAL RESIDENTIAL	R-6	MULTIPLE RESIDENTIAL
R-2	WHOLE FAMILY RESIDENTIAL	C-1	LIGHT COMMERCIAL
R-3	SUMMER RESIDENTIAL	C-2	GENERAL COMMERCIAL
R-4	RESTRICTED MULTIPLE FAMILY RESIDENTIAL	M-1	MANUFACTURING
D	CONDOMINIUM DISTRICT	U	UNCLASSIFIED

PLANNING COMMISSION

CHAIRMAN  
SPEAKER

QAG NO 929  
DATE 7-11-66

63

USE PERMIT NO. 1797 (AMENDED)

# City of Newport Beach

6

## COMMISSIONERS

## CITY OF NEWPORT BEACH

MINUTES

ROLL CALL

July 15, 1976

INDEX

Item #1

Request to construct a drive-through and take-out restaurant in the Koll Center Newport Planned Community.

USE PER  
NO. 179

Location: Lot 15, Tract 7953, located at 4501 Jamboree Road in Koll Center Newport.

APPROVE  
CONDI-  
TIONALL

Zone: P-C

Applicant: Colwell Ray Hornacek Okinaka, Inc., Tustin

Owner: Don Koll Company, Newport Beach

Staff presented the sign elevations to the Commission which were received subsequent to the distribution of the staff report.

Public hearing was opened in connection with this matter.

Tim Strader, Vice President of the Don Koll Company, owner of the property, appeared before the Commission and concurred with the staff report and recommended conditions.

Bob Hornacek, Architect, appeared before the Commission and reviewed the new prototype design of "Der Wienerschnitzel." He answered questions of the Commission relative to signs.

Assistant Community Development Director Hewicker reviewed the size of the various signs with the Commission as well as the location of each and advised that the proposed signs were well within the requirements allowed for drive-up/take-out restaurants. He also reviewed the various uses of the surrounding properties.

City Engineer Nolan advised that the driveway location had now been set to coincide with the original location as set forth on the tract map and therefore the recommended condition pertaining to access was obsolete.

Pat Allen of Landgon & Wilson, Architects, appeared before the Commission and confirmed the location of the driveway.

Planning Commission discussed direct driveway access to Jamboree Road and it was pointed out that additional street right-of-way was dedicated by the developer specifically for a right-turn lane which would allow for the merging of traffic.

There being no others desiring to appear and be heard, the public hearing was closed.

COMMISSIONERS

CITY OF NEWPORT BEACH

MINUTES

ROLL CALL

July 15, 1976

INDEX

Motion  
Ayes  
Absent

AGEE  
BALALIS  
FREDERICKSON  
HEATHER  
HUMMEL  
SEELY

Following discussion of the request, especially as to availability and required number of parking spaces, motion was made that Planning Commission make the following findings:

1. That the proposed development is consistent with the General Plan and the Koll Center Newport Development Standards and is compatible with surrounding land uses.
2. Adequate offstreet parking spaces and traffic circulation are being provided for the proposed development.
3. That the Police Department has indicated that they do not contemplate any problems.
4. That the waiver of the development standards as they pertain to parking, walls and landscaping, will not be detrimental to adjoining properties.
5. The approval of Use Permit No. 1797 will not, under the circumstances of this case, be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing and working in the neighborhood or be detrimental or injurious to property and improvements in the neighborhood or the general welfare of the City.

and approve Use Permit No. 1797, subject to the following conditions:

1. That development shall be in substantial conformance with the approved plot plan and elevations.
2. That all landscaping and lighting shall conform to Chapter 20.72 of the Newport Beach Municipal Code.
3. That the development standards related to walls, and a portion of the parking and signing requirements are waived.
4. That all signs shall be approved by the Director of Community Development. The proposed identification sign adjacent to Jamboree Road shall not exceed a height of 4 feet above grade.
5. That all mechanical equipment and trash areas shall be screened from adjacent properties as well as from Jamboree Road.
6. That a landscape plan shall be submitted to and approved by the Director of Parks, Beaches and Recreation. Screened landscaping shall be required along the interior side

8  
10

## CITY OF NEWPORT BEACH

## ROLL CALL

## INDEX

7. That all improvements be constructed as required by ordinance and the Public Works Department.

\*\*\*

R. V. Hogan, Ex-Officio Secretary  
City of Newport Beach  
Planning Commission

9

February 10, 1983

MINUTES

Allen  
Balais  
Goff  
King  
Kurlander  
McLaughlin  
Winburn

## City of Newport Beach

ROLL CALL

INDEX

Request to amend a previously approved use permit which allowed the establishment of a take-out food establishment (Der Wienerschnitzel) in the Koll Center Newport Planned Community. The proposed amendment is to allow the addition of an outdoor eating area to the existing restaurant and a request to waive a portion of the required off-street parking spaces for said expansion.

Item #6

## LOCATION:

Parcel 2 of Parcel Map No. 108-27 (Resubdivision No. 557) located at 4501 Jamboree Road, on the northwesterly side of Jamboree Road, between Birch Street and MacArthur Boulevard, in the Koll Center Newport Planned Community.

USE PERMIT  
NO. 1797  
(Amended)

## ZONE:

P-C

## APPLICANT:

Der Wienerschnitzel, Inc., Newport Beach

APPROVED  
CONDI-  
TIONALLY

## OWNER:

The Koll Company, Newport Beach

The public hearing opened in connection with this item and Mr. James Caulfield, representing the applicant, appeared before the Commission and requested approval of this item.

Motion  
All Ayes

X  
X X X X X X X

Motion was made for approval of Use Permit No. 1797 (Amended), subject to the following findings and conditions, which MOTION CARRIED:

## FINDINGS:

1. That the proposed development is consistent with the General Plan, and is compatible with surrounding land uses.
2. Adequate off-street parking spaces and traffic circulation are being provided for the proposed development.

Allen  
Balais  
Goff  
King  
Kurlander  
McLaughlin  
Winburn

## City of Newport Beach

ROLL CALL

INDEX

3. The Police Department has indicated that they do not contemplate any problems.
4. That the waiver of a portion of the parking requirements for the expanded take-out restaurant facility will not be detrimental to adjoining properties.
5. The approval of Use Permit No. 1797 (Amended) will not, under the circumstances of this case be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing and working in the neighborhood or be detrimental or injurious to property and improvements in the neighborhood or the general welfare of the City.

CONDITIONS

1. That development shall be in substantial conformance with the approved plot plan and floor plan.
2. That a portion of the required off-street parking spaces (i.e. 29 spaces) are waived.
3. That all applicable conditions of approval of the original Use Permit No. 1797 shall be maintained.

\* \* \*

Turner  
Gail  
Kuisel  
Eichner  
Person  
N-Sun

## City of Newport Beach

ROLL CALL

INDEX

~~Company addressed the Planning Commission stating that~~  
The Irvine Company agrees with the findings and conditions of Approval recommended in the Staff Report. The Planning Commission, Planning Staff, and the Irvine Company agreed that action on this item should be taken after the Commission recommends the approval of Development Agreement No. 2.

Motion  
All Ayes

x  
x x x x x x

Motion was, therefore, made to continue this item to the Planning Commission meeting of September 20, 1984, which MOTION CARRIED.

\* \* \*

Use Permit No. 1797 (Amended) (Public Hearing)

Request to change the operational characteristics of an existing take-out restaurant (Der Wienerschnitzel) so as to allow on-sale beer and wine in conjunction with the restaurant use.

LOCATION: Parcel No. 2 of Parcel Map 108-27 (Resubdivision No. 557), located at 4501 Jamboree Road, on the northwesterly side of Jamboree Road, between Birch Street and MacArthur Boulevard, in the Koll Center Newport Planned Community.

ZONE: P-C

APPLICANT: Der Wienerschnitzel International, Inc., Newport Beach

OWNERS: Thomas H. and Lynn K. Wooldridge, Templeton

The public hearing was opened in connection with this item, and Mr. John Harvey, Director of Development, Der Wienerschnitzel, appeared before the Planning Commission. Mr. Harvey agreed with the findings and conditions in the Staff Report with the exception that the restaurant will only be selling beer, and not wine. Mr. Laycock explained that beer and wine are under the same license from the Alcoholic Beverage Control Department.

Item #5

Use Permit  
#1797

Approved  
Condition-  
ally



Turner  
Goff  
Kurtlander  
Eichenhofer  
Person  
Winburn

## City of Newport Beach

## ROLL CALL

## INDEX

Motion  
All Ayes

x x x x

x

x

Motion was made for approval of Use Permit No. 1797 (Amended), subject to the findings and conditions of Exhibit "A", which MOTION CARRIED.

FINDINGS

1. That the subject restaurant is consistent with the General Plan, and is compatible with surrounding land uses.
2. The project will not have any significant environmental impact.
3. That the proposed change in the operational characteristics of the existing restaurant so as to include the service of beer and wine as an incidental use, will not increase the parking demand of the restaurant.
4. Adequate off-street parking spaces and traffic circulation are being provided for the proposed development.
5. The Police Department has indicated that they do not contemplate any problems.
6. The approval of Use Permit No. 1797 (Amended) will not, under the circumstances of this case be detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing and working in the neighborhood or be detrimental or injurious to property and improvements in the neighborhood or the general welfare of the City.

CONDITIONS

1. That development shall be in substantial conformance with the approved plot plan and floor plan.
2. That all applicable conditions of approval of the original Use Permit No. 1797 and 1797 (Amended) shall be maintained.
3. That the serving of beer and wine shall be incidental to the primary function of providing food service.
4. That no off-sale beer and wine shall be permitted in conjunction with the subject restaurant.

Turner  
Gott  
Kurlander  
Eichenhofer  
Person  
Winburn

## City of Newport Beach

ROLL CALL

INDEX

5. That the Planning Commission may add or modify conditions of approval to this use permit, or recommend to the City Council the revocation of this use permit, upon a determination that the operation which is the subject of this use permit, causes injury, or is detrimental to the health, safety, peace, morals, comfort, or general welfare of the community.

6. This use permit shall expire unless exercised within 24 months from the date of approval as specified in Section 20.80.090, A of the Newport Beach Municipal Code.

\* \* \*

Use Permit No. 3113 (Public Hearing)

Item #6.

Request to permit the establishment of a restaurant with on-sale alcoholic beverages on property located in the C-1-H District. The proposal also includes a modification to the Zoning Code so as to allow the use of tandem and compact parking spaces in conjunction with a valet parking service.

Use Permit  
# 3113

Continued  
to  
Sept. 20,  
1984

LOCATION: Parcel No. 1, Parcel Map 71-18  
(Resubdivision No. 478),  
located at 1000 West Coast Highway, on  
the northerly side of West Coast  
Highway, across from the Bayshores  
Residential Community.

ZONE: C-1-H

APPLICANT: Jack Chen, Los Angeles

OWNER: Dr. Edward Deeb, Toluca Lake

\* \* \*

Motion was made to continue this item to the Planning Commission meeting of September 20, 1984, which MOTION CARRIED.

\* \* \*

CITY OF NEWPORT BEACH  
PLANNING DEPARTMENT  
PLAN REVIEW REQUEST

Date May 30, 1985

☒ ADVANCE PLANNING DIVISION ✓  
☒ PUBLIC WORKS DEPARTMENT ✓  
☒ TRAFFIC ENGINEER ✓  
☒ FIRE DEPARTMENT ✓  
☒ PLAN REVIEW DIVISION ✓  
☐ PARKS & RECREATION  
☒ POLICE DEPARTMENT ✓  
☐ MARINE SAFETY  
☐ GRADING

☒ PLANS ATTACHED (PLEASE RETURN)

☐ PLANS ON FILE IN PLANNING DEPT.

APPLICATION OF Der Wienerschnitzel International, Inc.

FOR: USE PERMIT No. 1797 (Amended)

REQUEST TO: Amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

LOCATION: 4501 Jamboree Road

REPORT REQUESTED BY: June 10, 1985

COMMISSION REVIEW: June 20, 1985

COMMENTS: \_\_\_\_\_  
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Signature \_\_\_\_\_

Date \_\_\_\_\_

City of Newport Beach

CITY OF NEWPORT BEACH  
PLANNING DEPARTMENT  
PLAN REVIEW REQUEST

Date May 30, 1985

☒ ADVANCE PLANNING DIVISION  
☒ PUBLIC WORKS DEPARTMENT  
☒ TRAFFIC ENGINEER  
☒ FIRE DEPARTMENT  
☒ PLAN REVIEW DIVISION  
☐ PARKS & RECREATION  
☒ POLICE DEPARTMENT  
☐ MARINE SAFETY  
☐ GRADING

☒ PLANS ATTACHED (PLEASE RETURN)

☐ PLANS ON FILE IN PLANNING DEPT.

APPLICATION OF Der Wienerschnitzel International, Inc.

FOR: USE PERMIT No. 1797 (Amended)

REQUEST TO: Amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

LOCATION: 4501 Jamboree Road

REPORT REQUESTED BY: June 10, 1985

COMMISSION REVIEW: June 20, 1985

COMMENTS: None  
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Signature

Richard M. Edmonston

Date

6-6-85

City of Newport Beach

CITY OF NEWPORT BEACH  
PLANNING DEPARTMENT  
PLAN REVIEW REQUEST

Date May 30, 1985

☒ ADVANCE PLANNING DIVISION  
☒ PUBLIC WORKS DEPARTMENT  
☒ TRAFFIC ENGINEER  
☒ FIRE DEPARTMENT  
☒ PLAN REVIEW DIVISION  
☐ PARKS & RECREATION  
☒ POLICE DEPARTMENT  
☐ MARINE SAFETY  
☐ GRADING

☒ PLANS ATTACHED (PLEASE RETURN)

☐ PLANS ON FILE IN PLANNING DEPT.

APPLICATION OF Der Wienerschnitzel International, Inc.

FOR: USE PERMIT No. 1797 (Amended)

REQUEST TO: Amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

LOCATION: 4501 Jamboree Road

REPORT REQUESTED BY: June 10, 1985

COMMISSION REVIEW: June 20, 1985

COMMENTS: 1. HEALTH DEPT. APPROVAL IS REQUIRED  
2. Comply with The Uniform BLDG Code AND  
all local AMENDMENTS.  
3. TOTAL ENCLOSED AREA NOT TO EXCEED  
6000 SQ. FT. (IN construction, A3 occupancy).

Signature

*Fayal Jundi*

Date 6/10/85

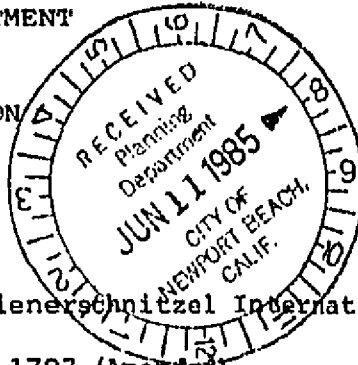
CITY OF NEWPORT BEACH  
PLANNING DEPARTMENT  
PLAN REVIEW REQUEST

Date May 30, 1985

X ADVANCE PLANNING DIVISION  
X PUBLIC WORKS DEPARTMENT  
X TRAFFIC ENGINEER  
X FIRE DEPARTMENT  
X PLAN REVIEW DIVISION  
PARKS & RECREATION  
X POLICE DEPARTMENT  
MARINE SAFETY  
GRADING

X PLANS ATTACHED (PLEASE RETURN)

PLANS ON FILE IN PLANNING DEPT.



APPLICATION OF Der Wienerschnitzel International, Inc.

FOR: USE PERMIT No. 1797 (Amended)

REQUEST TO: Amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

LOCATION: 4501 Jamboree Road

REPORT REQUESTED BY: June 10, 1985

COMMISSION REVIEW: June 20, 1985

COMMENTS: NONE

Signature

Date

6/10

CITY OF NEWPORT BEACH  
PLANNING DEPARTMENT  
PLAN REVIEW REQUEST

Date May 30, 1985

☒ ADVANCE PLANNING DIVISION  
☒ PUBLIC WORKS DEPARTMENT  
☒ TRAFFIC ENGINEER  
☒ FIRE DEPARTMENT  
☒ PLAN REVIEW DIVISION  
☐ PARKS & RECREATION  
☒ POLICE DEPARTMENT  
☐ MARINE SAFETY  
☐ GRADING

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☐ PLANS ON FILE IN PLANNING DEPT.

APPLICATION OF Der Wienerschnitzel International, Inc.

FOR: USE PERMIT No. 1797 (Amended)

REQUEST TO: Amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

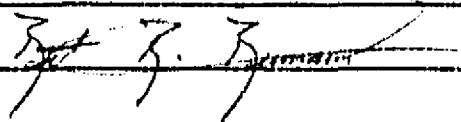
LOCATION: 4501 Jamboree Road

REPORT REQUESTED BY: June 10, 1985

COMMISSION REVIEW: June 20, 1985

COMMENTS: THE PARCEL IS LOCATED WITHIN AN AREA DESIGNATED FOR A MIXED USE  
OF "RETAIL AND SERVICE COMMERCIAL" AND "ADMINISTRATIVE PROFESSIONAL AND FINANCIAL COMMERCIAL" BY THE LAND USE ELEMENT AND PLAN OF THE CITY'S GENERAL  
PLAN. THE PROPOSED EXTENSION OF USES IS CONSISTENT WITH THOSE INTENDED FOR  
EITHER CATEGORY. THE PARCEL IS ALSO CONFORMANT WITH THE PLANNED COMMUNITY DE-  
VELOPMENT STANDARDS FOR 'MILL CREEK NEWPORT'. NO CONFLICT.

Signature



Date 4 JUNE 1985

CITY OF NEWPORT BEACH  
PLANNING DEPARTMENT  
PLAN REVIEW REQUEST

Date May 30, 1985

☒ ADVANCE PLANNING DIVISION  
☒ PUBLIC WORKS DEPARTMENT  
☒ TRAFFIC ENGINEER  
☒ FIRE DEPARTMENT  
☒ PLAN REVIEW DIVISION  
☐ PARKS & RECREATION  
☒ POLICE DEPARTMENT  
☐ MARINE SAFETY  
☐ GRADING

☒ PLANS ATTACHED (PLEASE RETURN)

☐ PLANS ON FILE IN PLANNING DEPT.

APPLICATION OF Der Wienerschnitzel International, Inc.

FOR: USE PERMIT No. 1797 (Amended)

REQUEST TO: Amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

LOCATION: 4501 Jamboree Road

REPORT REQUESTED BY: June 10, 1985

COMMISSION REVIEW: June 20, 1985

COMMENTS: \_\_\_\_\_

1. Construction shall meet the requirements of the U.B.C. \_\_\_\_\_

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\_\_\_\_\_

Signature

*J. Atkinson*

Date 6.5.85



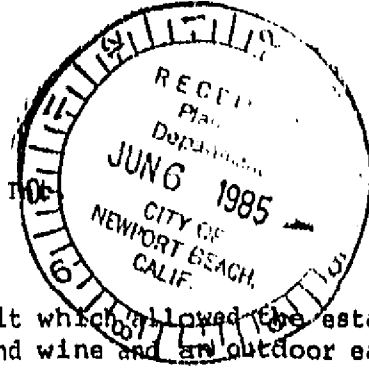
CITY OF NEWPORT BEACH  
PLANNING DEPARTMENT  
PLAN REVIEW REQUEST

Date May 30, 1985

X ADVANCE PLANNING DIVISION  
X PUBLIC WORKS DEPARTMENT  
X TRAFFIC ENGINEER  
X FIRE DEPARTMENT  
X PLAN REVIEW DIVISION  
PARKS & RECREATION  
X POLICE DEPARTMENT  
MARINE SAFETY  
GRADING

X PLANS ATTACHED (PLEASE RETURN)

    PLANS ON FILE IN PLANNING DEPT.



APPLICATION OF Der Wienerschnitzel International, Inc.

FOR: USE PERMIT No. 1797 (Amended)

REQUEST TO: Amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

LOCATION: 4501 Jamboree Road

REPORT REQUESTED BY: June 10, 1985

COMMISSION REVIEW: June 20, 1985

COMMENTS: No Anticipated Police Problems

\_\_\_\_\_  
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\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Signature A. Vella Lt.

Date 6-5-85

May 24, 1985

C · R · H · O

Architecture - Interiors - Planning

560 West First Street

Post Office Box 1066

Tustin, California 92681-1066

714-832-1834

City of Newport Beach  
Planning Department  
3300 Newport Blvd.  
Newport Beach, CA 92658

Re: USE PERMIT APPLICATION  
for remodel of existing Wienerschnitzel Restaurant  
4501 Jamboree Rd., Newport Beach, CA

Der Wienerschnitzel International Inc. has decided to add an ice cream parlour to their existing Wienerschnitzel Restaurant in Newport Beach. Wienerschnitzel will operate both the fast food restaurant and the ice cream parlour.

The number of employees for the restaurant and the ice cream parlour varies from 3 during the slow periods to 8 during the peak hours. The hours of operations are 7 a.m to 10 p.m..

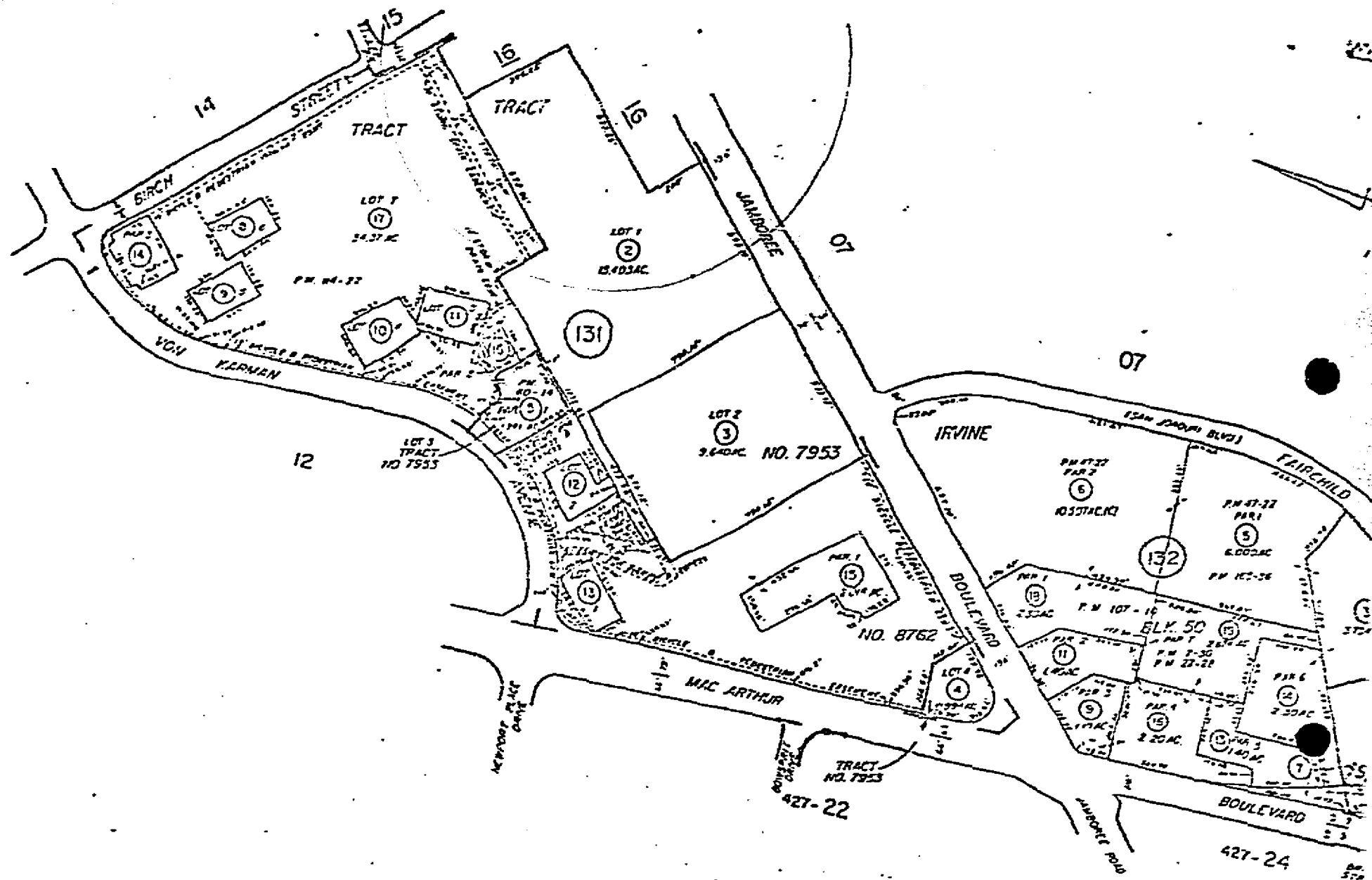
The existing Wienerschnitzel Restaurant has 31 parking spaces for 78 indoor seats and 74 outdoor seats. The addition of the ice cream parlour will reduce the number of outdoor seats by 32 to a total of 42 outdoor seats. It will add 4 indoor seats. the number of parking spaces will remain the same.

By adding ice cream to their menu of hamburgers and hot dogs Wienerschnitzel hopes to satisfy the sweet tooth of their customers and to add some variety to their menu.

Sincerely,



Eva M. Sloan  
C.R.H.O., Inc. Architects



MARCH 1973

IRVINE SUB.  
TRACT NO. 7953  
TRACT NO. 8762  
PARCEL MAPS  
PARCEL MAPS

M.M. 1-08  
M.M. 310-7 TO 11 INC.  
M.M. 350-24, 25, 26  
P.M. 47-22, 60-14, 107-10  
P.M. 114-22

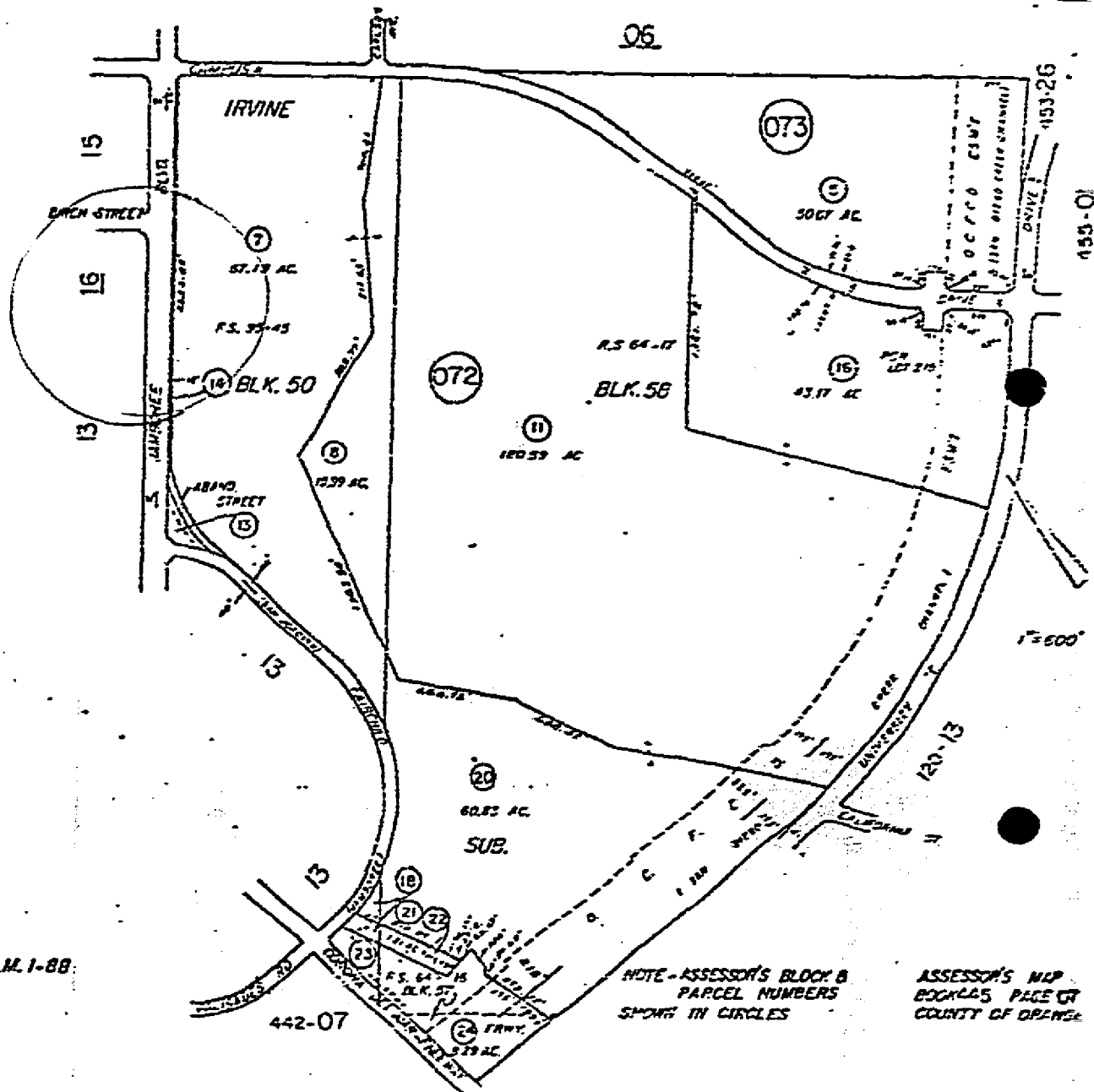
NOTE-ASSESSOR'S BLOCK &  
PARCEL NUMBERS  
SHOWN IN CIRCLES

ASSESSOR'S MAP  
BOOK 245 PAGE 13  
COUNTY OF ORANGE

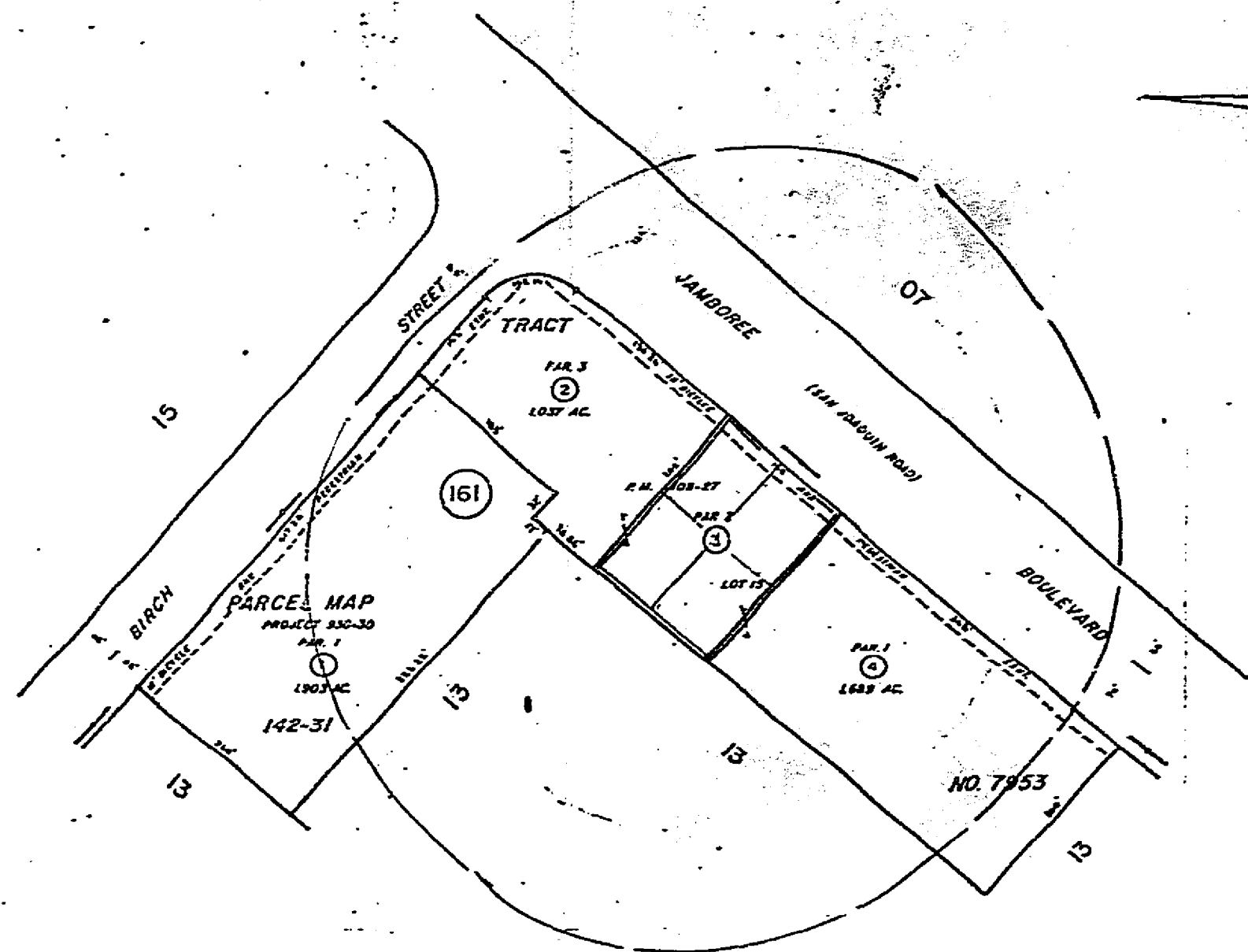
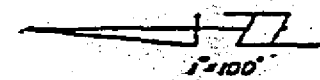
City of Newport Beach

MARCH 1966

IRVINE SUB. M.R.M. 1-88



City of Newport Beach



MARCH 1981

TRACT NO. 7953  
PARCEL MAP

LLA 310-77011 INC.  
P.M. 108-27, 142-31

NOTE - ASSESSOR'S BLOCK &  
PARCEL NUMBERS  
SHOWN IN CIRCLES

ASSESSOR'S MAP  
BOOK 445 PAGE 16  
COUNTY OF ORANGE



REGENTS OF THE UNIVERSITY  
Hall Room 689  
2200 University  
Berkeley, CA 94704  
AP No. 445-072-077

ROCKWELL INTER CORP.  
1700 E. Imperial  
El Segundo, CA 90245  
AP No. 445-131-02

KOLL CENTER NEWPORT  
NUMBER ELEVEN  
4490 Von Karman Avenue  
Newport Beach, CA 92660  
AP No. 445-161-02

WOOLDRIDGE, THOMAS H.  
4 Santa Rita Ranch Road  
Templeton, CA 93465

AP No. 445-161-03

JAMBOREE PLAZA  
433 N. Camden Drive  
Beverly Hills, CA 90210

AP No. 445-161-04

NEWPORT #9  
1001 E. La Habra Boulevard  
La Habra, CA 90631

AP No. 930-30-401

JAMBOREE LTD.  
5140 Birch Street  
Newport Beach, CA 92660

AP No. 930-30-402

DARTS BUILDING PARTNERS  
Attn: Steve Phair  
5120 Birch St., Suite 200  
Newport Beach, CA 92660  
AP No. 930-30-403

CP ASSOCIATES  
5100 Birch Street  
Newport Beach, CA 92660

AP No. 930-30-404

Der. Wienerschmigel Gnt.  
P.O. Box 7460  
Newport Beach, CA 92658-7460

NOTICE OF PUBLIC HEARING

Notice is hereby given that the Planning Commission of the City of Newport Beach will hold a public hearing on the application of Der Wienerschnitzel International, Inc. for Use Permit No. 1797 (Amended) on property located at 4501 Jamboree Road

Request to amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

This project has been reviewed, and it has been determined that it is categorically exempt under the requirements of the California Environmental Qualities Act.

Notice is hereby further given that said public hearing will be held on the 20th day of June 1985, at the hour of 7:30 p.m. in the Council Chambers of the Newport Beach City Hall, 3300 Newport Boulevard, Newport Beach, California, at which time and place any and all persons interested may appear and be heard thereon. For information call (714) 644-3200.

John Kurlander, Secretary, Planning Commission, City of Newport Beach

NOTE: The expense of this notice is paid from a filing fee collected from the applicant.

NOTICE OF PUBLIC HEARING

Notice is hereby given that the Planning Commission of the City of Newport Beach will hold a public hearing on the application of Der Wienerschnitzel International, Inc. for Use Permit No. 1797 (Amended) on property located at 4501 Jamboree Road

Request to amend a previously approved use permit which allowed the establishment of a take-out restaurant with on-sale beer and wine and an outdoor eating area. The proposed amendment includes a request to enclose a portion of the existing outdoor eating area so as to establish an ice cream sales area within the subject restaurant.

This project has been reviewed, and it has been determined that it is categorically exempt under the requirements of the California Environmental Qualities Act.

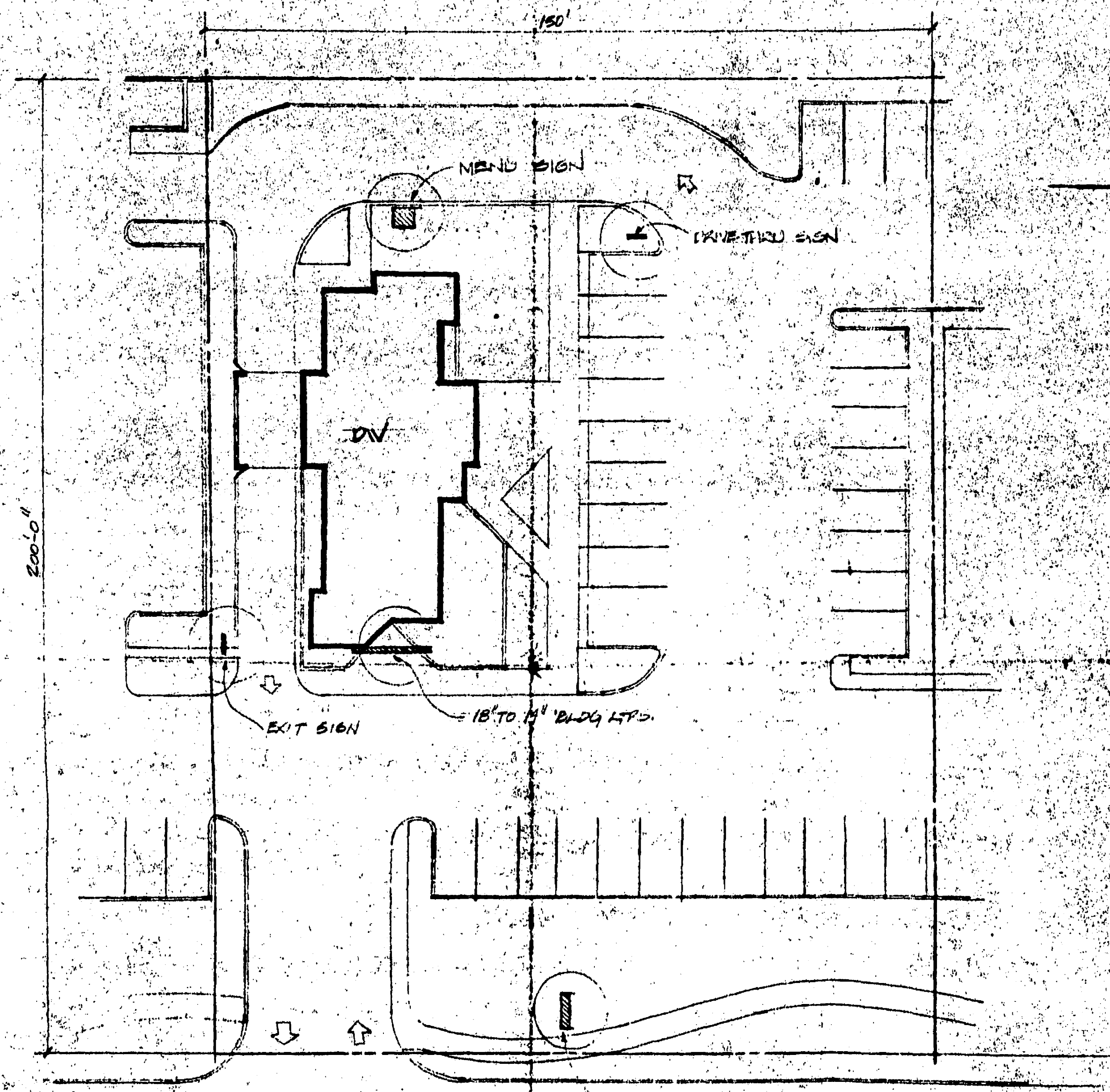
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John Kurlander, Secretary, Planning Commission, City of Newport Beach

NOTE: The expense of this notice is paid from a filing fee collected from the applicant.

City of Newport Beach



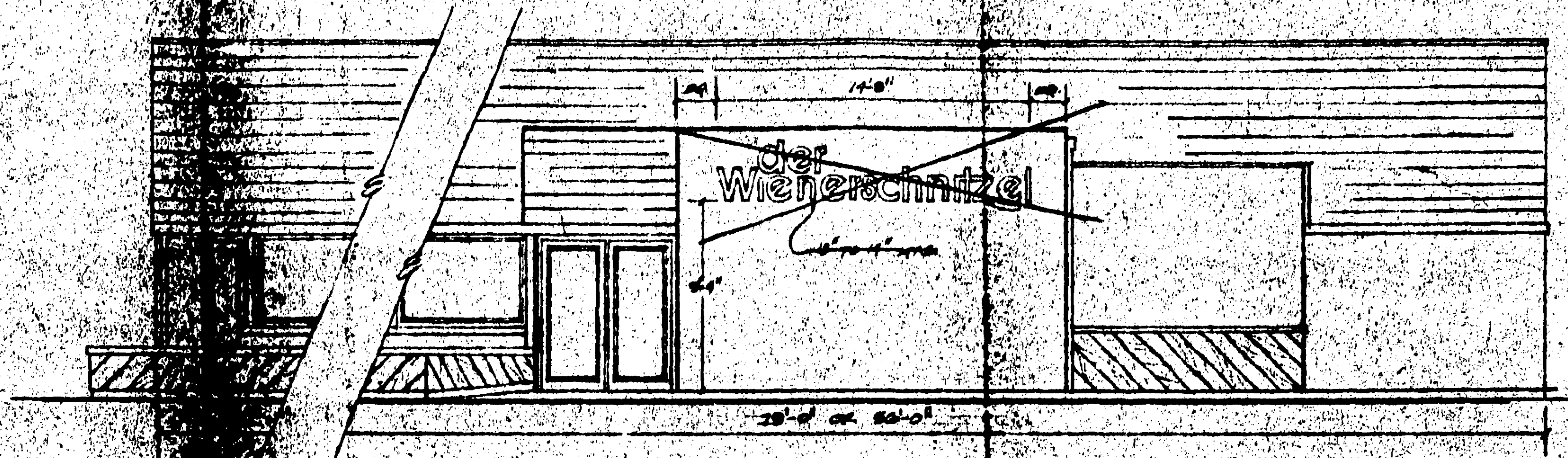
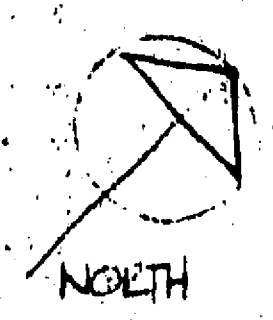


JAMES CREE RD.

IF MONUMENT SIGN  
REFER TO DWG # 14133

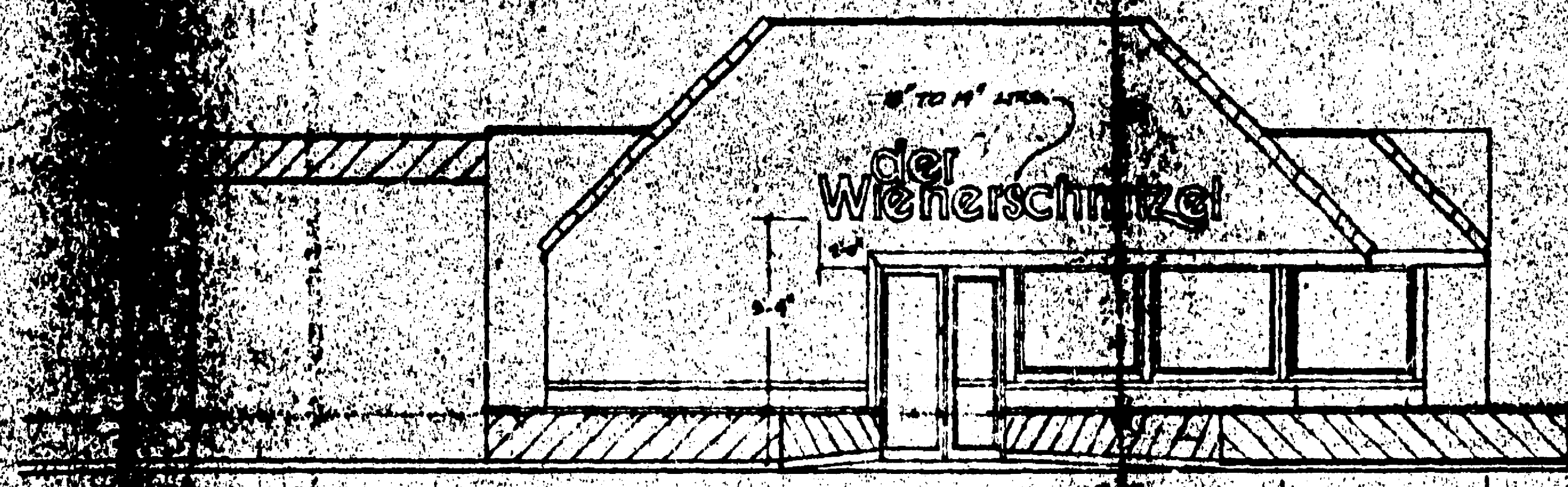
NOT PLAN

SCALE: 1/8" = 1'-0"



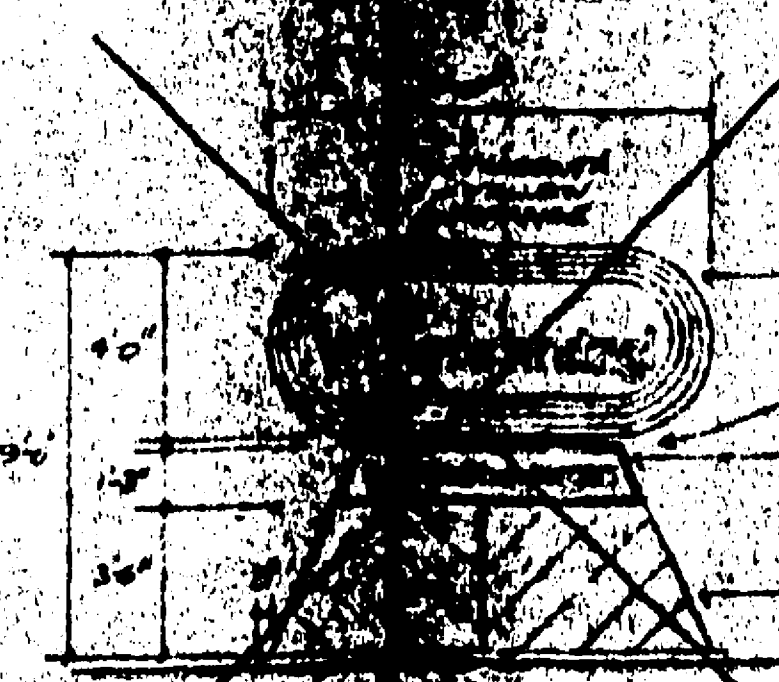
SIDE ELEVATION

SCALE: 1/4" = 1'-0"



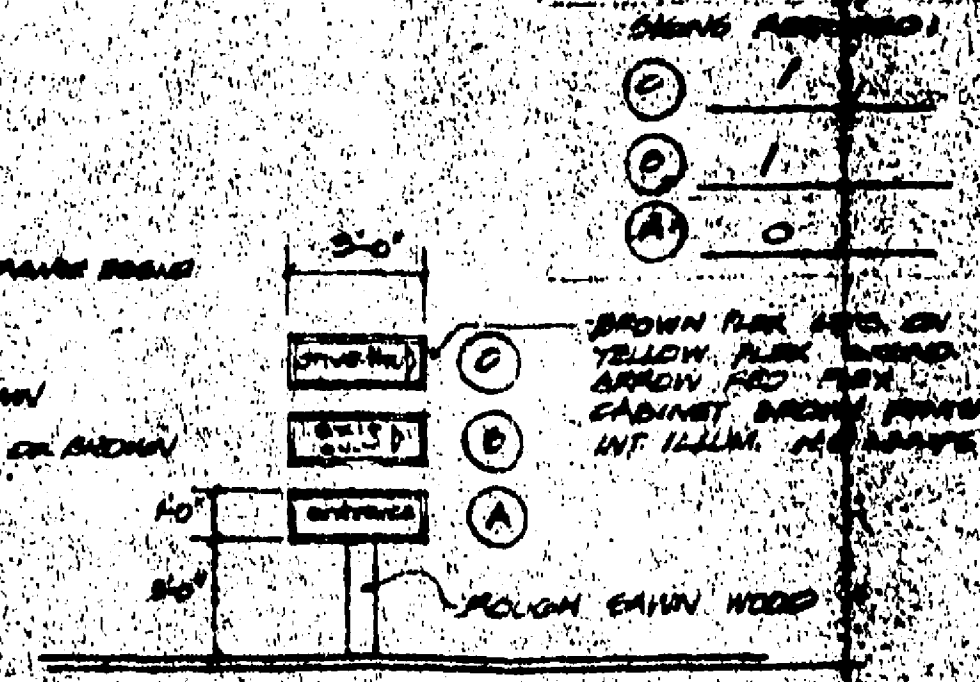
FRONT ELEVATION

SCALE: 1/4" = 1'-0"



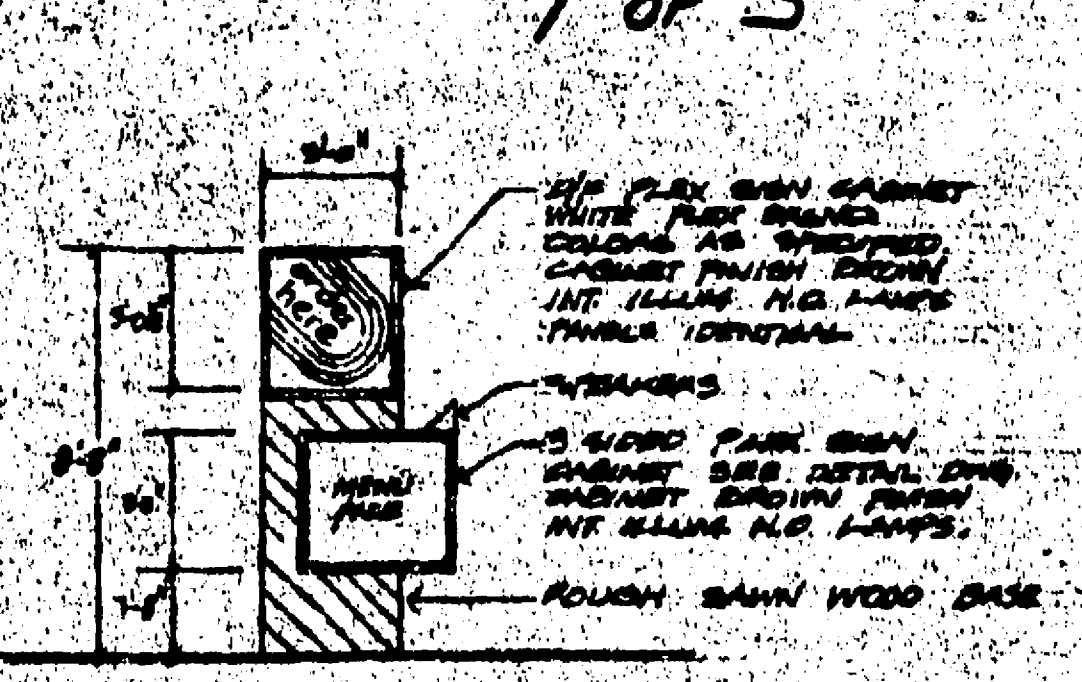
MONUMENT SIGN - ILLUM

SCALE: 1/4" = 1'-0"



DOUBLE FACE DIRECTIONAL SIGN - ILLUM

SCALE: 1/4" = 1'-0"

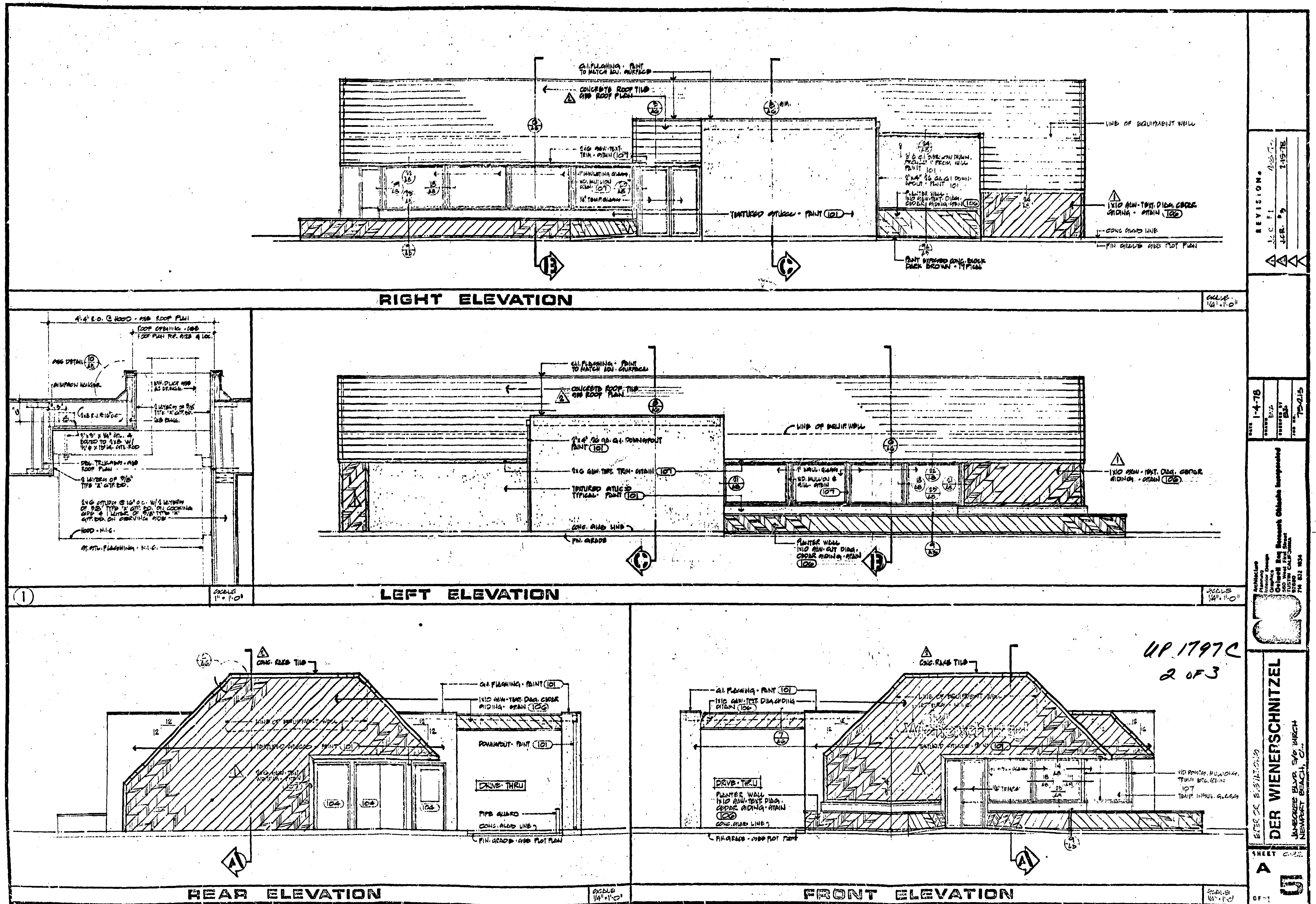


SPEAKER MENU BOARD SIGN - ILLUM

SCALE: 1/4" = 1'-0"

UP 1797 e  
1 OF 3





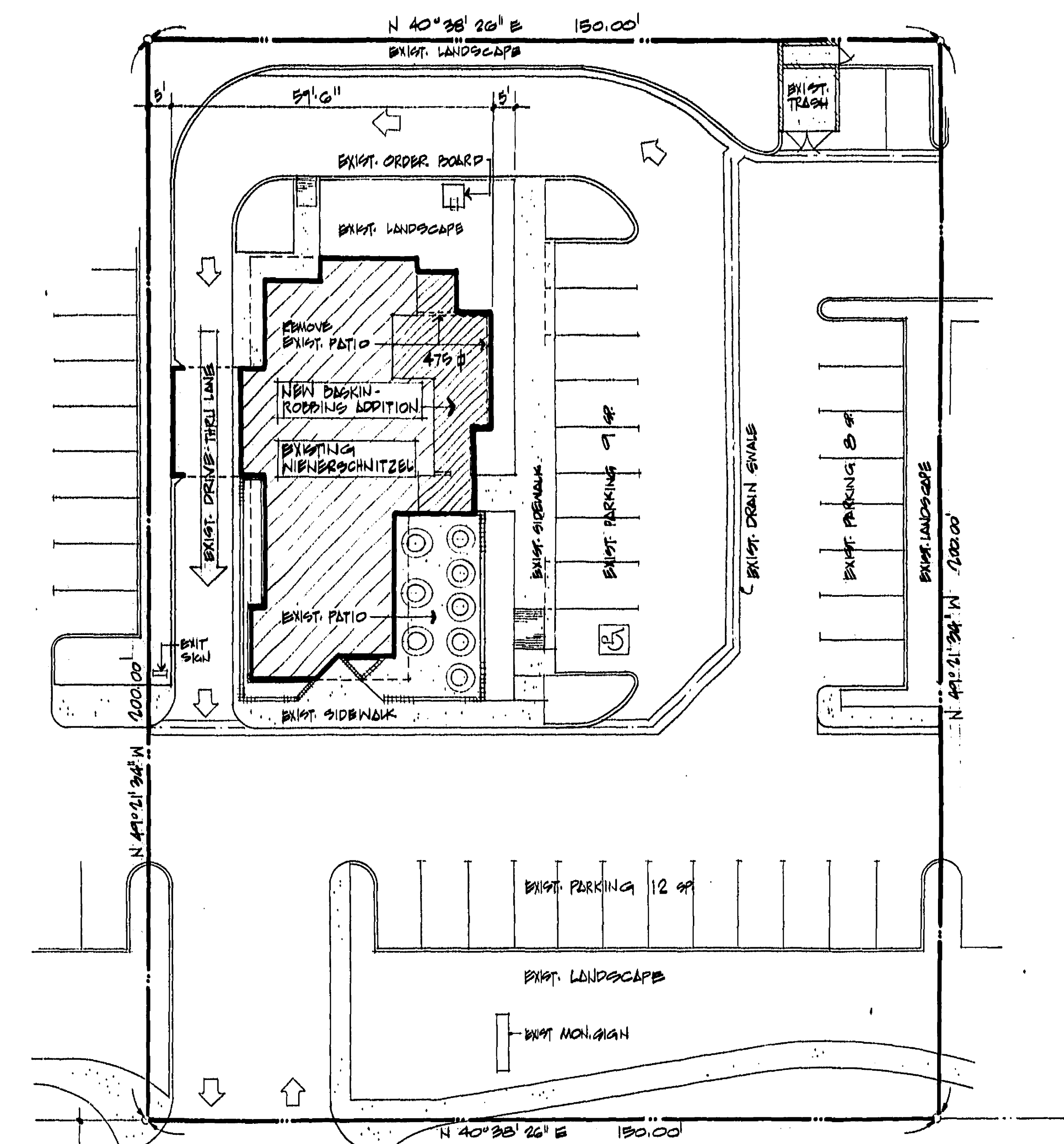
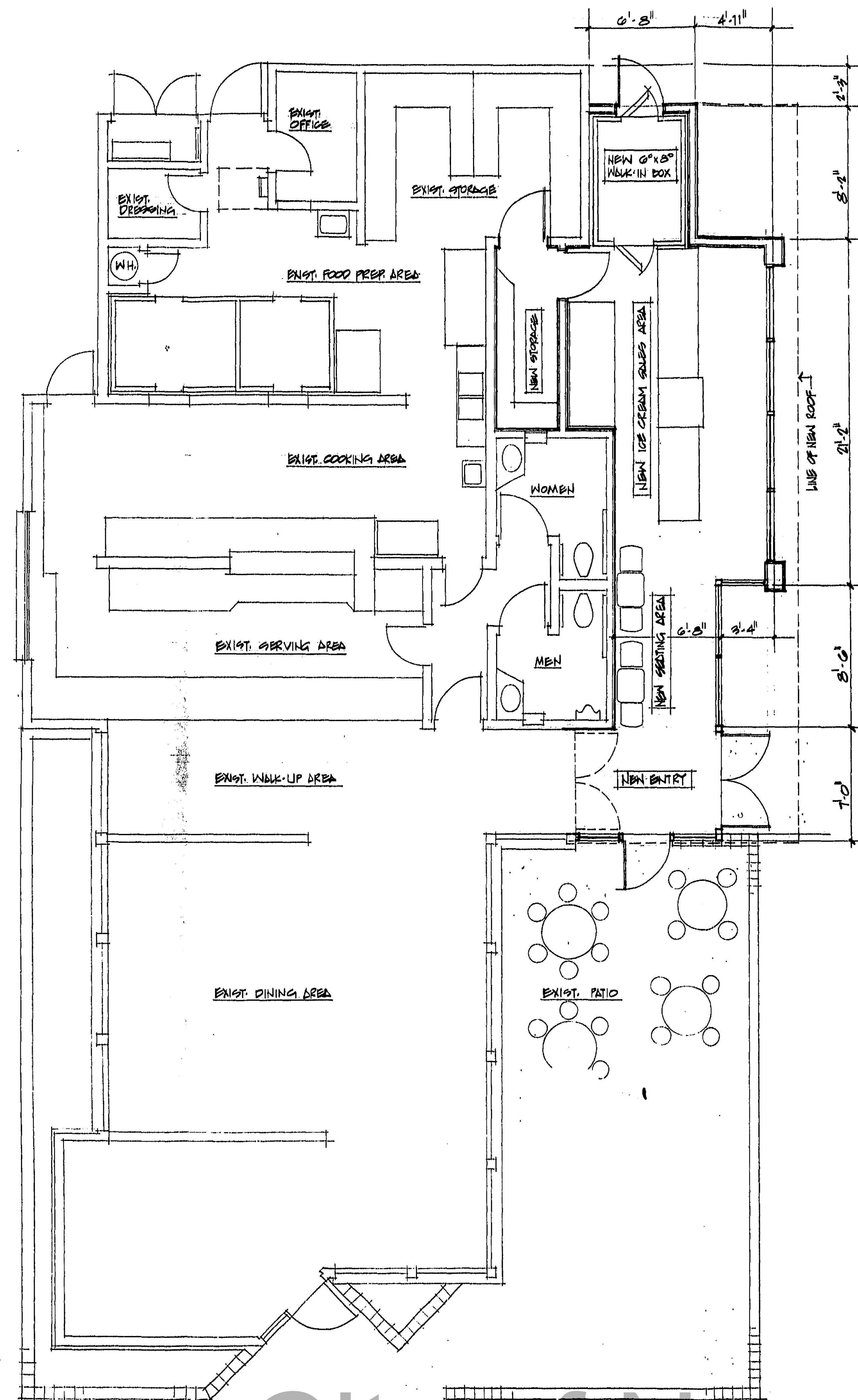
WIENERSCHNITZEL  
4501 JAMBOREE ROAD  
NEWPORT BEACH, CA

City of Newport Beach









UP 1797 D  
1 OF 2

**JAMBOREE BLVD**

## FLOOR PLAN

SCALE :  $\frac{1}{4}" = 1'-0"$

## PLOT PLAN

SCALE:  $\frac{1}{16}'' = 1'-0''$

## REVIEWS

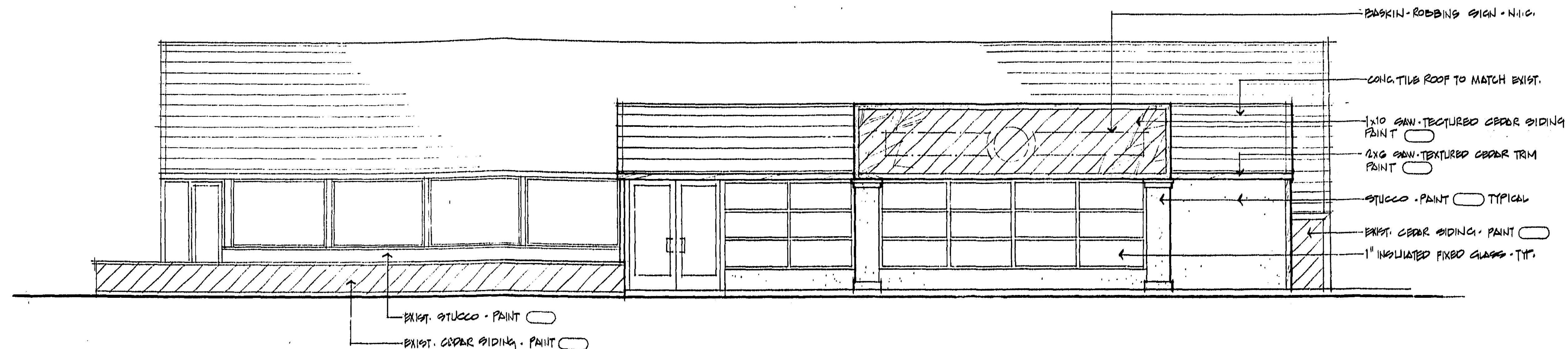
OWNER:  
WIENERSCHNITZEL INT. INC.  
4440 VON KARMAN AVE  
NEWPORT BEACH, CA.  
PH: 714 - 752-5800

DATE 11-21-88  
DRAWN  
EVA GLOAN  
CHECKED BY  
JOB NO.  
9999 001-174

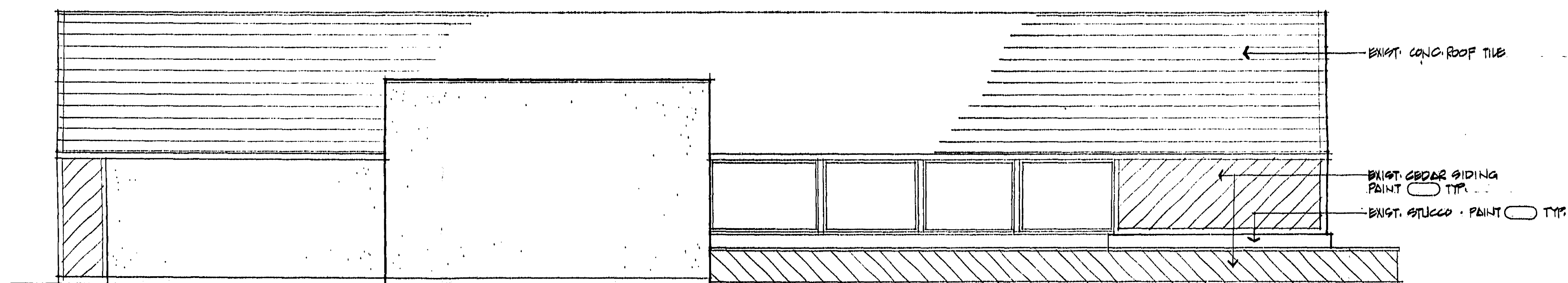
**C · R · H · O**  
*Architecture · Interiors · Planning*  
Cottrell Ray Hornsack Ottensmeyer Incorporated  
680 W. First St. P.O. Box 1098 Tustin, CA 92680  
714 832 1834

**#390**  
**PLOT & FLOOR PLAN**  
**WIENERSCHNITZEL**  
**REMODEL**  
**4501 JAMBOREE ROAD**  
**NEWPORT BEACH, CA**

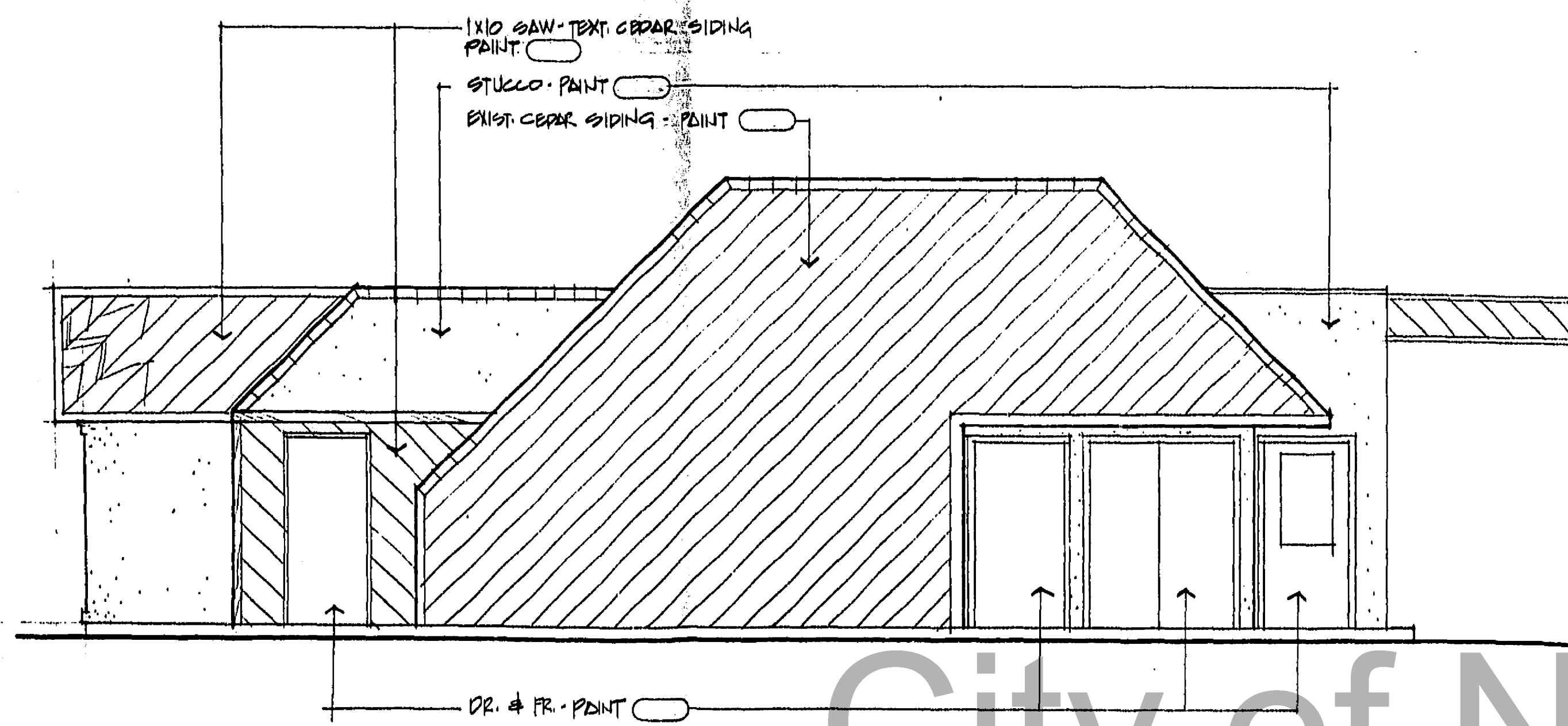
SHEET  
1  
OF 2



**NORTH-EAST ELEVATION**



**SOUTH-WEST ELEVATION**



**NORTH-WEST ELEVATION**



**SOUTH-EAST ELEVATION**

REVISIONS

OWNER:  
WIENERSCHNITZEL INC.  
4440 VON KARMAN AVE.  
NEWPORT BEACH, CA.  
PH: 714-752-5800

DATE: 8-21-85  
DESIGNER: B.A. MASON  
CHECKED BY:  
DATE: 10-30-01 174

C · R · H · O  
Architecture · Interiors · Planning  
Cohasset Bay, Norwalk, Connecticut  
550 W. First St., P.O. Box 1008, Tulsa, OK 74101  
714 832 1834

EXTERIOR ELEVATIONS **#390**  
**WIENERSCHNITZEL**  
REMODEL  
4501 JAMBOREE ROAD  
NEWPORT BEACH, CA.

SHEET

2

OF 2

# **Attachment No. CD 3**

Drive-Thru Queuing, Site Access, and Parking Evaluation



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Suite 250  
Irvine, CA 92614  
**949.825.6175** T  
949.825.5939 F  
www.llgengineers.com

Pasadena  
Irvine  
San Diego

March 27, 2024

Mr. Kareem Ali  
Starbucks  
555 Anton Boulevard, Suite 300  
Costa Mesa, CA 92626

LLG Reference: 2.22.4625.1

Subject: **Drive-Thru Queuing Evaluation for  
Starbucks at Birch & Jamboree**  
Newport Beach, California

Dear Mr. Ali:

Linscott, Law & Greenspan, Engineers (LLG) is pleased to present the findings of a drive-thru queuing evaluation for the proposed conversion of an existing Weinerschnitzel restaurant to a Starbucks at 4501 Jamboree Road, Newport Beach. This updates our prior letter report dated February 16, 2024 to address City staff comments.

The Project site located at the southwest corner of the Jamboree Road and Birch Street intersection. The existing building currently operated by Weinerschnitzel is 2,668 square feet (SF) in size, which will be reduced to approximately 2,117 SF (a reduction of approximately 551 SF) for the proposed Starbucks. The existing 1,242-SF Starbucks (with 10 interior and 10 outdoor dining seats) located at 4551 Jamboree Road and north of the Weinerschnitzel building will not be maintained for future operation by Starbucks.

Based on the findings of our analysis, the proposed design and configuration of the drive-thru facility will be adequate in serving drive-thru queues generated by the Project.

The following pages describe our study methodology and findings.

## PROJECT DESCRIPTION

**Figure 1** presents a vicinity map for the Project. **Figure 2** illustrates the proposed site plan and drive-thru design.

The Project consists of 2,117 SF of gross floor area with a 25-seat interior café and approximately 322 SF with 16 seats of patio/outdoor seating (the existing

David S. Shender, PE  
John A. Boarman, PE  
Richard E. Barretto, PE  
Keil D. Maberry, PE  
KC Yellapu, PE  
Dave Roseman, PE  
Shankar Ramakrishnan, PE



patio/outdoor seating is approximately 515 SF with 36 seats). Vehicular access is provided via a two-way (entry and exit) driveway located off southbound Jamboree Road. The existing drive-thru lane could accommodate 9 vehicles. The parking supply of 30 spaces (the same number of spaces as the existing Weinerschnitzel) will be provided, which includes 2 MOP (Mobile Order Pay) parking spaces that are time-restricted (through signage) for 5 minutes for customers that have ordered ahead using the MOP App to park, pick-up their order, and leave the site.

As shown on *Figure 2*, approximately 9 vehicles can be accommodated within the drive-thru lane, consisting of 3 vehicles before reaching the order/menu board, and 6 vehicles between the order/menu board and the pick-up window.

A “Do Not Enter” sign per CA MUTCD (sign code R5-1) will be installed at the exit of the drive-thru lane. This sign will be facing inbound traffic from Jamboree Road and drive aisle intersecting the drive-thru exit.

## DRIVE-THRU QUEUING ANALYSIS

Attached at the end of this letter report is LLG’s prior queuing and site access study dated January 27, 2022 that was prepared for the Starbucks at Bristol & Birch, and approved by the City of Newport Beach. As part of that study, and to estimate drive-thru queue storage length requirements for Starbucks, queue observations were conducted at the existing Starbucks located at:

- **Harbor & 91 Fwy Store# 23316 (1224 N. Harbor Boulevard, Anaheim)**

The proposed Starbucks at Birch & Jamboree would be similar to the existing Starbucks Anaheim in terms of size (Project: 2,400 SF, Anaheim: 2,100 SF), total number of employees (Project: 30, Anaheim: 35), and hours of operation (Project: 4:30am to 12:00am, Anaheim 4:30am to 11:00pm), and would provide a greater number of staff during the peak work shift (Project: 8 to 9 employees, Anaheim: 6 to 8 employees), and more parking spaces (Project: 30, Anaheim 22).

*Table 1* presents the drive-thru queue observation results for each of the three survey dates at the existing Starbucks Anaheim, indicating the number of vehicles in queue, and the number of occurrences each queue length was observed. As *Table 1* indicates, the average queue observed was 6 vehicles, the 85<sup>th</sup> percentile queue was 10 vehicles, the 95<sup>th</sup> percentile queue was 11 vehicles, and the 100<sup>th</sup> percentile or maximum queue was 14 vehicles. The site plan for the Anaheim café shows that approximately 8 vehicles can be accommodated within the drive-thru facility. When the queue exceeded this queue storage length, the vehicles were observed to use the drive aisle, but the queue dissipated quickly without any special/peak period management measures being implemented.

According to Starbucks, drive-thru transactions that were generated during the surveys are comparable to pre-COVID conditions, and likely to be greater than before the COVID pandemic since more customers are now choosing to use the drive-thru instead of going inside the café.

The observed queue lengths at the existing Starbucks Anaheim are anticipated to be greater than what the proposed Starbucks at Birch & Jamboree could generate. This is because the proposed Starbucks at Birch & Jamboree is projected to have approximately 25% less sales than the existing Starbucks Anaheim based on the fact that there are more Starbucks locations that exist near the Project (see the attached map at the end of this letter), and customers will clearly have more options to choose from, thereby reducing the demand on the Project's drive-thru facility by the estimated 25%.

In addition, the proposed Project will primarily be serving commuters (who do not linger like in a sit-down café without a drive-thru) and more local (and directional) traffic along the Jamboree Road corridor. Based on the characteristics of the proposed project site, it is not envisioned that patrons will linger within the café given the more inviting options presented in other nearby locations.

It should further be noted that the proposed Starbucks at Birch & Jamboree will have newer equipment that operate quicker, and the initial order point located one to two vehicles further than the existing Starbucks Anaheim. This is expected to result in greater efficiency of the Project's drive-thru operations, allowing the Starbucks team to receive the customer's order sooner and have it be ready when they arrive at the window, which in turn, will help facilitate processing orders and serve drive-thru customers more expediently, and move vehicles in the drive-thru quicker through the queue.

A drive-thru lane is self-regulating. That is, customers will approach the property and evaluate whether it would be faster to enter the drive-thru lane, or park their vehicle and walk in. This shows why expediting drive-thru operations and increasing parking space accessibility (by increasing parking turnover, shortening parking duration, and minimizing internal circulation "friction" or conflicts) go hand-in-hand in equalizing peak traffic demand on site, enhancing the Project's ability to fully address parking, site access and on-site circulation needs during periods of peak activity.

**Table 2** summarizes the findings of the drive-thru queuing analysis for the Project that presumes the 25% reduced demand on the drive-thru. **Figure 3** illustrates where the average queue of 5 vehicles, the 85<sup>th</sup> percentile queue of 8 vehicles, the 95<sup>th</sup> percentile queue of 9 vehicles, and the 100<sup>th</sup> percentile or maximum queue of 11 vehicles. As shown, there would be adequate queue storage provided in the drive-thru lane to meet the 85<sup>th</sup> percentile queue of 8 vehicles and the 95<sup>th</sup> percentile queue of 9 vehicles. The 100<sup>th</sup> percentile or maximum queue of 11 vehicles would spill over onto the drive aisle and temporarily block parking spaces adjoining the building





north of the Project. To address this, and consistent with drive-thru queue management measures implemented at other Starbucks locations, a Starbucks Team Member will be immediately dispatched and stationed at the drive-thru entrance at any time the queue reaches the end of the drive-thru lane (i.e., queue of 9 vehicles for this Project) to help facilitate drive-thru orders and prevent Starbucks customers from queuing on the drive aisle (i.e., queue exceeding 9 vehicles).

Based on the drive-thru design, operations, and service characteristics of the proposed Starbucks described above, we conclude that potential drive-thru queues could be accommodated within the proposed drive-thru lane with the implementation of the suggested management measure for a Starbucks Team Member to be stationed at the drive-thru entrance whenever the drive-thru queue reaches 9 vehicles.

Because the Project will maintain the same number of parking spaces as the existing Weinerschnitzel and provide a total supply of 30 spaces, the Project's parking supply is adequate.

• • • • •

We appreciate the opportunity to work on this study. If you have any questions regarding this analysis, please call us at 949.825.6175.

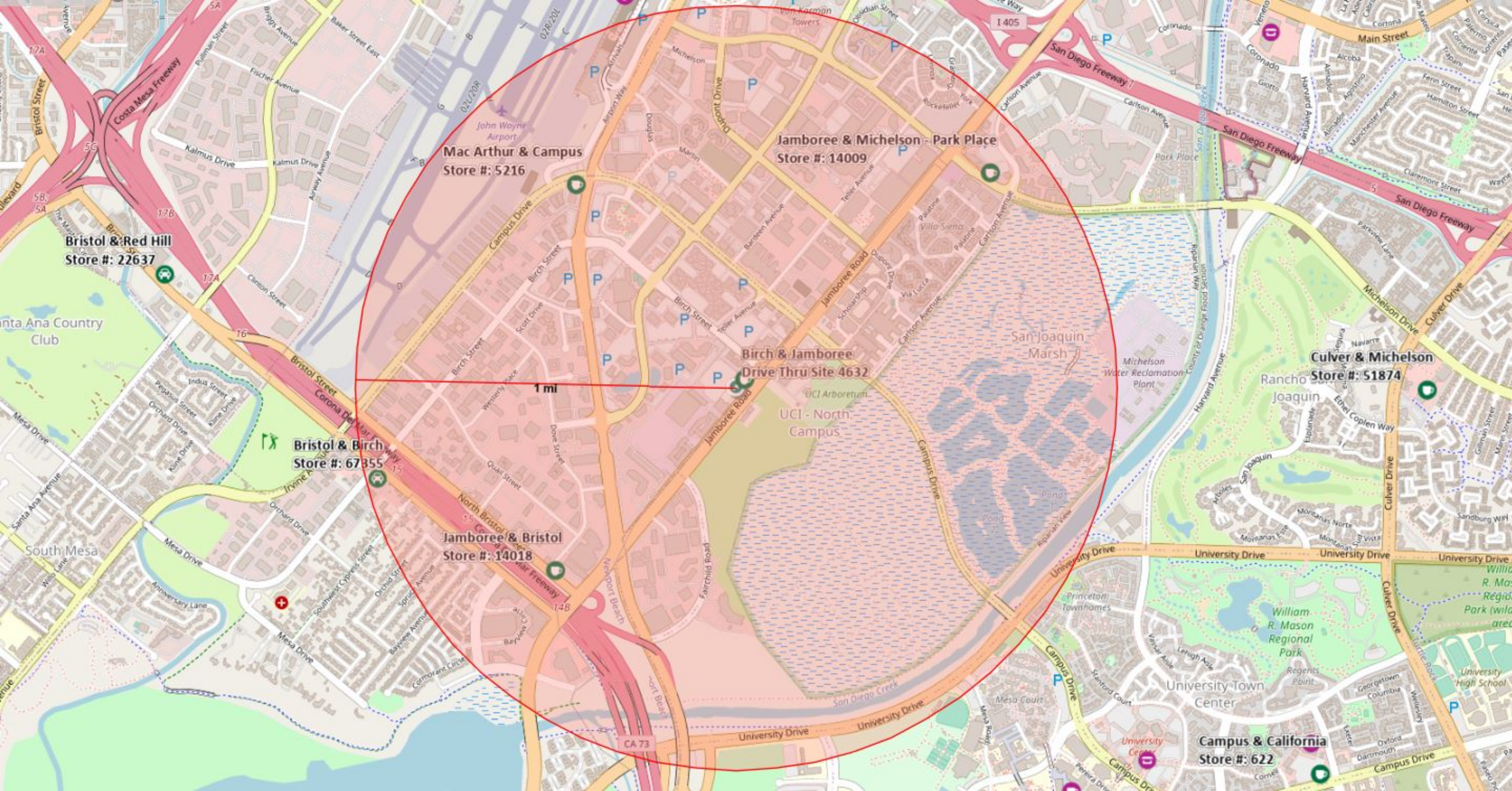
Sincerely,

**Linscott, Law & Greenspan, Engineers**

A handwritten signature in blue ink, reading 'Trissa (de Jesus) Allen'.

Trissa (de Jesus) Allen, P.E., RSP  
Senior Transportation Engineer





**Bristol & Red Hill**  
Store #: 22637

**Mac Arthur & Campus**  
Store #: 5216

**Jamboree & Michelson - Park Place**  
Store #: 14009

**Birch & Jamboree**  
Drive Thru Site 4632

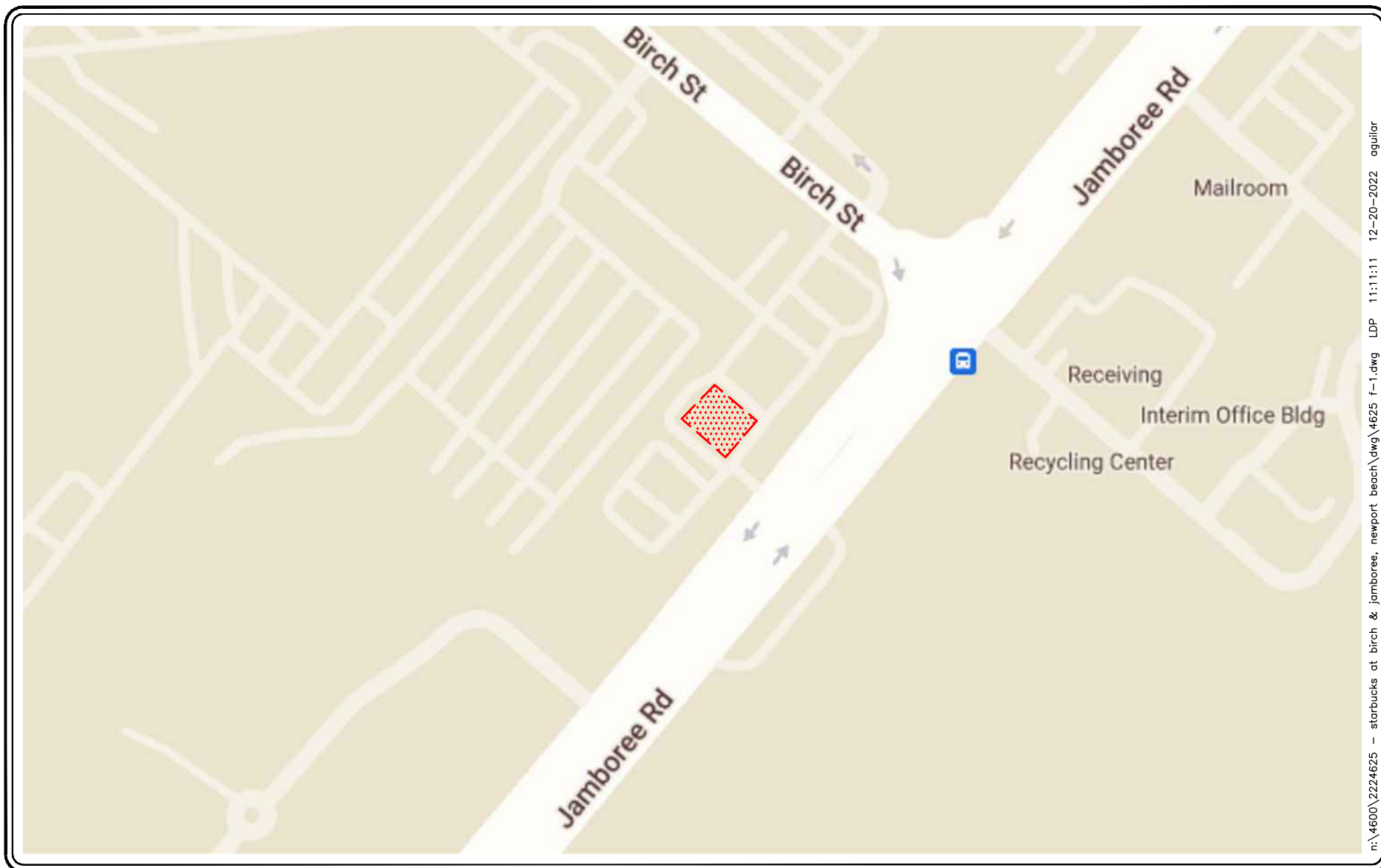
**Bristol & Birch**  
Store #: 67355

**Jamboree & Bristol**  
Store #: 14018

**Culver & Michelson**  
Store #: 51874

**Campus & California**  
Store #: 622





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SOURCE: GOOGLE

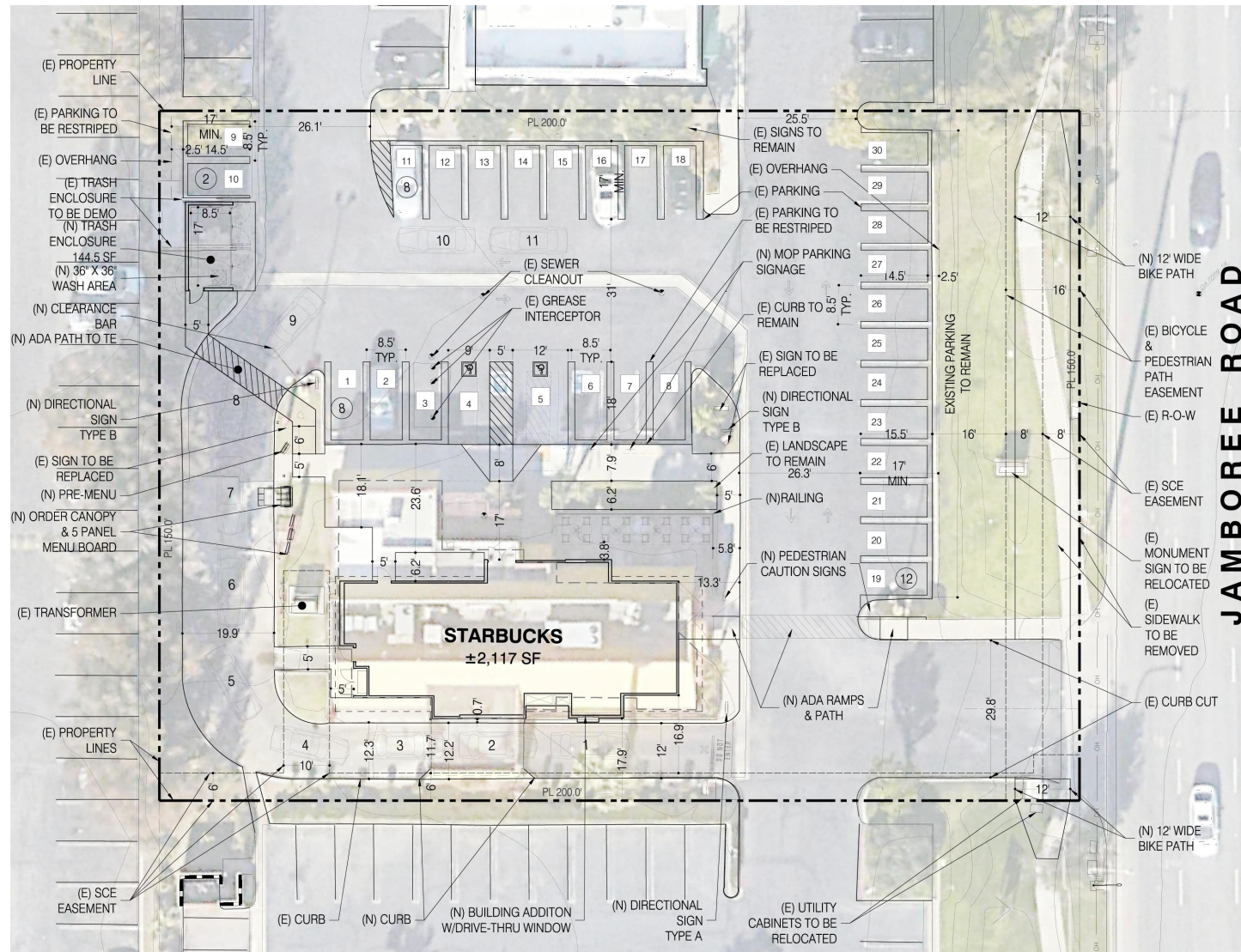
KEY

 = PROJECT SITE

## FIGURE 1

### VICINITY MAP

STARBUCKS AT BIRCH & JAMBOREE, NEWPORT BEACH



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SOURCE: GREENBERG FARROW

FIGURE 2

PROPOSED DRIVE-THRU DESIGN

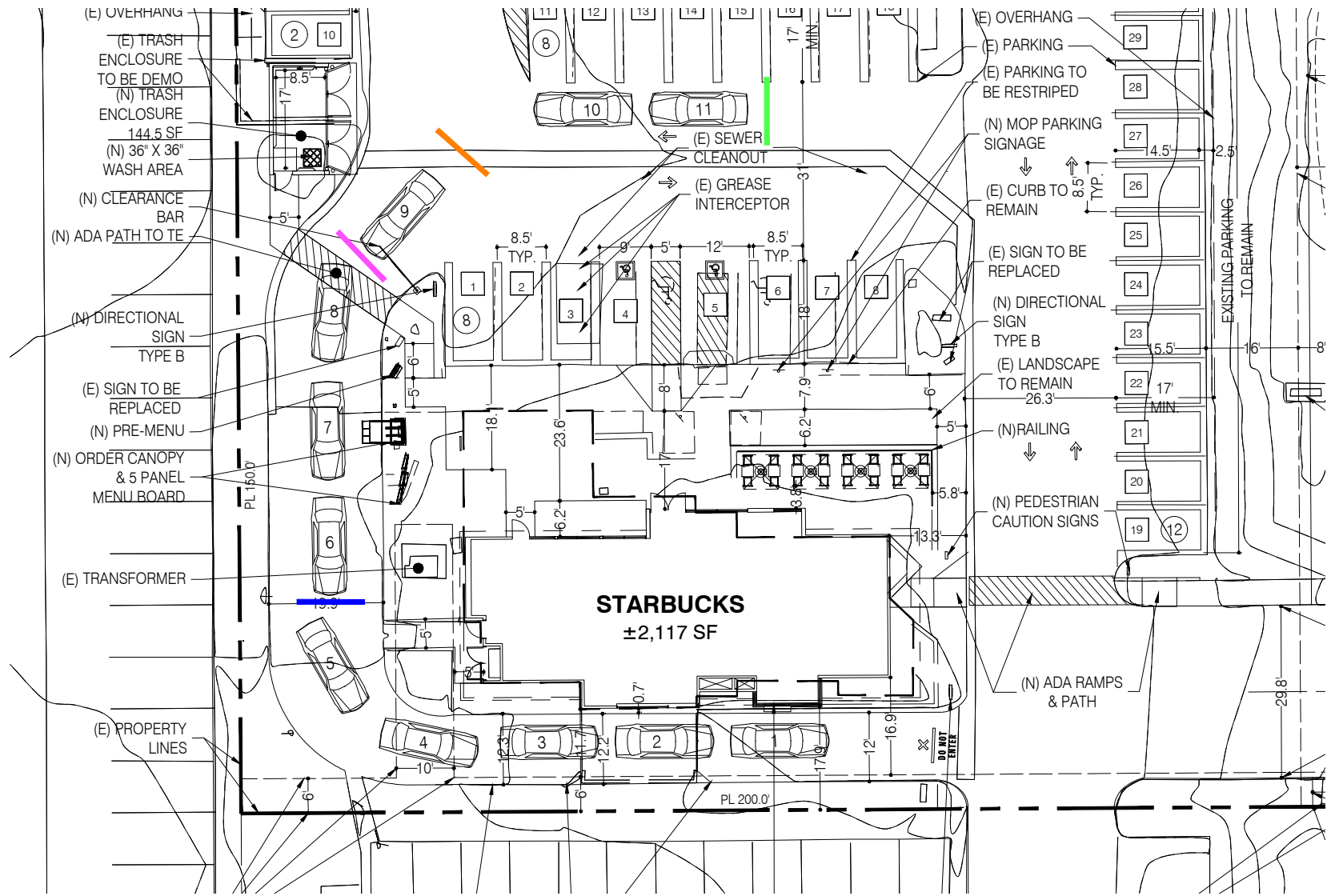
STARBUCKS AT BIRCH & JAMBOREE, NEWPORT BEACH

LINSCOTT  
LAW &  
GREENSPAN

engineers



NO SCALE



n:\4600\2224625 - starbucks at birch & jamboree, newport beach\dwg\4625 f-3.dwg LDP 10:59:51 01-05-2023 agular

LINSCOTT  
LAW &  
GREENSPAN  
engineers



- SOURCE: GREENBERG FARROW
- KEY
- = AVERAGE QUEUE
  - = 85th PERCENTILE QUEUE
  - = 95th PERCENTILE QUEUE
  - = MAXIMUM QUEUE

## FIGURE 3

### PROPOSED DRIVE-THROUGH LANE STACKING REQUIREMENTS

STARBUCKS AT BIRCH & JAMBOREE, NEWPORT BEACH

**TABLE 1**  
**SEPTEMBER/OCTOBER 2021 DRIVE-THROUGH QUEUE OBSERVATIONS AT STARBUCKS ANAHEIM<sup>1</sup>**  
**STARBUCKS AT BIRCH & JAMBOREE, NEWPORT BEACH**

Number of Vehicles Observed in Queue (N)	Number of Occurrences (N) Was Observed		
	Day #1 Wednesday September 29, 2021	Day #2 Thursday September 30, 2021	Day #3 Friday October 1, 2021
0	5	8	6
1	23	23	34
2	44	44	67
3	38	65	56
4	41	76	57
5	51	79	38
6	45	53	30
7	49	20	19
8	49	15	21
9	38	26	18
10	21	6	28
11	10	5	25
12	5	0	10
13	1	0	6
14	0	0	5
<b>Total</b>	<b>420</b>	<b>420</b>	<b>420</b>
<b>Average</b>	<b>6</b>	<b>5</b>	<b>5</b>
<b>85<sup>th</sup> Percentile</b>	<b>9</b>	<b>7</b>	<b>10</b>
<b>95<sup>th</sup> Percentile</b>	<b>10</b>	<b>9</b>	<b>11</b>
<b>100<sup>th</sup> Percentile (Max)</b>	<b>13</b>	<b>11</b>	<b>14</b>

Note:

[1] Drive-thru queue observations were conducted at Starbucks located at 1224 N. Harbor Boulevard, Anaheim from 7:00AM to 9:00AM, 11:00AM to 1:00PM, and 4:00PM to 7:00PM on each of the three survey dates, by AimTD, LLC.

**TABLE 2**  
**PROJECT QUEUEING FORECAST**  
**STARBUCKS AT BIRCH & JAMBOREE, NEWPORT BEACH**

Queue	Peak Queue Observed from Starbucks Anaheim per <i>Table 1</i> (veh)	Proposed Project (Starbucks at Birch & Jamboree)		
		Adjusted Queue for the Project with 25% Reduction (veh)	Proposed Drive-Thru Queue Storage (veh)	Adequate Drive-Thru Storage? (Yes/No)
Average Queue	6	5	9	Yes
85 <sup>th</sup> Percentile Queue	10	8	9	Yes
95 <sup>th</sup> Percentile Queue	11	9	9	Yes
100 <sup>th</sup> Percentile Queue (Max)	14	11	9	No [a]

Note:

[a] To address this, and consistent with drive-thru queue management measures implemented at other Starbucks locations, a Starbucks Team Member will be immediately dispatched and stationed at the drive-thru entrance at any time the queue reaches the end of the drive-thru lane (i.e., queue of 9 vehicles for this Project) to help facilitate drive-thru orders and prevent Starbucks customers from queuing on the drive aisle (i.e., queue exceeding 9 vehicles).



January 27, 2022

Mr. Kareem Ali  
Starbucks  
555 Anton Boulevard, Suite 300  
Costa Mesa, CA 92626

LLG Reference: 2.21.4466.1

Subject: **Revised  
Drive-Thru Queuing, Site Access, and Parking Evaluation for  
Starbucks at Bristol & Birch**  
Newport Beach, California

**Engineers & Planners**

Traffic  
Transportation  
Parking

**Linscott, Law &  
Greenspan, Engineers**

2 Executive Circle  
Suite 250  
Irvine, CA 92614  
**949.825.6175** T  
949.825.6173 F  
www.llgengineers.com

Pasadena  
Irvine  
San Diego  
Woodland Hills

Dear Mr. Ali:

Linscott, Law & Greenspan, Engineers (LLG) is pleased to present the findings of a drive-thru queuing, site access, and parking evaluation for the proposed Starbucks located at the southeast corner of the Bristol Street and Birch Street intersection (presuming Bristol Street runs in the east-west direction) in the City of Newport Beach. This letter report updates the prior study dated January 25, 2022 to address City staff comments on that previous study.

Based on the findings of our analysis, the proposed design and configuration of the drive-thru facility and site access points will be adequate in serving drive-thru queues, and inbound and outbound traffic generated by the Project. In addition, the proposed on-site supply of 20 spaces will be adequate in meeting the Project's total parking needs.

The following pages describe our study methodology and findings.

## PROJECT DESCRIPTION

**Figure 1** presents a vicinity map for the Project. **Figure 2** illustrates the proposed site plan and drive-thru design.

The Project consists of 2,565 square feet (SF) of gross floor area, plus 300 SF of patio seating. Vehicular access is provided via two-way (entry and exit) driveways located off eastbound Bristol Street and Birch Street. A drive-thru lane and 20 on-site parking spaces will be provided.

Philip M. Linscott, PE (1924-2000)  
Jack M. Greenspan, PE (Ret.)  
William A. Law, PE (Ret.)  
Paul W. Wilkinson, PE  
John P. Keating, PE  
David S. Shender, PE  
John A. Boorman, PE  
Clare M. Look-Jaeger, PE  
Richard E. Barretto, PE  
Keil D. Maberry, PE



As shown on *Figure 2*, approximately 11 vehicles can be accommodated within the drive-thru lane, consisting of 5 vehicles before reaching the order/menu board, and 6 vehicles between the order/menu board and the pick-up window.

The scope for this study was developed in close coordination with City staff, and included a thorough review of existing Starbucks locations in order to select sites that are comparable to the Project for the purposes of conducting drive-thru queuing observations and parking demand counts. *Appendix A* contains maps showing existing Starbucks cafes that were considered.

The first map in *Appendix A* shows that there are four Starbucks cafes that exist within a radius of one mile from the Project, as follows:

- **Bristol & Redhill Store# 22637**, which has a drive-thru, and located in the Bristol Village shopping center approximately 0.8 miles northwest of the Project
- **Jamboree & Bristol Store# 14018**, without a drive-thru, and located in the Back Bay Court shopping center approximately 0.8 miles southeast of the Project
- **MacArthur & Campus Store# 5216**, without a drive-thru, and located in the MacArthur/Campus shopping center
- **Jamboree & Birch Store# 5661**, without a drive-thru, and located in the Jamboree Plaza shopping center

Because the four existing cafes listed above are located within a shopping center, their development setting would differ from the Project, which is a standalone site. Two standalone Starbucks cafes located in Anaheim were identified to be more representative of the Project (i.e., not part of a shopping center or mixed-use development, freeway accessible, located along a major arterial, a parking supply of around 20 spaces). The second and third maps contained in *Appendix A* illustrate the locations of the two cafes selected as survey locations for this study, as follows:

- **Harbor & 91 Fwy Store# 23316 (1224 N. Harbor Boulevard, Anaheim)**, which is a standalone café, approximately 0.2 miles south of the SR-91 Freeway, with full access (unsignalized driveway) off Harbor Boulevard, a drive-thru facility that can accommodate approximately 8 vehicles in queue, and provides 22 spaces on site. *Appendix A* includes the site plan for this café. The City of Anaheim does not require the implementation of peak period traffic control or parking management measures for this site.
- **Lincoln & Magnolia Store# 59228 (2595 W. Lincoln Avenue, Anaheim)**, which is a standalone café, approximately 1.2 miles southwest of the I-5 Freeway, with right-turn in and out access (unsignalized driveways) off Magnolia Avenue and Lincoln Avenue, a drive-thru facility that can accommodate approximately 8 vehicles in queue, and provides 16 spaces on

site. *Appendix A* includes the site plan for this café. The City of Anaheim does not require the implementation of peak period traffic control or parking management measures for this site.

*Appendix B* contains the Project's prior site plan approved by the County of Orange (PA 95-109 Changed Plan), which shows a total of 24 spaces on site. The Project will result in a net reduction of 4 spaces due to parking lot and site access design improvements required by the City for the Project, as follows:

- Improvement to the accessible path from the Bristol Street right-of-way to the store entrance.
- Modification to the existing accessible parking space and sidewalk ramp for compliance with current accessible code requirements.
- Relocation of the trash enclosure to place it on an accessible path of travel from the building for compliance with accessible code requirements. Relocation of the trash enclosure affected the existing non-conforming tandem parking spaces. Removal of tandem parking spaces resulted in the loss of three stalls.
- One stall was removed to provide for a compliant turn around space.

## DRIVE-THRU QUEUING ANALYSIS

### *Queuing Survey Methodology*

To estimate drive-thru queue storage length requirements for Starbucks, queue observations were conducted at the existing Starbucks located at:

- **Harbor & 91 Fwy Store# 23316 (1224 N. Harbor Boulevard, Anaheim)**

*Figure 3* presents the hourly transaction profile for the above site for each day of the week (September 27, 2021 through October 3, 2021). It shows that the peak number of transactions occurred on Wednesday, Thursday, and Friday, coinciding with the drive-thru queuing observations that were conducted during these peak operating days of the week (September 29, 2021, September 30, 2021, and October 1, 2021). The time periods selected for each survey date were 7:00 AM to 9:00 AM, 11:00 AM to 1:00 PM, and 4:00 PM to 7:00 PM.

*Table 1* provides a comparison between drive-thru and indoor transactions (comprised of mobile orders and other transactions made inside the café) during the survey week. For Site #1 (1224 N. Harbor) evaluated for queuing purposes, *Table 1* indicates that the greatest number of total transactions occurred on Thursday, with approximately 66% of sales generated from drive-thru operations, 20% from mobile orders from inside the café (representing customers who parked in a space and walked into the café), and 14% from other indoor orders. These are important aspects

to note because they indicate that the majority of Starbucks orders were made from the drive-thru lane, as opposed to walk-in traffic occupying a parking space, and that there was a high percentage of mobile orders generated.

### ***Survey Results and Data Interpretation***

**Table 2** presents the drive-thru queue observation results for each of the three survey dates, indicating the number of vehicles in queue, and the number of occurrences each queue length was observed. As **Table 2** indicates, the average queue observed was 6 vehicles, the 85<sup>th</sup> percentile queue was 10 vehicles, the 95<sup>th</sup> percentile queue was 11 vehicles, and the 100<sup>th</sup> percentile or maximum queue was 14 vehicles. The site plan for this café shows that approximately 8 vehicles can be accommodated within the drive-thru facility. When the queue exceeded this queue storage length, the vehicles were observed to use the drive aisle, but the queue dissipated quickly without any special/peak period management measures being implemented.

According to Starbucks, drive-thru transactions that were generated during the surveys are comparable to pre-Covid conditions, and likely to be greater than before since more customers are now choosing to use the drive-thru instead of going inside the café.

The observed queue lengths at the existing Starbucks Anaheim are anticipated to be greater than what the proposed Starbucks at Bristol & Birch could generate. This is because the proposed Starbucks at Bristol & Birch is projected to have approximately 25% less sales than the existing Starbucks Anaheim based on the fact that there are more Starbucks locations that exist within one mile of the Project (as discussed in the Project Description section of this study). With four Starbucks cafes that exist near the Project, customers clearly have more options to choose from, thereby reducing the demand on the Project's drive-thru facility by the estimated 25%.

In addition, the Anaheim Starbucks evaluated for drive-thru queuing purposes serves a larger, more regional market that includes traffic from the SR-91 Freeway, Harbor Boulevard corridor, Downtown Fullerton to the north, and Downtown Anaheim to the south. There is only one other café that exists within a one-mile radius. In contrast, the proposed Project will primarily be serving commuters (who do not linger like in a sit-down café without a drive-thru) and more local (and directional) traffic along the Bristol Street one-way couplet, and SR-73 Freeway further disperses because of high proliferation of Starbucks cafes (a total of four cafes within a one-mile radius) in the Newport Beach area. Based on the characteristics of the proposed project site, it is not envisioned that patrons will linger within the café given the more inviting options presented in the other nearby locations.

It should further be noted that the proposed Starbucks at Bristol & Birch will have newer equipment that operate quicker, and the initial order point located one to two vehicles further than the existing Starbucks Anaheim. This is expected to result in

greater efficiency of the Project's drive-thru operations, allowing the Starbucks team to receive the customer's order sooner and have it be ready when they arrive at the window, which in turn, will help facilitate processing orders and serve drive-thru customers more expediently, and move vehicles in the drive-thru quicker through the queue.

A drive-thru lane is self-regulating. That is, customers will approach the property and evaluate whether it would be faster to enter the drive-thru lane, or park their vehicle and walk in. This shows why expediting drive-thru operations and increasing parking space accessibility (by increasing parking turnover, shortening parking duration, and minimizing internal circulation "friction" or conflicts) go hand-in-hand in equalizing peak traffic demand on site, enhancing the Project's ability to fully address parking, site access and on-site circulation needs during periods of peak activity. The next sections of this letter report discuss our findings from detailed site access and parking analyses that indicate the Project will be able to adequately address site access and parking needs.

As described previously, the Project is anticipated to have a reduced demand of 25% based on the fact that there are four more Starbucks locations that exist within one mile of the Project (compared to only one other café that exists within the same one-mile radius of the Starbucks Anaheim observed for queuing purposes). The projected 25% reduced demand on the Project's drive-thru operations also accounts for the potential benefits of two operational strategies, which are not currently implemented at the existing Starbucks Anaheim, and are described below.

As presented previously on *Table 1* (which provides a comparison between drive-thru and indoor transactions during the survey week), the majority of Starbucks orders were made from the drive-thru lane, as opposed to walk-in traffic occupying a parking space, and that there was a high percentage of mobile orders generated. Based on this, and as a conservative measure to better facilitate drive-thru queuing during peak periods for the Project, it is recommended for the Project to position an order taker outside to expedite service from the drive-thru and help vehicle queues to dissipate quicker. In addition, the Project should designate 1 or 2 spaces (farthest away from Bristol Street) for mobile pick-up orders to help reduce drive-thru queuing and shorten parking duration. The potential benefit of allocating spaces to mobile pick-up orders will be discussed further in the parking section of this letter report.

**Table 3** summarizes the findings of the drive-thru queuing analysis for the Project that presumes the 25% reduced demand on the drive-thru. **Figure 4** illustrates where the average queue of 5 vehicles, the 85<sup>th</sup> percentile queue of 8 vehicles, the 95<sup>th</sup> percentile queue of 9 vehicles, and the 100<sup>th</sup> percentile or maximum queue of 11 vehicles would be located within the proposed drive-thru lane for the Project. As shown, there would be adequate queue storage provided in the proposed design of the drive-thru lane to fully accommodate all estimates of queue lengths; however, it should be noted that the 85<sup>th</sup> percentile queue length is considered to be the industry's

most appropriate and solid basis for planning and designing a drive-thru facility. As described previously, drive-thru queues are expected to be shorter and would dissipate quicker due to projections of 25% less sales for the Project compared to the existing Starbucks Anaheim, and greater efficiency of processing drive-thru orders because of the proposed order/menu board and pickup window configuration, positioning an order taker outside to expedite drive-thru service during peak periods, and designating 1 or 2 spaces for mobile order pick-up.

Based on the drive-thru design, operations, and service characteristics of the proposed Starbucks described above, we conclude that potential drive-thru queues could be accommodated within the proposed drive-thru lane without the implementation of any on-site traffic control or additional management measures (going beyond what has previously been described).

## SITE ACCESS ANALYSIS

**Table 4** presents the ITE trip rates (per the 11<sup>th</sup> Edition of ITE's Trip Generation manual) and the resulting ITE trip generation estimates for the Starbucks Project and for the existing Burger King to be replaced by the Project.

As indicated in *Table 4*, compared to the existing Burger King, the Project would generate an increase of 170 weekday daily trips, 106 AM peak hour trips, and 15 PM peak hour trips. These ITE-based incremental peak hour trips for the Project were assigned to each project driveway intersection, and provide the basis for detailed peak hour level of service and queuing analyses at each of the two project driveway intersections under Existing Plus Project conditions. It should be noted that the LOS analysis conducted for existing conditions account for actual trips generated by the existing Burger King currently operating on the project site based on the AM and PM peak hour traffic counts collected at the two project driveway intersections on Tuesday, October 12, 2021.

**Figure 5** illustrates the general directional traffic distribution pattern for the proposed Project. Project traffic volumes both entering and exiting the Project site have been distributed and assigned to the adjacent street system based on the following considerations:

- directional flows on the freeways in the immediate vicinity of the project site (i.e., SR-73 Freeway)
- the site's proximity to major traffic carriers (i.e., Bristol Street, Birch Street, etc.)
- expected localized traffic flow patterns based on adjacent street channelization and presence of traffic signals
- ingress/egress restrictions and availability at the project site

The HCM level of service methodology was applied to evaluate project driveway intersection operations and queues for specific movements. **Table 5** indicates that the two intersections currently operate at acceptable levels of service (i.e., LOS D or better), and would continue to operate at the same acceptable levels with the Project. Furthermore, the proposed Starbucks would not increase the queue lengths for specific movements at each project driveway intersection, as indicated in **Table 6**.

## PARKING ANALYSIS

Based upon the proposed site plan for the project, a total of 20 surface parking spaces will be provided on site.

For “Food Service-Fast Food” restaurants, *Chapter 21.40 Off-Street Parking* of the City’s Municipal Code requires 1 space per 50 SF of gross floor area, and 1 space per 100 SF of outdoor dining areas. The application of this City Code ratio to the proposed Starbucks (2,565 SF GFA, plus 300 SF of outdoor seating) yields a Code-based parking requirement of 55 spaces. The parking requirement for this Project is based on the existing Conditional Use Permit, which was approved by the County of Orange in 1995. The Conditional Use Permit requires 24 parking spaces. As such, the City requires 24 parking spaces in order to determine compliance with the parking requirement. A 4-space parking waiver is being sought to address the shortfall between the 24 required parking spaces, and the 20 parking spaces proposed.

In order to determine the parking demand for a Starbucks with a drive-thru, field studies were conducted at two separate Starbucks locations. Those locations include the same Anaheim location observed for drive-thru queuing purposes detailed herein, and second Starbucks, located at:

- **Lincoln & Magnolia Store# 59228 (2595 W. Lincoln Avenue, Anaheim)**

As required by the City, the following summarizes operational information for the Project and each of the two existing Starbucks locations:

- **Project**
  - Lot Size: 21,954 SF
  - Building Size: 2,565 SF (plus 300 SF of outdoor seating)
  - Number of Staff on Peak Shift: 6-8 (this varies depending on the day)
  - Total Number of Employees: 30
  - Hours of Operation: 4:00 AM to 12:00 AM daily
  - Parking Supply: 20 spaces



- **1224 N. Harbor Boulevard, Anaheim**
  - Lot Size: 32,626 SF
  - Building Size: 1,700 SF (plus 400 SF of outdoor seating)
  - Number of Staff on Peak Shift: 6-8 (this varies depending on the day)
  - Total Number of Employees: 35
  - Hours of Operation: 4:30 AM to 11:00 PM daily
  - Parking Supply: 22 spaces
- **2595 W. Lincoln Avenue, Anaheim**
  - Lot Size: 22,499 SF
  - Building Size: 2,000 SF (plus 400 SF of outdoor seating)
  - Number of Staff on Peak Shift: 6-8 (this varies depending on the day)
  - Total Number of Employees: 40
  - Hours of Operation: 5:00 AM to 10:00 PM daily
  - Parking Supply: 16 spaces

As discussed previously, *Table 1* provides a comparison between drive-thru and indoor transactions (comprised of mobile orders and other transactions made inside the cafe) during the survey week.

For Site #1 (1224 N. Harbor) evaluated for queuing and parking purposes, *Table 1* indicates that the greatest number of total transactions occurred on Thursday, with approximately 66% of sales generated from drive-thru operations, 20% from mobile orders from inside the café (representing customers who parked in a space and walked into the café), and 14% from other indoor orders. These are important aspects to note because they indicate that the majority of Starbucks orders were made from the drive-thru lane, as opposed to walk-in traffic occupying a parking space, and that there was a high percentage of mobile orders generated.

For Site #2 (2595 W. Lincoln) evaluated for parking purposes, *Table 1* indicates that the greatest number of total transactions occurred on the same Thursday as Site #1, with approximately 65% of sales generated from drive-thru operations, 15% from mobile orders from inside the café, and 20% from other indoor orders.

**Table 7** presents a summary of the parking supply and demand counts for the two existing Starbucks Anaheim locations. The hourly demand counts were performed between 6:00 AM and 8:00 PM on Friday, October 8, 2021 and Saturday, October 9, 2021. All employees park on site, and were accounted for in the parking demand counts conducted.

According to Starbucks, mobile order and other indoor transactions that were generated during the surveys are comparable to pre-Covid conditions, and likely to be significantly less than before since more customers are now choosing to use the drive-

thru instead of going inside the café. Mobile orders significantly reduce the amount of time that patrons spend parked within a parking space because they are able to quickly park, pick up their pre-ordered items, and proceed to their destination.

As discussed previously, the observed parking demand at the existing Starbucks Anaheim locations are anticipated to be greater than what the proposed Starbucks at Bristol & Birch could generate. This is because the proposed Starbucks at Bristol & Birch is projected to have approximately 25% less sales than the existing Starbucks Anaheim cafes based on the fact that there are more Starbucks locations that exist within one mile of the Project (as discussed in the Project Description section of this study). With four Starbucks cafes that exist near the Project (compared to only one other café that exists within the same one-mile radius of each of the two Anaheim cafes observed for parking demand purposes), customers clearly have more options to choose from, thereby reducing the Project's parking demand by the estimated 25%.

In addition, the two Anaheim Starbucks cafes evaluated for parking demand purposes serve a larger, more regional market that includes traffic from the SR-91 Freeway, Harbor Boulevard corridor, Beach Boulevard corridor, Downtown Fullerton to the north, and Downtown Anaheim to the south. There is only one other café that exists within a one-mile radius of each Anaheim café surveyed. In contrast, the proposed Project will primarily be serving commuters (who do not linger like in a sit-down café without a drive-thru) and more local (and directional) traffic along the Bristol Street one-way couplet, and SR-73 Freeway already dispersed because of high proliferation of Starbucks cafes (a total of four cafes within a one-mile radius) in the Newport Beach area.

It should further be noted that the proposed Starbucks at Bristol & Birch will have newer equipment that operate quicker. This is expected to result in greater efficiency of mobile order and other indoor cafe operations.

The projected 25% reduced parking demand also accounts for the potential benefits of two operational strategies, which are not currently implemented at the existing Starbucks Anaheim. As described previously, these include positioning an order taker outside to expedite drive-thru service during peak periods, and designating 1 or 2 spaces for mobile order pick-up. The implementation of these measures will not only help expedite drive-thru operations but also increase parking space accessibility.

The allocation of parking spaces for mobile order pick-up nearest the doorways and farthest away from Bristol Street will maximize parking efficiency on site by increasing parking turnover, shortening parking duration, and will help minimize internal circulation "friction" or conflicts (due to customers parking and backing out of the space) along the drive aisle and the Bristol access (reducing the potential for back-ups onto Bristol Street). As discussed in the prior sections of this study regarding the "self-regulating" aspect of the Project's tripmaking characteristics, drive-thru operations go hand-in-hand with on-site parking in equalizing peak traffic



demand on site; one serves as a “relief valve” for the other. In other words, customers will approach the property and evaluate whether it would be faster to enter the drive-thru lane, or park their vehicle and walk in.

The Project anticipates having 6 to 8 employees, and, consistent with the two Anaheim cafes surveyed for parking demand evaluation purposes, will be parked on site. Because employees have a longer parking duration compared to customers, they will be parking away from the front doors, in the southern portion of the parking lot nearest the drive-thru lane. This would ensure that more conveniently located spaces are preserved for customer use, and would help minimize vehicular and pedestrian conflicts near the drive-thru entrance (by lessening the frequency of vehicles accessing and departing a parking space during peak periods because the spaces would primarily serve employees parked for the majority of the day).

Based on the above considerations, having 1 or 2 spaces for the exclusive use by mobile order customers, positioning an order taker outside to expedite drive-thru service during peak times, and parking employees in a strategic portion of the lot, will enhance the Project’s ability to fully meet peak parking demand, in addition to addressing drive-thru, site access, and on-site circulation needs during periods of peak activity.

As *Table 7* indicates, peak parking demand observed was 14 spaces for Site #1 and 16 spaces for Site #2. The bottom portion shows the total size (GFA plus outdoor seating) for each site, and the resulting empirical parking ratio derived (peak demand divided by the total SF) for each survey date. The greatest ratio derived is 6.7 spaces per 1,000 SF. This is consistent with empirical ratios from LLG’s older parking studies completed for Starbucks, and is therefore considered to be a solid demand factor for application in estimating the Project’s parking needs.

Applying a 25% reduction to the empirical ratio of 6.7 spaces per 1,000 SF (to further calibrate the empirical results to account for the unique aspects and setting for the Project) to the proposed Starbucks (2,565 SF GFA, plus 300 SF of outdoor seating) results in a parking ratio of 5.0 spaces per 1,000 SF and a parking demand of 14 spaces. Adding a 10% contingency to the 14-space demand yields in an adjusted demand of 15 spaces. Compared against the proposed on-site supply of 20 spaces, the 15-space demand results in a surplus of 5 spaces.

Even without applying the 25% reduction, the proposed 20-space on-site supply would be adequate in meeting an empirical-based demand of 19 spaces (6.7 spaces per 1,000 SF applied to 2,865 SF).

*Appendix B* contains the prior site plan approved by the County of Orange (PA 95-109 Changed Plan), which shows a total of 24 spaces on site. The Project will result in a net reduction of 4 spaces due to parking lot and site access design improvements

required by the City for the Project to meet accessible code requirements and design standards.

Based on the above, we conclude that the on-site supply of 20 spaces will be adequate in meeting the Project's parking demand.

We appreciate the opportunity to work on this study. If you have any questions regarding this analysis, please call us at 949.825.6175.

Sincerely,

**Linscott, Law & Greenspan, Engineers**



Trissa (de Jesus) Allen, P.E.  
Senior Transportation Engineer

Attachments

**TABLE 1**  
**DRIVE-THRU VS. INDOOR TRANSACTIONS**  
**STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH**

Day of Week	Site #1: 1224 N. Harbor Blvd, Anaheim (9/27/2021 to 10/3/2021)							Site #2: 2595 W. Lincoln Ave, Anaheim (9/27/2021 to 10/3/2021)						
	Total Transactions	Drive-Thru Transactions		Mobile Order Indoor		Other Indoor Transactions		Total Transactions	Drive-Thru Transactions		Mobile Order Indoor		Other Indoor Transactions	
		Drive-Thru	Drive-Thru %	Mobile Order	Mobile %	Other Indoor	Other Indoor %		Drive-Thru	Drive-Thru %	Mobile Order	Mobile %	Other Indoor	Other Indoor %
Monday	811	539	66%	152	19%	120	15%	790	561	71%	103	13%	126	16%
Tuesday	869	543	62%	180	21%	146	17%	902	613	68%	118	13%	171	19%
Wednesday	956	582	61%	184	19%	190	20%	932	624	67%	140	15%	168	18%
Thursday	962	631	66%	197	20%	134	14%	938	610	65%	140	15%	188	20%
Friday	952	617	65%	175	18%	160	17%	894	572	64%	143	16%	179	20%
Saturday	885	519	59%	169	19%	197	22%	902	595	66%	127	14%	180	20%
Sunday	855	535	63%	141	16%	179	21%	885	593	67%	142	16%	150	17%

Note:

[a] Source: Starbucks.

**TABLE 2**  
**DRIVE-THROUGH QUEUE OBSERVATIONS<sup>1</sup>**  
**STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH**

Number of Vehicles Observed in Queue (N)	Number of Occurrences (N) Was Observed		
	Day #1 Wednesday September 29, 2021	Day #2 Thursday September 30, 2021	Day #3 Friday October 1, 2021
0	5	8	6
1	23	23	34
2	44	44	67
3	38	65	56
4	41	76	57
5	51	79	38
6	45	53	30
7	49	20	19
8	49	15	21
9	38	26	18
10	21	6	28
11	10	5	25
12	5	0	10
13	1	0	6
14	0	0	5
<b>Total</b>	<b>420</b>	<b>420</b>	<b>420</b>
<b>Average</b>	<b>6</b>	<b>5</b>	<b>5</b>
<b>85<sup>th</sup> Percentile</b>	<b>9</b>	<b>7</b>	<b>10</b>
<b>95<sup>th</sup> Percentile</b>	<b>10</b>	<b>9</b>	<b>11</b>
<b>100<sup>th</sup> Percentile (Max)</b>	<b>13</b>	<b>11</b>	<b>14</b>

Note:

[1] Drive-thru queue observations were conducted at Starbucks located at 1224 N. Harbor Boulevard, Anaheim from 7:00AM to 9:00AM, 11:00AM to 1:00PM, and 4:00PM to 7:00PM on each of the three survey dates, by AimTD, LLC.

**TABLE 3**  
**PROJECT QUEUEING SUMMARY**  
**STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH**

Queue	Peak Queue Observed from Survey Sites from Table 2 (veh)	Proposed Project		
		Adjusted Queue for the Project with 25% Reduction (veh)	Drive-Thru Queue Storage (veh)	Adequate Drive-Thru Storage? (Yes/No)
Average Queue	6	5	11	Yes
85 <sup>th</sup> Percentile Queue	10	8	11	Yes
95 <sup>th</sup> Percentile Queue	11	9	11	Yes
100 <sup>th</sup> Percentile Queue (Max)	14	11	11	Yes

**TABLE 4**  
**PROJECT TRIP GENERATION RATES AND ESTIMATES<sup>1</sup>**  
**STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH**

ITE Land Use Code / Project Description	Daily 2-Way	AM Peak Hour			PM Peak Hour		
		Enter	Exit	Total	Enter	Exit	Total
<b><u>Trip Rates:</u></b>							
937: Coffee Shop with Drive-Thru (TE/1,000 SF)	533.57	51%	49%	85.88	50%	50%	38.99
934: Fast-Food Restaurant with Drive-Thru (TE/1,000 SF)	467.48	51%	49%	44.61	52%	48%	33.03
<b><u>Trip Estimates:</u></b>							
<b><u>Proposed Project</u></b>							
Starbucks (2,565 SF) Trips [A]	1,369	112	108	220	50	50	100
<b><u>Existing Land Use</u></b>							
Burger King (2,565 SF) Trips [B]	1,199	58	56	114	44	41	85
<b>Project Trip Increase</b>							
<b>Starbucks minus Burger King [A] – [B]</b>	<b>170</b>	<b>54</b>	<b>52</b>	<b>106</b>	<b>6</b>	<b>9</b>	<b>15</b>

Notes:

[1] Source: *Trip Generation, 11th Edition*, Institute of Transportation Engineers, (ITE) [Washington, D.C. (2021)].

- TE/1,000 SF = Trip End per Thousand Square Feet

**TABLE 5**  
**PROJECT DRIVEWAY INTERSECTION LOS SUMMARY**  
**STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH**

Project Driveway	Minimum Acceptable LOS	Jurisdiction	Time Period	(1)		(2)		(3)	
				Existing Traffic Conditions		Future Project Traffic Conditions		Poor LOS?	
				HCM	LOS	HCM	LOS	Increase	Yes/No
1. Project Dwy No. 1 at Bristol Street	D	Newport Beach	AM	15.9 s/v	C	17.2 s/v	C	1.3 s/v	No
			PM	18.2 s/v	C	18.2 s/v	C	0.0 s/v	No
2. Birch Street at Project Dwy No. 2	D	Newport Beach	AM	10.1 s/v	B	11.1 s/v	B	1.0 s/v	No
			PM	10.5 s/v	B	10.7 s/v	B	0.2 s/v	No

Notes:

- s/v = seconds per vehicle
- **Bold HCM/LOS** values indicate adverse service levels based on City of Newport Beach LOS standards.

**TABLE 6**  
**PROJECT DRIVEWAY INTERSECTION QUEUE SUMMARY**  
**STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH**

Project Driveway	Estimated Storage Provided (feet)	(1) Existing Traffic Conditions				(2) Future Project Traffic Conditions			
		AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
		Max. Queue/ Min. Storage Required <sup>1</sup>	Adequate Storage (Yes/No)	Max. Queue/ Min. Storage Required <sup>1</sup>	Adequate Storage (Yes/No)	Max. Queue/ Min. Storage Required <sup>1</sup>	Adequate Storage (Yes/No)	Max. Queue/ Min. Storage Required <sup>1</sup>	Adequate Storage (Yes/No)
1. Project Driveway No. 1 at Bristol Street									
Northbound Right-Turn Lane	22'	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes
Eastbound Shared Through/Right-Turn Lane	22'	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes
2. Birch Street at Project Driveway No. 2									
Northbound Shared Through/Right-Turn Lane	100'	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes
Southbound Left-Turn Lane	170'	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes
Westbound Shared Left/Right-Turn Lane	22'	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes	22' [2]	Yes

**Notes:**

[1] Per HCM 6 methodology, queue is based on the 95<sup>th</sup> percentile queues and is reported in total queue length (feet) per lane for unsignalized intersections.

[2] A minimum queue of 22 feet per vehicle is assumed for all reported queues less than 22 feet.

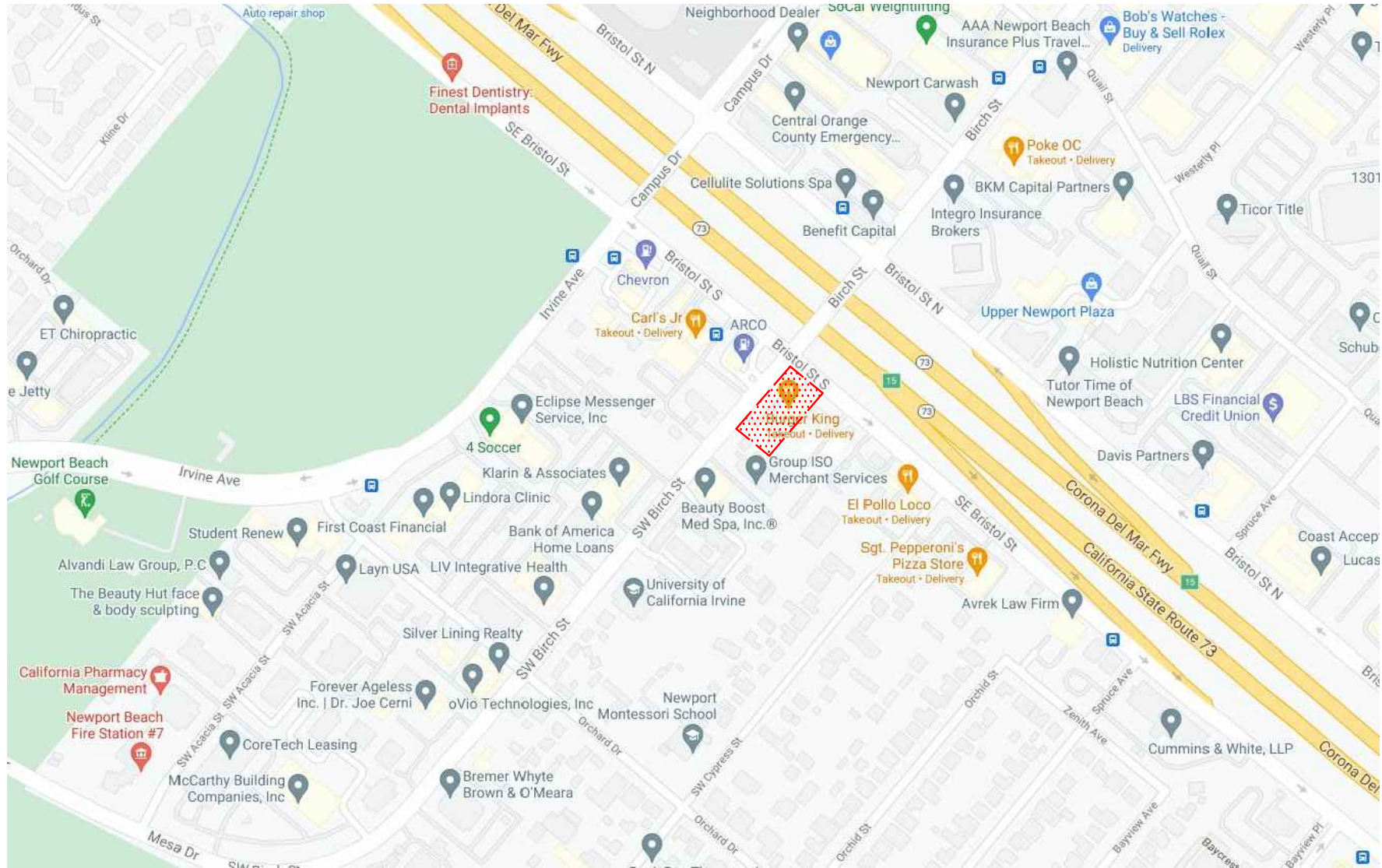


TABLE 7  
PARKING SUMMARY  
STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH

Start Time	Site #1: 1224 N. Harbor Blvd Anaheim, CA Friday, October 8, 2021			Site #1: 1224 N. Harbor Blvd Anaheim, CA Saturday, October 9, 2021			Site #2: 2595 W. Lincoln Ave Anaheim, CA Friday, October 8, 2021			Site #2: 2595 W. Lincoln Ave Anaheim, CA Saturday, October 9, 2021		
	Supply = 22			Supply = 22			Supply = 16			Supply = 16		
	Spaces Occupied	Parking Surplus	Percent Occupancy	Spaces Occupied	Parking Surplus	Percent Occupancy	Spaces Occupied	Parking Surplus	Percent Occupancy	Spaces Occupied	Parking Surplus	Percent Occupancy
6:00 AM	7	15	32%	3	19	14%	5	11	31%	3	13	19%
7:00 AM	10	12	45%	4	18	18%	8	8	50%	4	12	25%
8:00 AM	13	9	59%	5	17	23%	16	0	100%	7	9	44%
9:00 AM	11	11	50%	11	11	50%	13	3	81%	15	1	94%
10:00 AM	13	9	59%	10	12	45%	16	0	100%	15	1	94%
11:00 AM	11	11	50%	14	8	64%	11	5	69%	14	2	88%
12:00 PM	14	8	64%	10	12	45%	13	3	81%	16	0	100%
1:00 PM	13	9	59%	13	9	59%	11	5	69%	12	4	75%
2:00 PM	10	12	45%	14	8	64%	9	7	56%	11	5	69%
3:00 PM	11	11	50%	14	8	64%	14	2	88%	12	4	75%
4:00 PM	14	8	64%	14	8	64%	10	6	63%	10	6	63%
5:00 PM	14	8	64%	14	8	64%	10	6	63%	8	8	50%
6:00 PM	14	8	64%	10	12	45%	7	9	44%	9	7	56%
7:00 PM	14	8	64%	6	16	27%	10	6	63%	5	11	31%
8:00 PM	10	12	45%	5	17	23%	8	8	50%	5	11	31%
Size (SF)	2,100	--	--	2,100	--	--	2,400	--	--	2,400	--	--
Empirical Ratio	6.7 sp/KSF	--	--	6.7 sp/KSF	--	--	6.7 sp/KSF	--	--	6.7 sp/KSF	--	--
<p>Adjusted Empirical Ratio (with 25% Reduction): 5.0 sp/KSF</p> <p>Project Size: 2,865 (2,565 SF GFA + 300 SF outdoor seating)</p> <p>Project Peak Parking Demand: 14 spaces</p> <p>Project Peak Demand w/ 10% Contingency: 15 spaces</p> <p>Proposed Supply: 20 spaces</p> <p>Parking Surplus: 5 spaces</p>												


Note:

[a] Source: Parking counts conducted by AimTD, LLC.



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**LINSCOTT  
LAW &  
GREENSPAN**  
*engineers*



NO SCALE

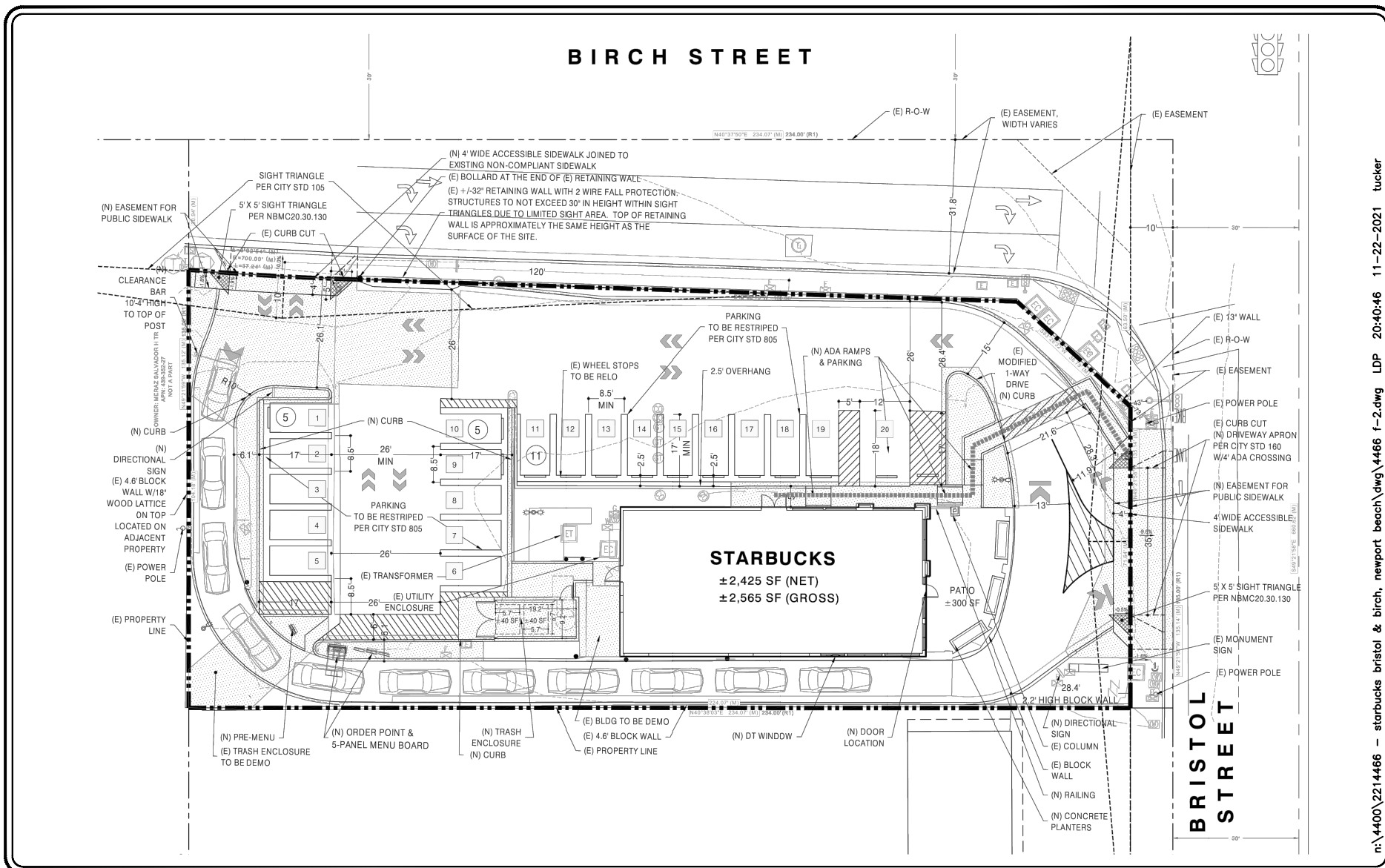
SOURCE: GOOGLE

**KEY**

  = PROJECT SITE

# FIGURE 1

**VICINITY MAP**  
STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH

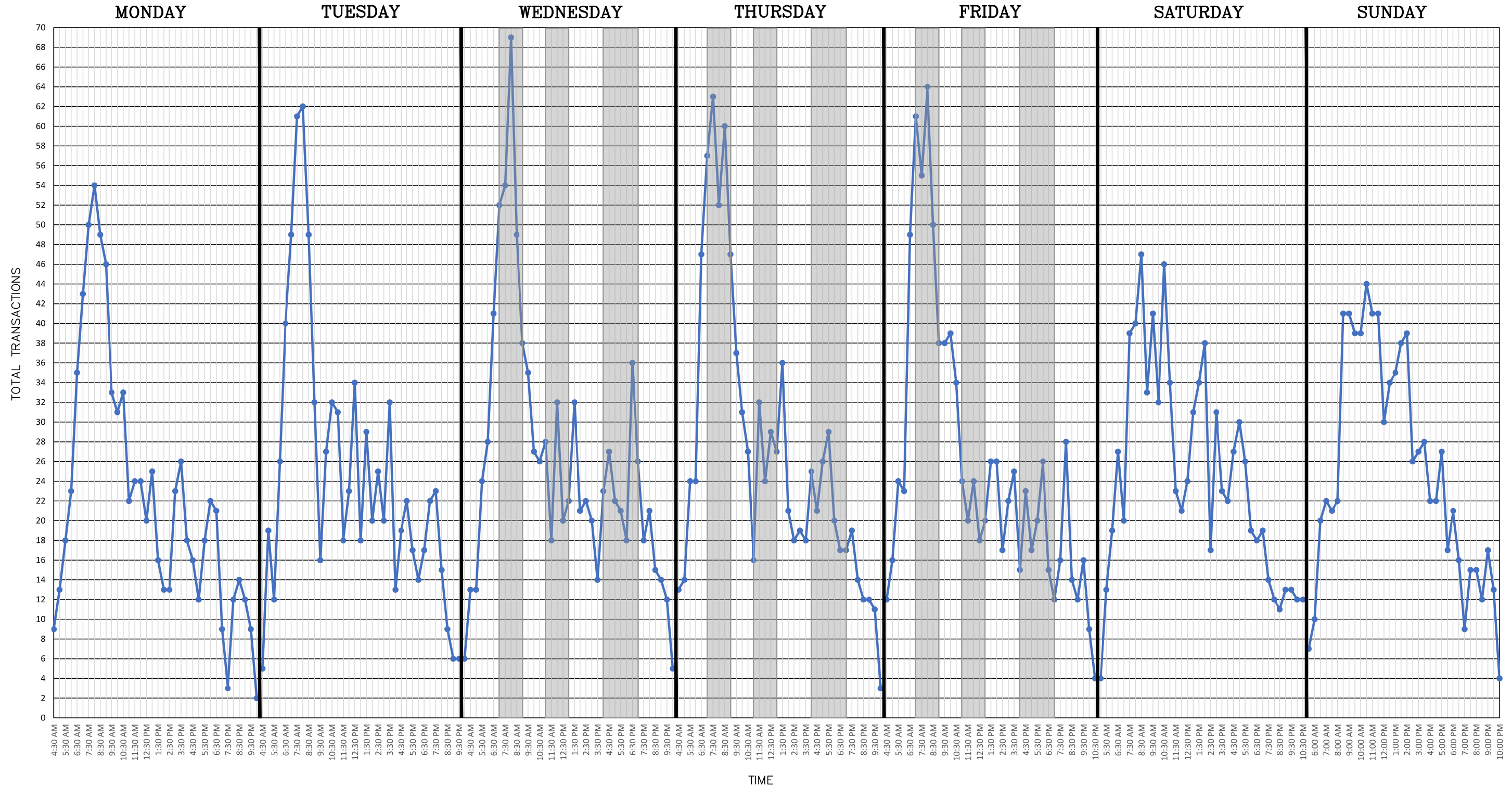


SOURCE: GREENBERG FARROW

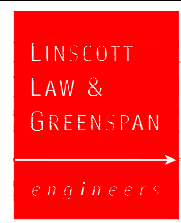
FIGURE 2

**PROPOSED DRIVE-THRU DESIGN**  
STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH



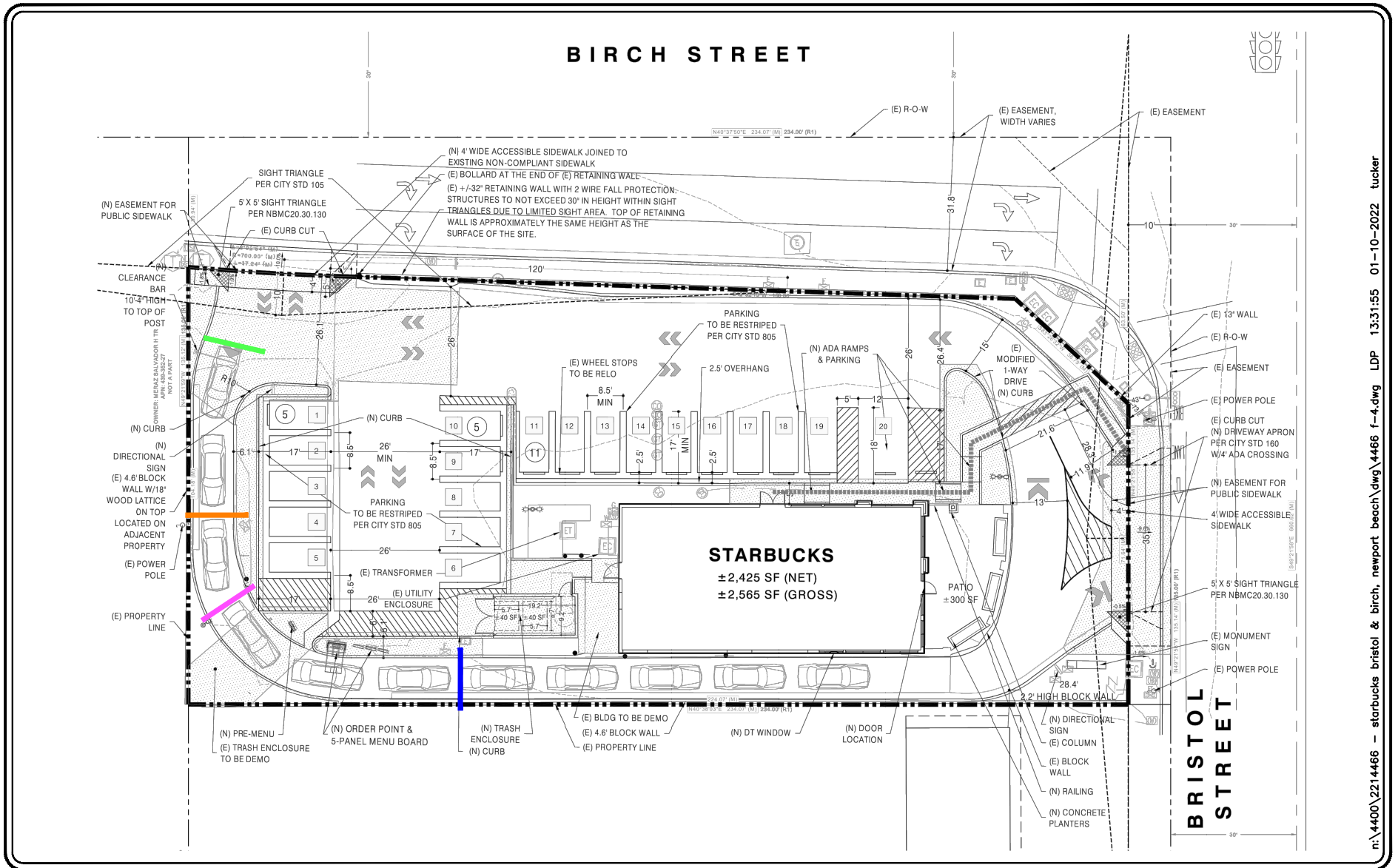


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**FIGURE 3**

**HOURLY TRANSACTION PROFILE**  
**STARBUCKS #23316 – 1224 N. HARBOR BLVD, ANAHEIM, CA**  
 STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH



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engineers



NO SCALE

SOURCE: GREENBERG FARROW  
KEY

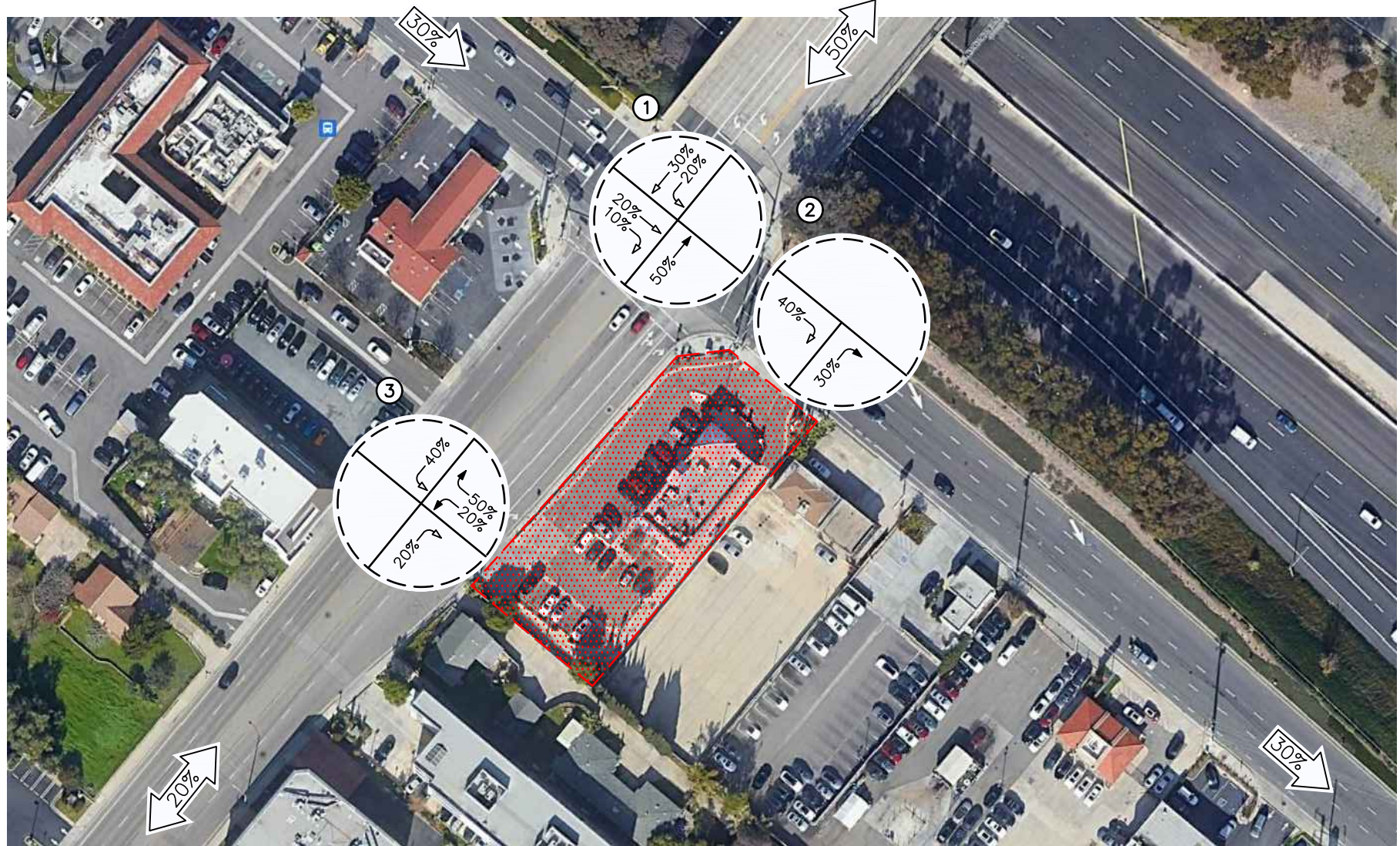
- = AVERAGE QUEUE
- = 85th PERCENTILE QUEUE
- = 95th PERCENTILE QUEUE
- = MAXIMUM QUEUE

**FIGURE 4**

**PROPOSED DRIVE-THROUGH LANE STACKING REQUIREMENTS**

**STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH**





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LINSCOTT  
LAW &  
GREENSPAN

engineers



NO SCALE

SOURCE: GOOGLE

#### KEY

- ← = INBOUND PERCENTAGE
- = OUTBOUND PERCENTAGE
- = PROJECT SITE

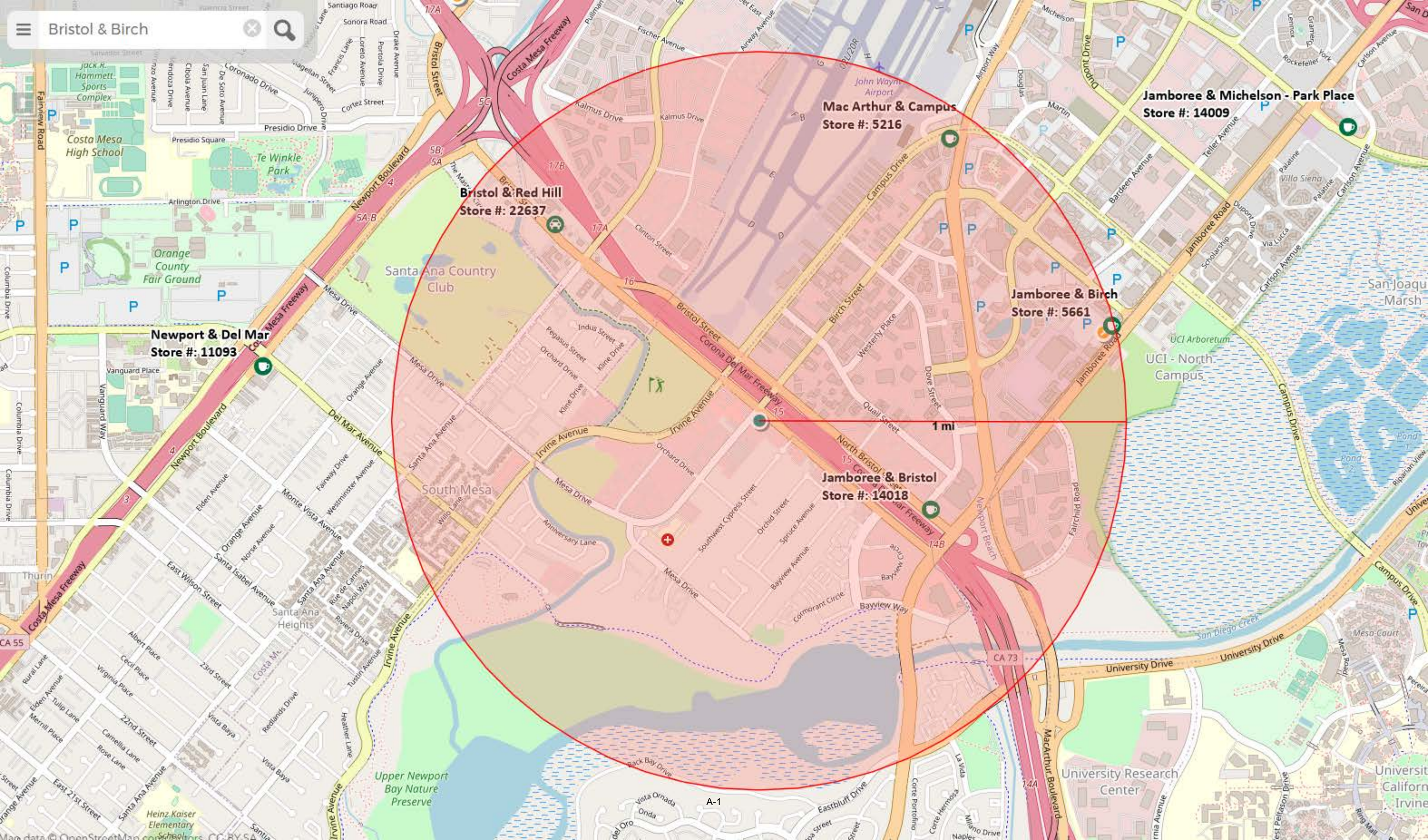
## FIGURE 5

**PROJECT TRAFFIC DISTRIBUTION PATTERN**  
STARBUCKS BRISTOL & BIRCH, NEWPORT BEACH

## APPENDIX A

### EXISTING STARBUCKS LOCATIONS





**Newport & Del Mar**  
Store #: 11093

**Bristol & Red Hill**  
Store #: 22637

**MacArthur & Campus**  
Store #: 5216

**Jamboree & Michelson - Park Place**  
Store #: 14009

**Jamboree & Birch**  
Store #: 5661

**Jamboree & Bristol**  
Store #: 14018

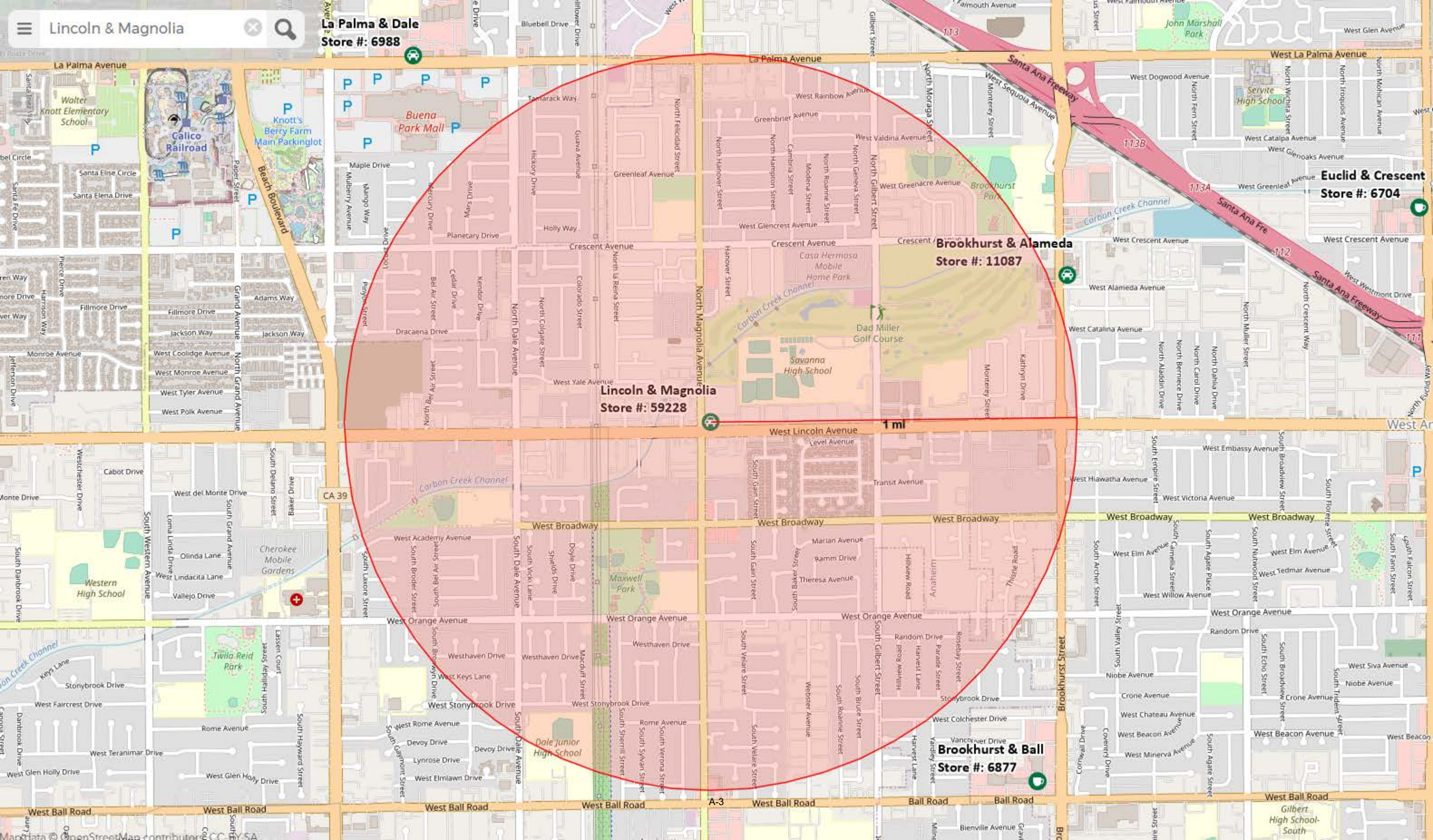
1 mi







La Palma & Dale  
Store #: 6988



Lincoln & Magnolia  
Store #: 59228

Brookhurst & Alameda  
Store #: 11087

Euclid & Crescent  
Store #: 6704

Brookhurst & Ball  
Store #: 6877



HARBOR BLVD.

SITE SCHEDULE - "S"					
DESIGN ID	COUNT	DESCRIPTION	RESP.	BULB	COMMENTS
ADL50	1	BOLLARD	GC		
EXTERIOR MENU					
14116	1	MENU BOARD - DT DIGITAL ORDER SCREEN - FLAT BLACK MT0028	VD		
14119	1	MENU BOARD - DT 5 PANEL FREESTANDING - 100X65IN 2745X1650MM - FLAT BLACK MT0028	VD		
14120	1	MENU BOARD - DT PRE MENU FREESTANDING - 36X65IN 915X1650MM - FLAT BLACK MT0028	VD		
OTHER					
10020	1	DT WINDOW SHELF - 48IN 1205MM - SST	GC		MFR: READY ACCESS
10042	2	BIKE RACK HOOP	VD		
14093	1	DT WAYFINDING GRAPHIC ORDER POINT STAR PATTERN - WHITE	GC		MFR: ENNIS FLINT
14103	2	BOLLARD NONILLUMINATED SQUARE - FLAT BLACK MT0028	VD		

SITE SCHEDULE - "S"					
DESIGN ID	COUNT	DESCRIPTION	RESP.	BULB	COMMENTS
14163	1	DT ORDER POINT CANOPY FREESTANDING - FLAT BLACK MT0028	VD		
ADL1	1	DT WAYFINDING GRAPHIC EXIT ARROW - WHITE	GC		MFR: ENNIS FLINT
ADL2	1	DT WAYFINDING GRAPHIC ENTRY ARROW - GREEN	GC		MFR: ENNIS FLINT
ADL3	3	DT WAYFINDING GRAPHIC DIRECTIONAL DOUBLE ARROW - GREEN	GC		MFR: PAVEMENT STENCIL (RE-USABLE), REFERENCE PAINT P936
ADL4	3	DT WAYFINDING GRAPHIC DIRECTIONAL DOUBLE ARROW - WHITE	GC		MFR: PAVEMENT STENCIL (RE-USABLE), REFERENCE PAINT P935
UMBRELLA					
10065	6	UMBRELLA - 72IN 1805MM - LOGO	3PL		
12139	6	UMBRELLA - BASE FOR CANOPY - SILVER MT0021	3PL		

## SHEET NOTES #

- DISTRIBUTE ALL UNDER SLAB UTILITIES (ELECTRICAL, WATER, GAS AND SANITARY WASTE, DRAINS AND VENTING) TO THE FINAL POINT OF CONNECTION.
- FURNISH AND INSTALL AN APPROVED, TESTED AND CERTIFIED BACKFLOW PREVENTION ASSEMBLY, IF REQUIRED BY APPLICABLE CODES.
- PROVIDE ADEQUATE LIGHTING FOR EXTERIOR PATIO.
- PROVIDE KEYED, FROST-FREE RECESSED HOSE BIB ACCESSIBLE TO TRASH ENCLOSURE. SEE CIVIL DRAWINGS
- INSTALL BIKE RACK, SEE DETAIL 7/G-0007. 2 RACK = 4 BICYCLE SPACES.
- PROVIDE FLASHED ROOF PENETRATIONS FOR ALL TENANT'S VENTS INCLUDING TENANT SUPPLIED WATER HEATER AND RTU.
- PROVIDE SLEEPERS, CURBS AND PADS TO SUPPORT TENANT SUPPLIED ROOF TOP EQUIPMENT.
- PERMIT, FURNISH AND INSTALL BUILDING MONITORING AND FIRE PROTECTION ALARM SYSTEM.
- FURNISH AND INSTALL ALL PLUMBING VENTS FROM THE ROOF THROUGH FLASHING TO A LOCATION ABOVE THE PROPOSED TENANT CEILING IN THE PREMISES.
- PROVIDE SIDEWALKS AND PATIO.
- FURNISH AND INSTALL HVAC ROOF TOP UNITS OR SPLIT SYSTEMS INCLUDING ALL ELECTRICAL PLUMBING CONNECTIONS AND THERMOSTAT/HVAC CONTROLS.
- PROVIDE DUCTED FRESH AIR INTAKE INTEGRATED WITH AIR HANDLING SYSTEM.
- FURNISH AND INSTALL ALL RIGID "HARD" DUCTWORK.
- FURNISH AND INSTALL ELECTRICAL PANEL, SEE ELECTRICAL DRAWINGS.
- FURNISH AND INSTALL PARKING LOT POLE LIGHTS IN ACCORDANCE WITH FINAL APPROVED SITE PLAN, AT A MINIMUM OF 1.5 FTC AROUND THE BUILDING AND SITE PERIMETER. EXTERIOR BUILDING LIGHTING TO INCLUDE INGRESS AND EGRESS LIGHTS.
- FURNISH AND INSTALL ALL UNDERGROUND ELECTRICAL CONDUITS, LABELED WITH PULL STRINGS FROM THE ELECTRICAL PANELS TO THE FOLLOWING EXTERIOR DRIVE-THRU EQUIPMENT:  
  
ONE (1) 1" CONDUIT TO MONUMENT  
ONE (1) 1" CONDUIT TO PRE-ORDER MENU BOARD TO ELECTRICAL PANELS.  
ONE (1) 1" CONDUIT TO PRE-MENU/MENU BOARD FOR FUTURE;  
ONE (1) 1" CONDUIT FROM THE MENU BOARD TO ELECTRICAL PANELS;  
TWO (2) 1" CONDUITS FROM OCS/SPEAKER POST TO DRIVE THRU WINDOW;  
ONE (1) 1" CONDUIT FROM ALL DIRECTIONAL SIGNS TO ELECTRICAL PANELS;  
ONE (1) CONDUIT FROM GREEN/LIVING WALL FEATURE CONTROL BOX TO ELECTRICAL PANELS.  
ONE (1) SPARE 1" CONDUIT TO BE RUN THROUGH FOUNDATION WALL OUT REAR OF BUILDING, CAPPED AND TERMINATED ABOVE CEILING. VERIFY LOCATIONS FOR STUB UP WITH TENANT'S REPRESENTATIVE.
- FURNISH AND INSTALL CONCRETE FOOTINGS AND ANCHOR BOLTS FOR THE FOLLOWING ITEMS:  
PREVIEW MENU BOARD  
ORDER MENU BOARD  
SPEAKER POST/ORDER CONFIRMATION SYSTEM (OCS)  
DIRECTIONAL SIGNAGE  
HEIGHT RESTRICTION BAR  
MONUMENT SIGN
- FURNISH AND INSTALL 6" DIAMETER STEEL PIPE BOLLARDS, FILLED WITH CONCRETE. PAINT TO MATCH COLOR OF DARK BRONZE POWDER COATED RAILING.
- FURNISH AND INSTALL DARK BRONZE POWDER COATED RAILING, SEE DETAILS 8 & 9/G-0005
- 2 FEET OVERHANG
- ACCESSIBLE PARKING SPACE IDENTIFICATION SIGN.
- RECYCLING COLLECTION AREA.
- TRASH COLLECTION AREA
- ACCESSIBLE ROUTE
- INSTALL "UNAUTHORIZED VEHICLE..." SIGN, SEE CIVIL DRAWINGS.
- INSTALL ADA DIRECTION SIGN LEADING TO AN ACCESSIBLE ROUTE OF TRAVEL FROM PUBLIC WAY
- FURNISH AND INSTALL TRANSFORMER. SEE CIVIL AND ELECTRICAL DRAWINGS.
- INSTALL NON ILLUMINATED SQUARE BOLLARD.

## ARCHITECTURAL SITE PLAN NOTES

- A. REFER TO EXTERIOR ELEVATIONS FOR BUILDING SIGNAGE LOCATION AND DESIGN ID. REFER TO ELECTRICAL PLANS FOR ELECTRICAL REQUIREMENTS.
- B. LANDSCAPING TO BE PROVIDED PER ZONING CODE AND LEED REQUIREMENTS.
- C. DRIVE-THRU EQUIPMENT INCLUDING VEHICLE DETECTION LOOP, WIRELESS COMMUNICATION, AND MONITORS COORDINATED BY CONSTRUCTION MANAGER. REFER TO ELECTRICAL SPECIFICATIONS FOR ELECTRICAL REQUIREMENTS.
- D. CONTRACTOR IS RESPONSIBLE FOR PROVIDING SITE WORK, INCLUDING NEW BOLLARDS, FOOTING AND CONDUIT FOR DRIVE-THRU SIGNAGE. COORDINATE WITH CONSTRUCTION MANAGER.
- E. ALL PARKING AND DRIVE-THRU STRIPING TO BE PROVIDED.
- F. TRASH ENCLOSURE TO BE LOCKABLE USING STEEL GATES OR CHAIN.
- G. PROVIDE 6" (150MM) THICK CONCRETE PAVING THE LENGTH OF THE DRIVE-THRU LANE, EXTENT TO INCLUDE DRIVE-THRU WINDOW STANDING PAD.
- H. GENERAL CONTRACTOR TO APPLY CONCRETE SEALER TO ALL EXTERIOR CONCRETE PATIO AND WALKWAY SURFACES.
- J. CONSTRUCTION IN THE PUBLIC RIGHT OF WAY AND PROJECTION BEYOND THE PROPERTY LINES OR INTO THE ALLEYS SHALL COMPLY WITH COUNTY OF LOS ANGELES BUILDING CODE CHAPTER 32.
- K. PEDESTRIANS SHALL BE PROTECTED DURING CONSTRUCTION, REMODELING AND DEMOLITION ACTIVITIES AS REQUIRED BY COUNTY OF LOS ANGELES BUILDING CODE CHAPTER 33. (3306)



**STARBUCKS COFFEE COMPANY**  
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SEATTLE, WASHINGTON 98134  
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## GreenbergFarrow

19000 MacArthur Blvd, Suite 250  
Irvine, CA 92612  
949-296-0450

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Revision Schedule				
Rev	Date	By	Description	
P	9/22/14		CHECK SET	
Q	9/29/14		CITY SUBMITTAL	
2	11/20/14		PLAN CHECK CORR.	

PROJECT NAME:  
**HARBOR & 91**

PROJECT ADDRESS:  
**1224 N. HARBOR BLVD  
ANAHEIM, CA 92801  
ORANGE COUNTY**

### PROFESSIONAL SEAL

STORE #: 23316  
PROJECT #: 60124-001  
CONCEPT: NCS  
PALETTE: RUSTIC CONTEMPO  
ISSUE DATE: AS NOTED IN REVISION SCHEDULE  
DESIGN MANAGER: SLADE BLANCHARD  
LEED® AP: JOHN HARRISON  
PRODUCTION DESIGNER: GREENBERGFARROW  
CHECKED BY: DOUGLAS S. COUPER

SHEET TITLE:  
**ARCHITECTURAL SITE PLAN**

SCALE: As indicated

SHEET NUMBER:  
**G-0004**

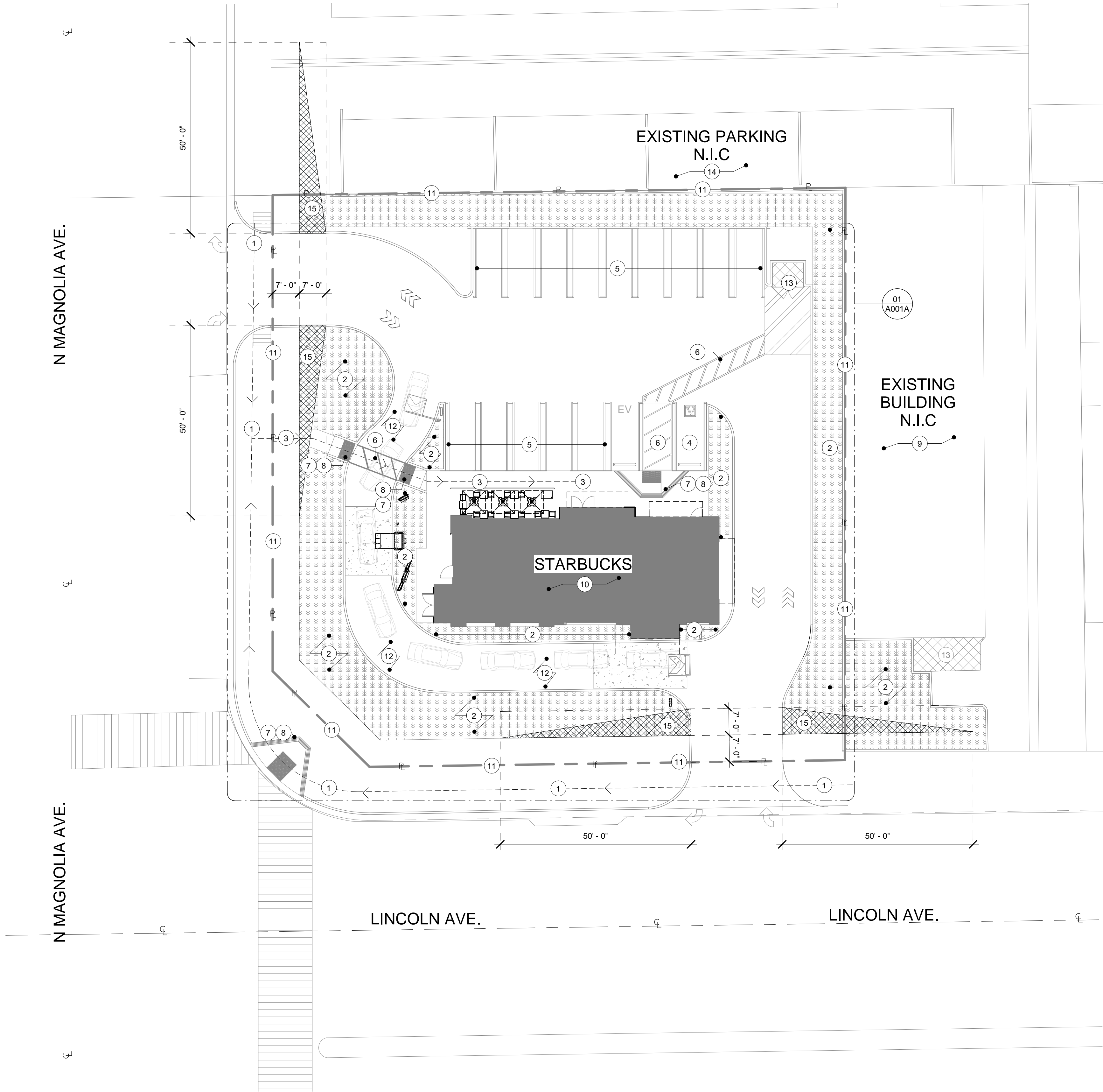
## 1 ARCHITECTURAL SITE PLAN

Scale: 3/32" = 1'-0"

0 8' 16' 24'

Scale: 3/32" = 1'-0"

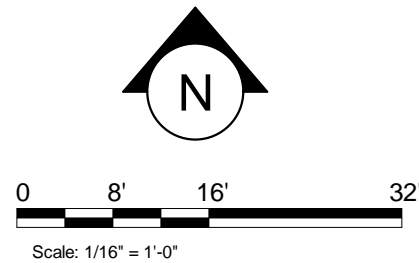




1ST FLOOR - ARCHITECTURAL OVERALL SITE PLAN

Scale: 1/16" = 1'-0"

01



### GENERAL NOTES

- REFER TO EXTERIOR ELEVATIONS ON SHEET A201 FOR BUILDING SIGNAGE LOCATION AND DESIGN ID. REFER TO ELECTRICAL PLANS FOR ELECTRICAL REQUIREMENTS.
- LANDSCAPING TO BE PROVIDED PER ZONING CODE AND SUSTAINABILITY REQUIREMENTS.
- DRIVE-THRU EQUIPMENT INCLUDING VEHICLE DETECTION LOOP, WIRELESS COMMUNICATION AND MONITORS SHALL BE COORDINATED BY STARBUCKS CONSTRUCTION MANAGER. REFER TO ELECTRICAL DRAWINGS FOR ADDITIONAL REQUIREMENTS.
- PROVIDE 6" (150MM) THICK CONCRETE PAVING THE LENGTH OF THE DRIVE-THRU LANE, EXTENT TO INCLUDE DRIVE-THRU ENTRY POINT THROUGH WINDOW STANDING PAD.
- GENERAL CONTRACTOR TO APPLY CONCRETE SEALER TO ALL EXTERIOR CONCRETE PATIO AND WALKWAY SURFACES.
- GENERAL CONTRACTOR TO VERIFY ALL EXISTING ELEVATIONS AND BUILDING CONDITIONS IN FIELD PRIOR TO START OF CONSTRUCTION.
- PROVIDE DETECTABLE WARNING (IF APPLICABLE PER LOCAL CODE) AT TRANSITION FROM SIDEWALK TO DRIVE AISLE.
- ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL HAVE SURFACE SLOPE NOT TO EXCEED 2% IN ALL DIRECTIONS.
- REFER TO ELECTRICAL DRAWINGS FOR SITE RELATED ELECTRICAL WORK.
- SCRAPE AND REPAINT ALL EXISTING PAINTED SITE FEATURES, INCLUDING, BUT NOT LIMITED TO CURBS, BOLLARDS, RAILINGS AND SITE LIGHTING BASES.
- SEE SHEET A002 FOR ARCHITECTURAL SITE DETAILS.

### KEYNOTES

- (E) PUBLIC RIGHT OF WAY TO REMAIN
- (E) LANDSCAPING AREA TO REMAIN. (UNDER SHELL PERMIT#BLD2020-02247)
- (N) ACCESSIBLE PATH OF TRAVEL FOR PUBLIC WRIGHT OF WAY. (UNDER SHELL PERMIT#BLD2020-02247)
- (E) ACCESSIBLE PARKING STALL TO REMAIN. (UNDER SHELL PERMIT#BLD2020-02247)
- (E) PARKING STALL, TYP. TO REMAIN (UNDER SHELL PERMIT#BLD2020-02247)
- (E) ACCESSIBLE STRIPING TO REMAIN (UNDER SHELL PERMIT#BLD2020-02247)
- (E) CURB CUT TO REMAIN (UNDER SHELL PERMIT#BLD2020-02247)
- (E) DETECTABLE WARNING, SEE 12/G011 (UNDER SHELL PERMIT#BLD2020-02247)
- (E) BUILDING TO REMAIN N.I.C.
- (N)PROPOSED 2,000 SQ. F.T STARBUCKS TENANT IMPROVEMENT
- ALL PROPERTY LINES ARE SHOWN ON THIS SITE PLAN AND ARE EXISTING TO REMAIN. NO CHANGE
- EXISTING DRIVE THRU LANE TO REMAIN. (UNDER SHELL PERMIT#BLD2020-02247)
- EXISTING TRASH ENCLOSURE TO REMAIN. (UNDER SHELL PERMIT#BLD2020-02247)
- EXISTING SHADED PARKING TO REMAIN.
- HATCHED REGION REPRESENTS UNOBSTRUCTED LINE OF SIGHT AT DRIVEWAYS PER CITY OF ANAHEIM PUBLIC WORKS STANDARD #115-B. ANY OBSTRUCTING OBJECTS NOT TO EXCEED 24" MAX HEIGHT. (UNDER SHELL PERMIT #BLD2020-02247)

### OVERALL SITE PLAN LEGEND

- SCOPE OF WORK
- NIC
- LANDSCAPE
- TRASH ENCLOSURE



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STARBUCKS TEMPLATE VERSION: i2020-01-31.1



**ADDRESS:** 43180 BUSINESS PARK DR.  
SUITE: 203  
TEMECULA, CA92590  
(714)331-6114  
**PHONE:**  
**EMAIL:** info@urbandwellarchitects.com



PROJECT NAME:  
**LINCOLN & MAGNOLIA**

PROJECT ADDRESS:  
2595 W Lincoln Avenue  
Anaheim, CA 92801 USA

STORE #: 59228  
PROJECT #: 84843-001  
ISSUE DATE: 06.18.2020  
DESIGN MANAGER: GIANCARLO RENELLA  
PRODUCTION DESIGNER: ROGINA KHALED  
CHECKED BY: MAIKEL FARAGALLA

Revision Schedule			
Rev	Date	By	Description

SHEET TITLE:  
**ARCHITECTURAL  
OVERALL SITE PLAN**  
SCALE: AS SHOWN

SHEET NUMBER:  
**A001**

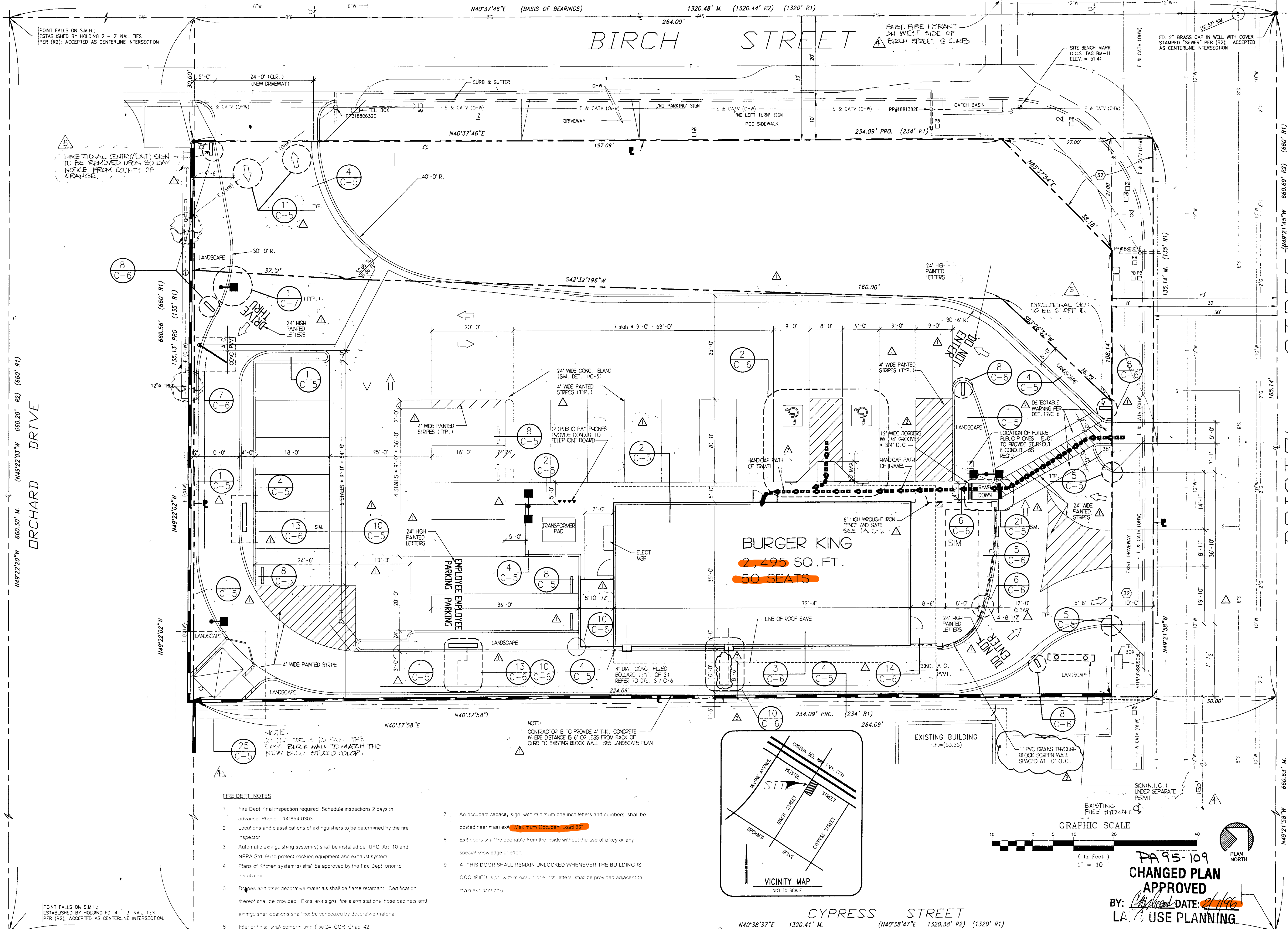
Permitted Set 11-6-2020



## **APPENDIX B**

### **COUNTY OF ORANGE – APPROVED SITE PLAN (PA95-109 CHANGED PLAN)**

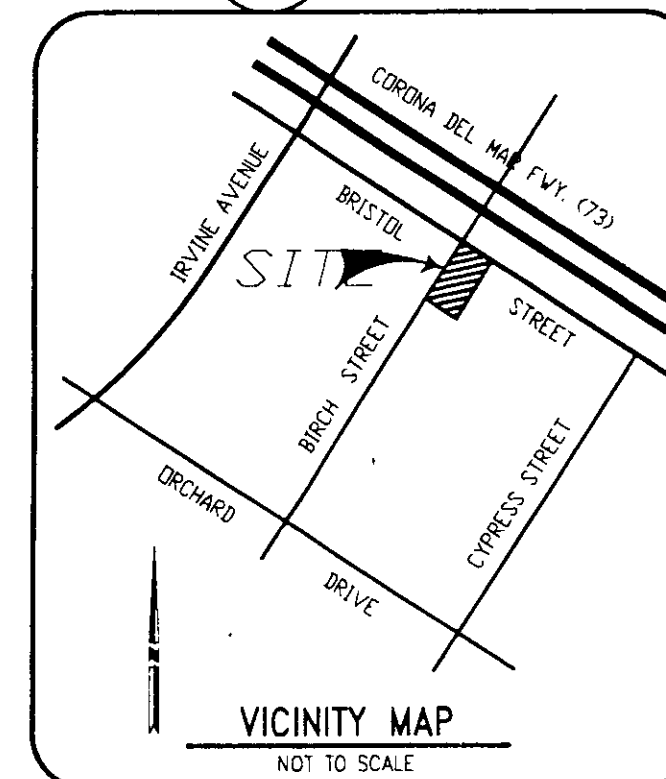
N49°22'20"W 660.30' M. (N49°22'03"W 660.20' R2) (660' R1)  
ORCHARD DRIVE



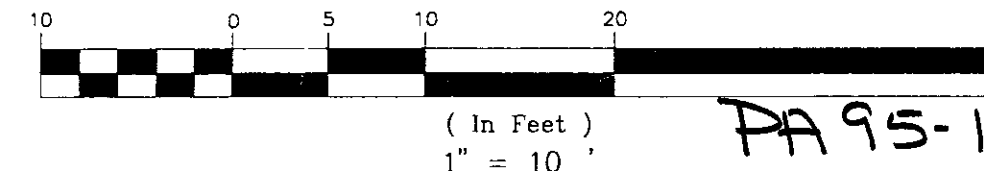
#### FIRE DEPT. NOTES

1. Fire Dept. final inspection required. Schedule inspections 2 days in advance. Phone: 714-854-0303.
2. Locations and classifications of extinguishers to be determined by the fire inspector.
3. Automatic extinguishing system(s) shall be installed per UFC, Art. 10 and NFPA Std. 96 to protect cooking equipment and exhaust system.
4. Plans of Kitchen system(s) shall be approved by the Fire Dept. prior to installation.
5. Drapes and other decorative materials shall be flame retardant. Certification thereof shall be provided. Exits, exit signs, fire alarm stations, hose cabinets and extinguisher locations shall not be concealed by decorative material.
6. Interior finish shall conform with The 24 CCR Chap. 42.

7. An occupant capacity sign, with minimum one inch letters and numbers, shall be posted near main exit. **Maximum Occupant Load 55**.
8. Exit doors shall be operable from the inside without the use of a key or any special knowledge or effort.
9. THIS DOOR SHALL REMAIN UNLOCKED WHENEVER THE BUILDING IS OCCUPIED. Sign, with minimum one inch letters, shall be provided adjacent to main exit door only.



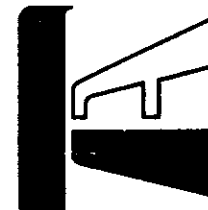
#### GRAPHIC SCALE



PA 95-109  
CHANGED PLAN  
APPROVED

BY: *[Signature]* DATE: 8/14/95  
LA: USE PLANNING

**TARLOS & ASSOCIATES**  
ARCHITECTS - ENGINEERS  
AIA - A.R.A.  
17802 MITCHELL  
IRVINE, CA. 92714  
FAX (714) 250-1678  
(714) 250-4117



**BURGER KING**  
222 BRISTOL AVENUE  
NEWPORT BEACH, CA

#### SITE PLAN

REVISION	DATE	JOB NO.
1. BLDG. DEPT. CORR. 9/14/95	8/14/95	572025
2. BLDG. DEPT. CORR. 10/23/95		
3. OWNER REVISION. 10/23/95		
4. FIRE DEPT. NOTES & COMMENTS 10/20/95		
5. BLDG. COMMENTS 10/20/95		

DRAWING NO.

**C-3**

A/94  
B/C/M/C3

## **APPENDIX C**

### **DRIVE-THROUGH QUEUING STUDY DATA**



Starbucks  
1224 N Harbor Blvd  
Anaheim, CA 92801  
9/29/2021 Wednesday AM Pk Hr

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
7:00 AM	2	0	2	7:30 AM	3	3	6	8:00 AM	4	2	6	8:30 AM	2	7	9
7:01 AM	2	0	2	7:31 AM	2	3	5	8:01 AM	2	1	3	8:31 AM	3	7	10
7:02 AM	1	1	2	7:32 AM	2	3	5	8:02 AM	1	1	2	8:32 AM	2	7	9
7:03 AM	1	0	1	7:33 AM	2	2	4	8:03 AM	1	3	4	8:33 AM	2	6	8
7:04 AM	1	0	1	7:34 AM	1	1	2	8:04 AM	1	4	5	8:34 AM	2	5	7
7:05 AM	1	0	1	7:35 AM	1	3	4	8:05 AM	1	5	6	8:35 AM	2	3	5
7:06 AM	0	1	1	7:36 AM	1	5	6	8:06 AM	3	7	10	8:36 AM	1	4	5
7:07 AM	0	2	2	7:37 AM	1	6	7	8:07 AM	4	6	10	8:37 AM	1	4	5
7:08 AM	1	2	3	7:38 AM	2	6	8	8:08 AM	4	7	11	8:38 AM	3	4	7
7:09 AM	2	2	4	7:39 AM	3	4	7	8:09 AM	4	7	11	8:39 AM	3	2	5
7:10 AM	3	1	4	7:40 AM	1	4	5	8:10 AM	3	8	11	8:40 AM	3	2	5
7:11 AM	2	1	3	7:41 AM	1	5	6	8:11 AM	3	9	12	8:41 AM	1	3	4
7:12 AM	1	1	2	7:42 AM	1	5	6	8:12 AM	1	9	10	8:42 AM	2	3	5
7:13 AM	1	3	4	7:43 AM	1	5	6	8:13 AM	2	10	12	8:43 AM	1	4	5
7:14 AM	1	3	4	7:44 AM	3	6	9	8:14 AM	2	10	12	8:44 AM	2	3	5
7:15 AM	1	3	4	7:45 AM	2	7	9	8:15 AM	1	10	11	8:45 AM	2	4	6
7:16 AM	2	2	4	7:46 AM	3	7	10	8:16 AM	1	10	11	8:46 AM	1	6	7
7:17 AM	1	1	2	7:47 AM	1	7	8	8:17 AM	2	10	12	8:47 AM	3	5	8
7:18 AM	1	2	3	7:48 AM	3	8	11	8:18 AM	3	10	13	8:48 AM	4	6	10
7:19 AM	1	2	3	7:49 AM	3	7	10	8:19 AM	3	9	12	8:49 AM	4	4	8
7:20 AM	3	2	5	7:50 AM	1	7	8	8:20 AM	3	8	11	8:50 AM	4	3	7
7:21 AM	3	1	4	7:51 AM	2	6	8	8:21 AM	3	7	10	8:51 AM	4	3	7
7:22 AM	1	4	5	7:52 AM	2	5	7	8:22 AM	3	6	9	8:52 AM	4	4	8
7:23 AM	2	4	6	7:53 AM	2	4	6	8:23 AM	4	6	10	8:53 AM	4	4	8
7:24 AM	2	4	6	7:54 AM	2	3	5	8:24 AM	2	6	8	8:54 AM	3	4	7
7:25 AM	2	5	7	7:55 AM	1	4	5	8:25 AM	1	6	7	8:55 AM	2	5	7
7:26 AM	3	5	8	7:56 AM	1	5	6	8:26 AM	2	5	7	8:56 AM	0	6	6
7:27 AM	2	4	6	7:57 AM	2	4	6	8:27 AM	4	3	7	8:57 AM	1	7	8
7:28 AM	2	3	5	7:58 AM	4	3	7	8:28 AM	3	4	7	8:58 AM	1	7	8
7:29 AM	2	2	4	7:59 AM	4	2	6	8:29 AM	2	4	6	8:59 AM	0	7	7



Starbucks  
1224 N Harbor Blvd  
Anaheim, CA 92801  
9/29/2021 Wednesday MD Pk Hr

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
11:00 AM	4	2	6	11:30 AM	2	1	3	12:00 PM	4	5	9	12:30 PM	4	2	6
11:01 AM	4	3	7	11:31 AM	1	2	3	12:01 PM	4	3	7	12:31 PM	4	0	4
11:02 AM	4	3	7	11:32 AM	2	2	4	12:02 PM	4	3	7	12:32 PM	3	0	3
11:03 AM	4	3	7	11:33 AM	2	1	3	12:03 PM	4	4	8	12:33 PM	2	0	2
11:04 AM	3	3	6	11:34 AM	2	0	2	12:04 PM	4	5	9	12:34 PM	2	0	2
11:05 AM	3	2	5	11:35 AM	2	1	3	12:05 PM	4	5	9	12:35 PM	0	1	1
11:06 AM	3	3	6	11:36 AM	1	1	2	12:06 PM	3	5	8	12:36 PM	1	1	2
11:07 AM	3	4	7	11:37 AM	1	1	2	12:07 PM	2	6	8	12:37 PM	1	0	1
11:08 AM	3	4	7	11:38 AM	1	0	1	12:08 PM	2	6	8	12:38 PM	1	0	1
11:09 AM	3	3	6	11:39 AM	1	0	1	12:09 PM	3	6	9	12:39 PM	1	1	2
11:10 AM	3	3	6	11:40 AM	1	2	3	12:10 PM	4	5	9	12:40 PM	2	2	4
11:11 AM	3	3	6	11:41 AM	1	3	4	12:11 PM	4	5	9	12:41 PM	2	1	3
11:12 AM	3	1	4	11:42 AM	2	2	4	12:12 PM	4	5	9	12:42 PM	3	0	3
11:13 AM	3	1	4	11:43 AM	3	1	4	12:13 PM	4	5	9	12:43 PM	3	1	4
11:14 AM	3	0	3	11:44 AM	4	0	4	12:14 PM	4	4	8	12:44 PM	3	1	4
11:15 AM	2	1	3	11:45 AM	3	0	3	12:15 PM	4	5	9	12:45 PM	2	2	4
11:16 AM	2	1	3	11:46 AM	2	0	2	12:16 PM	4	5	9	12:46 PM	2	4	6
11:17 AM	2	0	2	11:47 AM	0	0	0	12:17 PM	4	5	9	12:47 PM	3	4	7
11:18 AM	1	1	2	11:48 AM	0	0	0	12:18 PM	4	5	9	12:48 PM	3	4	7
11:19 AM	0	1	1	11:49 AM	0	2	2	12:19 PM	3	6	9	12:49 PM	3	5	8
11:20 AM	1	1	2	11:50 AM	1	1	2	12:20 PM	3	6	9	12:50 PM	3	5	8
11:21 AM	1	1	2	11:51 AM	1	2	3	12:21 PM	3	5	8	12:51 PM	4	4	8
11:22 AM	2	1	3	11:52 AM	2	4	6	12:22 PM	3	5	8	12:52 PM	4	4	8
11:23 AM	1	0	1	11:53 AM	1	4	5	12:23 PM	3	5	8	12:53 PM	4	4	8
11:24 AM	0	1	1	11:54 AM	3	4	7	12:24 PM	4	4	8	12:54 PM	4	4	8
11:25 AM	1	1	2	11:55 AM	3	5	8	12:25 PM	4	3	7	12:55 PM	4	3	7
11:26 AM	1	0	1	11:56 AM	3	5	8	12:26 PM	4	3	7	12:56 PM	4	3	7
11:27 AM	1	1	2	11:57 AM	4	5	9	12:27 PM	4	3	7	12:57 PM	4	4	8
11:28 AM	1	2	3	11:58 AM	4	5	9	12:28 PM	4	3	7	12:58 PM	4	4	8
11:29 AM	2	1	3	11:59 AM	3	5	8	12:29 PM	4	3	7	12:59 PM	4	4	8

Starbucks  
1224 N Harbor Blvd  
Anaheim, CA 92801  
9/29/2021 Wednesday PM Pk Hr

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
4:00 PM	0	1	1	4:30 PM	4	7	11	5:00 PM	4	2	6	5:30 PM	3	2	5
4:01 PM	1	0	1	4:31 PM	4	6	10	5:01 PM	4	1	5	5:31 PM	3	2	5
4:02 PM	0	0	0	4:32 PM	4	6	10	5:02 PM	3	1	4	5:32 PM	3	1	4
4:03 PM	0	2	2	4:33 PM	4	6	10	5:03 PM	4	1	5	5:33 PM	3	0	3
4:04 PM	1	1	2	4:34 PM	4	5	9	5:04 PM	3	1	4	5:34 PM	2	1	3
4:05 PM	2	1	3	4:35 PM	4	4	8	5:05 PM	4	1	5	5:35 PM	3	1	4
4:06 PM	2	2	4	4:36 PM	4	5	9	5:06 PM	3	0	3	5:36 PM	2	0	2
4:07 PM	3	1	4	4:37 PM	4	4	8	5:07 PM	2	0	2	5:37 PM	0	2	2
4:08 PM	3	2	5	4:38 PM	4	4	8	5:08 PM	2	0	2	5:38 PM	1	3	4
4:09 PM	4	2	6	4:39 PM	4	4	8	5:09 PM	2	0	2	5:39 PM	2	3	5
4:10 PM	4	1	5	4:40 PM	4	3	7	5:10 PM	2	0	2	5:40 PM	3	2	5
4:11 PM	4	1	5	4:41 PM	4	2	6	5:11 PM	1	1	2	5:41 PM	3	2	5
4:12 PM	3	0	3	4:42 PM	4	1	5	5:12 PM	0	3	3	5:42 PM	3	2	5
4:13 PM	3	3	6	4:43 PM	4	1	5	5:13 PM	1	3	4	5:43 PM	4	3	7
4:14 PM	4	3	7	4:44 PM	4	1	5	5:14 PM	2	3	5	5:44 PM	4	2	6
4:15 PM	4	3	7	4:45 PM	4	3	7	5:15 PM	2	4	6	5:45 PM	4	1	5
4:16 PM	4	3	7	4:46 PM	4	4	8	5:16 PM	3	3	6	5:46 PM	4	1	5
4:17 PM	4	4	8	4:47 PM	4	6	10	5:17 PM	4	2	6	5:47 PM	4	2	6
4:18 PM	4	3	7	4:48 PM	4	6	10	5:18 PM	4	2	6	5:48 PM	4	1	5
4:19 PM	4	4	8	4:49 PM	4	6	10	5:19 PM	4	1	5	5:49 PM	4	1	5
4:20 PM	4	3	7	4:50 PM	4	5	9	5:20 PM	4	1	5	5:50 PM	4	0	4
4:21 PM	4	4	8	4:51 PM	4	5	9	5:21 PM	4	1	5	5:51 PM	4	1	5
4:22 PM	4	4	8	4:52 PM	4	5	9	5:22 PM	3	1	4	5:52 PM	4	1	5
4:23 PM	4	5	9	4:53 PM	4	5	9	5:23 PM	4	2	6	5:53 PM	4	1	5
4:24 PM	3	5	8	4:54 PM	4	5	9	5:24 PM	4	3	7	5:54 PM	4	1	5
4:25 PM	4	5	9	4:55 PM	4	4	8	5:25 PM	4	3	7	5:55 PM	4	1	5
4:26 PM	4	6	10	4:56 PM	4	3	7	5:26 PM	3	3	6	5:56 PM	3	1	4
4:27 PM	4	5	9	4:57 PM	4	3	7	5:27 PM	2	3	5	5:57 PM	3	1	4
4:28 PM	4	6	10	4:58 PM	4	3	7	5:28 PM	3	3	6	5:58 PM	3	1	4
4:29 PM	4	7	11	4:59 PM	4	3	7	5:29 PM	3	3	6	5:59 PM	2	1	3

Starbucks  
1224 N Harbor Blvd  
Anaheim, CA 92801  
9/29/2021 Wednesday PM Pk Hr

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
6:00 PM	2	1	3	6:30 PM	4	6	10
6:01 PM	2	0	2	6:31 PM	4	7	11
6:02 PM	2	1	3	6:32 PM	4	6	10
6:03 PM	2	0	2	6:33 PM	4	6	10
6:04 PM	2	0	2	6:34 PM	4	5	9
6:05 PM	1	0	1	6:35 PM	4	5	9
6:06 PM	1	1	2	6:36 PM	3	4	7
6:07 PM	1	1	2	6:37 PM	4	4	8
6:08 PM	0	1	1	6:38 PM	4	2	6
6:09 PM	1	1	2	6:39 PM	4	2	6
6:10 PM	1	0	1	6:40 PM	4	2	6
6:11 PM	1	0	1	6:41 PM	4	2	6
6:12 PM	0	0	0	6:42 PM	4	2	6
6:13 PM	0	1	1	6:43 PM	3	2	5
6:14 PM	1	1	2	6:44 PM	3	2	5
6:15 PM	1	1	2	6:45 PM	2	1	3
6:16 PM	2	1	3	6:46 PM	3	1	4
6:17 PM	1	0	1	6:47 PM	3	1	4
6:18 PM	1	2	3	6:48 PM	3	1	4
6:19 PM	1	2	3	6:49 PM	2	1	3
6:20 PM	2	3	5	6:50 PM	2	1	3
6:21 PM	3	7	10	6:51 PM	0	1	1
6:22 PM	3	6	9	6:52 PM	1	1	2
6:23 PM	3	5	8	6:53 PM	1	1	2
6:24 PM	4	5	9	6:54 PM	2	1	3
6:25 PM	4	4	8	6:55 PM	2	0	2
6:26 PM	4	5	9	6:56 PM	0	0	0
6:27 PM	4	5	9	6:57 PM	0	1	1
6:28 PM	4	5	9	6:58 PM	1	2	3
6:29 PM	4	4	8	6:59 PM	2	2	4

Starbucks  
1224 N Harbor Blvd  
Anaheim, CA 92801  
9/30/2021 Thursday AM Pk Hr

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
7:00 AM	4	2	6	7:30 AM	1	0	1	8:00 AM	2	2	4	8:30 AM	2	4	6
7:01 AM	4	2	6	7:31 AM	0	2	2	8:01 AM	3	1	4	8:31 AM	2	4	6
7:02 AM	3	4	7	7:32 AM	0	5	5	8:02 AM	3	1	4	8:32 AM	2	2	4
7:03 AM	2	4	6	7:33 AM	0	6	6	8:03 AM	2	2	4	8:33 AM	1	1	2
7:04 AM	1	4	5	7:34 AM	2	7	9	8:04 AM	1	2	3	8:34 AM	1	2	3
7:05 AM	1	4	5	7:35 AM	3	7	10	8:05 AM	1	2	3	8:35 AM	1	2	3
7:06 AM	3	2	5	7:36 AM	2	8	10	8:06 AM	0	2	2	8:36 AM	2	3	5
7:07 AM	2	2	4	7:37 AM	3	8	11	8:07 AM	2	2	4	8:37 AM	4	2	6
7:08 AM	3	2	5	7:38 AM	3	8	11	8:08 AM	3	2	5	8:38 AM	3	2	5
7:09 AM	3	3	6	7:39 AM	3	7	10	8:09 AM	2	1	3	8:39 AM	1	2	3
7:10 AM	3	3	6	7:40 AM	2	7	9	8:10 AM	1	1	2	8:40 AM	1	3	4
7:11 AM	3	2	5	7:41 AM	3	6	9	8:11 AM	2	1	3	8:41 AM	2	6	8
7:12 AM	3	1	4	7:42 AM	3	8	11	8:12 AM	2	4	6	8:42 AM	3	6	9
7:13 AM	3	1	4	7:43 AM	1	8	9	8:13 AM	1	6	7	8:43 AM	4	5	9
7:14 AM	3	2	5	7:44 AM	2	7	9	8:14 AM	2	6	8	8:44 AM	4	5	9
7:15 AM	1	4	5	7:45 AM	3	6	9	8:15 AM	3	4	7	8:45 AM	2	7	9
7:16 AM	3	3	6	7:46 AM	2	6	8	8:16 AM	2	4	6	8:46 AM	3	6	9
7:17 AM	3	1	4	7:47 AM	3	6	9	8:17 AM	2	3	5	8:47 AM	2	5	7
7:18 AM	2	3	5	7:48 AM	4	5	9	8:18 AM	2	3	5	8:48 AM	2	4	6
7:19 AM	1	3	4	7:49 AM	3	5	8	8:19 AM	2	3	5	8:49 AM	1	4	5
7:20 AM	2	2	4	7:50 AM	2	5	7	8:20 AM	3	4	7	8:50 AM	2	3	5
7:21 AM	2	2	4	7:51 AM	3	4	7	8:21 AM	2	3	5	8:51 AM	2	4	6
7:22 AM	4	2	6	7:52 AM	4	3	7	8:22 AM	2	3	5	8:52 AM	4	5	9
7:23 AM	3	2	5	7:53 AM	4	2	6	8:23 AM	3	1	4	8:53 AM	3	3	6
7:24 AM	2	1	3	7:54 AM	4	1	5	8:24 AM	2	1	3	8:54 AM	2	4	6
7:25 AM	1	1	2	7:55 AM	3	1	4	8:25 AM	1	1	2	8:55 AM	2	4	6
7:26 AM	2	1	3	7:56 AM	3	1	4	8:26 AM	0	2	2	8:56 AM	2	3	5
7:27 AM	1	2	3	7:57 AM	3	0	3	8:27 AM	2	3	5	8:57 AM	2	2	4
7:28 AM	2	1	3	7:58 AM	2	2	4	8:28 AM	2	3	5	8:58 AM	2	2	4
7:29 AM	2	0	2	7:59 AM	1	4	5	8:29 AM	1	3	4	8:59 AM	1	5	6

Starbucks  
1224 N Harbor Blvd  
Anaheim, CA 92801  
9/30/2021 Thursday MD Pk Hr

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
11:00 AM	1	0	1	11:30 AM	2	1	3	12:00 PM	3	1	4	12:30 PM	1	4	5
11:01 AM	1	0	1	11:31 AM	2	1	3	12:01 PM	3	1	4	12:31 PM	1	3	4
11:02 AM	0	1	1	11:32 AM	2	2	4	12:02 PM	1	2	3	12:32 PM	3	2	5
11:03 AM	1	2	3	11:33 AM	2	4	6	12:03 PM	1	1	2	12:33 PM	3	0	3
11:04 AM	2	1	3	11:34 AM	2	4	6	12:04 PM	2	1	3	12:34 PM	3	0	3
11:05 AM	2	1	3	11:35 AM	2	4	6	12:05 PM	2	1	3	12:35 PM	1	1	2
11:06 AM	2	0	2	11:36 AM	2	6	8	12:06 PM	2	3	5	12:36 PM	1	1	2
11:07 AM	2	0	2	11:37 AM	3	5	8	12:07 PM	2	3	5	12:37 PM	1	2	3
11:08 AM	2	2	4	11:38 AM	2	5	7	12:08 PM	2	3	5	12:38 PM	1	2	3
11:09 AM	2	2	4	11:39 AM	2	5	7	12:09 PM	2	1	3	12:39 PM	2	1	3
11:10 AM	2	2	4	11:40 AM	2	5	7	12:10 PM	2	1	3	12:40 PM	3	3	6
11:11 AM	3	2	5	11:41 AM	2	7	9	12:11 PM	1	2	3	12:41 PM	3	3	6
11:12 AM	2	0	2	11:42 AM	3	8	11	12:12 PM	2	3	5	12:42 PM	3	3	6
11:13 AM	2	0	2	11:43 AM	3	7	10	12:13 PM	3	2	5	12:43 PM	4	2	6
11:14 AM	1	0	1	11:44 AM	3	6	9	12:14 PM	3	1	4	12:44 PM	3	1	4
11:15 AM	0	0	0	11:45 AM	3	8	11	12:15 PM	2	1	3	12:45 PM	3	1	4
11:16 AM	0	0	0	11:46 AM	3	6	9	12:16 PM	2	1	3	12:46 PM	3	0	3
11:17 AM	0	1	1	11:47 AM	3	6	9	12:17 PM	1	0	1	12:47 PM	2	0	2
11:18 AM	1	1	2	11:48 AM	2	7	9	12:18 PM	0	0	0	12:48 PM	1	0	1
11:19 AM	2	1	3	11:49 AM	3	7	10	12:19 PM	0	0	0	12:49 PM	1	0	1
11:20 AM	2	0	2	11:50 AM	4	6	10	12:20 PM	0	0	0	12:50 PM	0	2	2
11:21 AM	2	0	2	11:51 AM	4	5	9	12:21 PM	0	0	0	12:51 PM	2	2	4
11:22 AM	1	1	2	11:52 AM	4	5	9	12:22 PM	1	2	3	12:52 PM	2	0	2
11:23 AM	1	1	2	11:53 AM	4	5	9	12:23 PM	2	4	6	12:53 PM	1	1	2
11:24 AM	1	2	3	11:54 AM	3	5	8	12:24 PM	2	3	5	12:54 PM	1	2	3
11:25 AM	2	1	3	11:55 AM	3	3	6	12:25 PM	2	3	5	12:55 PM	2	2	4
11:26 AM	2	0	2	11:56 AM	3	3	6	12:26 PM	3	2	5	12:56 PM	2	3	5
11:27 AM	2	1	3	11:57 AM	4	2	6	12:27 PM	3	0	3	12:57 PM	3	1	4
11:28 AM	1	1	2	11:58 AM	3	1	4	12:28 PM	3	2	5	12:58 PM	4	1	5
11:29 AM	1	2	3	11:59 AM	2	0	2	12:29 PM	2	3	5	12:59 PM	4	1	5

Starbucks  
1224 N Harbor Blvd  
Anaheim, CA 92801  
9/30/2021 Thursday PM Pk Hr

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
4:00 PM	1	0	1	4:30 PM	2	3	5	5:00 PM	4	0	4	5:30 PM	4	4	8
4:01 PM	0	0	0	4:31 PM	3	2	5	5:01 PM	3	0	3	5:31 PM	4	4	8
4:02 PM	0	2	2	4:32 PM	4	2	6	5:02 PM	2	0	2	5:32 PM	4	4	8
4:03 PM	1	3	4	4:33 PM	4	2	6	5:03 PM	3	1	4	5:33 PM	4	5	9
4:04 PM	2	2	4	4:34 PM	3	3	6	5:04 PM	3	1	4	5:34 PM	4	4	8
4:05 PM	2	1	3	4:35 PM	3	3	6	5:05 PM	3	2	5	5:35 PM	4	3	7
4:06 PM	2	0	2	4:36 PM	3	2	5	5:06 PM	2	2	4	5:36 PM	4	3	7
4:07 PM	1	0	1	4:37 PM	3	2	5	5:07 PM	2	1	3	5:37 PM	4	3	7
4:08 PM	1	1	2	4:38 PM	4	0	4	5:08 PM	2	2	4	5:38 PM	4	3	7
4:09 PM	1	0	1	4:39 PM	3	0	3	5:09 PM	2	3	5	5:39 PM	3	2	5
4:10 PM	0	1	1	4:40 PM	4	1	5	5:10 PM	3	3	6	5:40 PM	3	2	5
4:11 PM	0	2	2	4:41 PM	3	1	4	5:11 PM	3	3	6	5:41 PM	3	2	5
4:12 PM	2	2	4	4:42 PM	4	1	5	5:12 PM	4	3	7	5:42 PM	3	2	5
4:13 PM	2	2	4	4:43 PM	4	2	6	5:13 PM	4	2	6	5:43 PM	3	1	4
4:14 PM	2	2	4	4:44 PM	4	1	5	5:14 PM	4	1	5	5:44 PM	3	1	4
4:15 PM	2	2	4	4:45 PM	3	1	4	5:15 PM	3	2	5	5:45 PM	2	1	3
4:16 PM	3	2	5	4:46 PM	3	0	3	5:16 PM	3	2	5	5:46 PM	2	1	3
4:17 PM	4	1	5	4:47 PM	3	1	4	5:17 PM	3	2	5	5:47 PM	2	2	4
4:18 PM	4	1	5	4:48 PM	3	2	5	5:18 PM	3	3	6	5:48 PM	3	2	5
4:19 PM	4	1	5	4:49 PM	2	3	5	5:19 PM	4	4	8	5:49 PM	3	1	4
4:20 PM	2	0	2	4:50 PM	2	3	5	5:20 PM	4	3	7	5:50 PM	3	0	3
4:21 PM	2	0	2	4:51 PM	3	1	4	5:21 PM	4	3	7	5:51 PM	3	0	3
4:22 PM	1	0	1	4:52 PM	3	0	3	5:22 PM	3	3	6	5:52 PM	3	1	4
4:23 PM	0	0	0	4:53 PM	3	1	4	5:23 PM	3	3	6	5:53 PM	3	1	4
4:24 PM	0	1	1	4:54 PM	4	2	6	5:24 PM	4	4	8	5:54 PM	4	0	4
4:25 PM	1	1	2	4:55 PM	4	2	6	5:25 PM	4	5	9	5:55 PM	1	1	2
4:26 PM	2	1	3	4:56 PM	4	2	6	5:26 PM	4	5	9	5:56 PM	1	3	4
4:27 PM	2	2	4	4:57 PM	4	1	5	5:27 PM	4	4	8	5:57 PM	1	3	4
4:28 PM	2	3	5	4:58 PM	4	1	5	5:28 PM	4	4	8	5:58 PM	2	3	5
4:29 PM	3	3	6	4:59 PM	4	0	4	5:29 PM	4	5	9	5:59 PM	3	3	6

Starbucks  
1224 N Harbor Blvd  
Anaheim, CA 92801  
9/30/2021 Thursday PM Pk Hr

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
6:00 PM	3	3	6	6:30 PM	2	0	2
6:01 PM	4	3	7	6:31 PM	1	0	1
6:02 PM	4	2	6	6:32 PM	0	1	1
6:03 PM	4	2	6	6:33 PM	1	1	2
6:04 PM	4	2	6	6:34 PM	1	0	1
6:05 PM	4	3	7	6:35 PM	1	0	1
6:06 PM	4	2	6	6:36 PM	1	2	3
6:07 PM	4	2	6	6:37 PM	1	2	3
6:08 PM	4	1	5	6:38 PM	1	2	3
6:09 PM	4	1	5	6:39 PM	2	2	4
6:10 PM	4	1	5	6:40 PM	2	2	4
6:11 PM	4	1	5	6:41 PM	2	2	4
6:12 PM	2	0	2	6:42 PM	3	2	5
6:13 PM	2	0	2	6:43 PM	3	1	4
6:14 PM	1	0	1	6:44 PM	3	0	3
6:15 PM	0	1	1	6:45 PM	3	0	3
6:16 PM	1	1	2	6:46 PM	3	0	3
6:17 PM	1	0	1	6:47 PM	3	0	3
6:18 PM	1	0	1	6:48 PM	3	0	3
6:19 PM	1	2	3	6:49 PM	3	0	3
6:20 PM	1	3	4	6:50 PM	1	1	2
6:21 PM	2	3	5	6:51 PM	1	2	3
6:22 PM	3	1	4	6:52 PM	2	2	4
6:23 PM	3	2	5	6:53 PM	2	2	4
6:24 PM	3	2	5	6:54 PM	2	2	4
6:25 PM	3	1	4	6:55 PM	2	1	3
6:26 PM	3	1	4	6:56 PM	2	0	2
6:27 PM	3	1	4	6:57 PM	1	2	3
6:28 PM	3	0	3	6:58 PM	2	2	4
6:29 PM	2	0	2	6:59 PM	3	2	5

**Starbucks**  
**1224 N Harbor Blvd**  
**Anaheim, CA 92801**  
**10/1/2021      Friday      AM Pk Hr**

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
7:00 AM	4	6	10	7:30 AM	3	4	7	8:00 AM	3	7	10	8:30 AM	2	10	12
7:01 AM	4	7	11	7:31 AM	3	4	7	8:01 AM	3	7	10	8:31 AM	1	10	11
7:02 AM	4	7	11	7:32 AM	3	5	8	8:02 AM	3	7	10	8:32 AM	2	10	12
7:03 AM	4	6	10	7:33 AM	3	7	10	8:03 AM	3	7	10	8:33 AM	2	10	12
7:04 AM	4	6	10	7:34 AM	3	6	9	8:04 AM	4	7	11	8:34 AM	2	10	12
7:05 AM	4	5	9	7:35 AM	3	6	9	8:05 AM	3	7	10	8:35 AM	4	10	14
7:06 AM	4	4	8	7:36 AM	3	6	9	8:06 AM	4	7	11	8:36 AM	4	9	13
7:07 AM	4	5	9	7:37 AM	3	6	9	8:07 AM	3	9	12	8:37 AM	3	10	13
7:08 AM	4	6	10	7:38 AM	4	6	10	8:08 AM	3	9	12	8:38 AM	3	9	12
7:09 AM	4	6	10	7:39 AM	4	6	10	8:09 AM	4	8	12	8:39 AM	3	8	11
7:10 AM	3	5	8	7:40 AM	4	7	11	8:10 AM	3	8	11	8:40 AM	4	8	12
7:11 AM	3	4	7	7:41 AM	4	7	11	8:11 AM	3	8	11	8:41 AM	4	9	13
7:12 AM	4	3	7	7:42 AM	3	7	10	8:12 AM	4	10	14	8:42 AM	3	10	13
7:13 AM	4	3	7	7:43 AM	3	8	11	8:13 AM	3	8	11	8:43 AM	3	9	12
7:14 AM	4	3	7	7:44 AM	2	7	9	8:14 AM	2	9	11	8:44 AM	4	7	11
7:15 AM	4	3	7	7:45 AM	2	7	9	8:15 AM	2	9	11	8:45 AM	4	6	10
7:16 AM	4	3	7	7:46 AM	3	6	9	8:16 AM	2	9	11	8:46 AM	3	5	8
7:17 AM	3	3	6	7:47 AM	4	6	10	8:17 AM	3	7	10	8:47 AM	4	5	9
7:18 AM	3	5	8	7:48 AM	4	6	10	8:18 AM	4	7	11	8:48 AM	4	4	8
7:19 AM	3	4	7	7:49 AM	2	6	8	8:19 AM	4	7	11	8:49 AM	4	4	8
7:20 AM	3	5	8	7:50 AM	0	8	8	8:20 AM	2	7	9	8:50 AM	4	3	7
7:21 AM	4	5	9	7:51 AM	2	9	11	8:21 AM	3	1	4	8:51 AM	4	2	6
7:22 AM	4	4	8	7:52 AM	2	9	11	8:22 AM	4	7	11	8:52 AM	4	2	6
7:23 AM	3	7	10	7:53 AM	4	9	13	8:23 AM	4	7	11	8:53 AM	4	2	6
7:24 AM	2	9	11	7:54 AM	3	7	10	8:24 AM	3	8	11	8:54 AM	3	3	6
7:25 AM	2	8	10	7:55 AM	4	6	10	8:25 AM	4	7	11	8:55 AM	2	4	6
7:26 AM	3	7	10	7:56 AM	4	6	10	8:26 AM	4	9	13	8:56 AM	4	4	8
7:27 AM	3	7	10	7:57 AM	3	7	10	8:27 AM	4	10	14	8:57 AM	4	5	9
7:28 AM	2	7	9	7:58 AM	4	5	9	8:28 AM	4	10	14	8:58 AM	4	6	10
7:29 AM	3	6	9	7:59 AM	4	5	9	8:29 AM	4	10	14	8:59 AM	4	6	10



**Starbucks**  
**1224 N Harbor Blvd**  
**Anaheim, CA 92801**  
**10/1/2021 Friday MD Pk Hr**

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
11:00 AM	3	3	6	11:30 AM	4	1	5	12:00 PM	1	1	2	12:30 PM	2	2	4
11:01 AM	3	3	6	11:31 AM	3	0	3	12:01 PM	1	1	2	12:31 PM	2	1	3
11:02 AM	3	2	5	11:32 AM	2	1	3	12:02 PM	0	2	2	12:32 PM	2	1	3
11:03 AM	2	4	6	11:33 AM	1	1	2	12:03 PM	1	2	3	12:33 PM	1	1	2
11:04 AM	2	4	6	11:34 AM	0	1	1	12:04 PM	2	1	3	12:34 PM	1	3	4
11:05 AM	2	5	7	11:35 AM	1	1	2	12:05 PM	2	2	4	12:35 PM	3	2	5
11:06 AM	3	5	8	11:36 AM	1	1	2	12:06 PM	1	2	3	12:36 PM	3	0	3
11:07 AM	3	5	8	11:37 AM	1	1	2	12:07 PM	1	2	3	12:37 PM	3	0	3
11:08 AM	4	5	9	11:38 AM	1	1	2	12:08 PM	1	2	3	12:38 PM	1	1	2
11:09 AM	4	4	8	11:39 AM	2	1	3	12:09 PM	2	2	4	12:39 PM	1	1	2
11:10 AM	4	4	8	11:40 AM	2	0	2	12:10 PM	2	3	5	12:40 PM	2	1	3
11:11 AM	4	3	7	11:41 AM	0	1	1	12:11 PM	3	3	6	12:41 PM	2	0	2
11:12 AM	4	4	8	11:42 AM	1	1	2	12:12 PM	4	2	6	12:42 PM	2	0	2
11:13 AM	4	3	7	11:43 AM	1	0	1	12:13 PM	4	2	6	12:43 PM	1	1	2
11:14 AM	2	3	5	11:44 AM	0	0	0	12:14 PM	4	2	6	12:44 PM	1	1	2
11:15 AM	3	3	6	11:45 AM	0	1	1	12:15 PM	4	1	5	12:45 PM	1	1	2
11:16 AM	3	2	5	11:46 AM	1	1	2	12:16 PM	4	0	4	12:46 PM	2	1	3
11:17 AM	4	1	5	11:47 AM	2	1	3	12:17 PM	2	1	3	12:47 PM	1	1	2
11:18 AM	4	0	4	11:48 AM	3	1	4	12:18 PM	1	1	2	12:48 PM	1	1	2
11:19 AM	4	0	4	11:49 AM	2	1	3	12:19 PM	1	3	4	12:49 PM	1	0	1
11:20 AM	4	0	4	11:50 AM	3	2	5	12:20 PM	1	4	5	12:50 PM	1	0	1
11:21 AM	3	1	4	11:51 AM	3	0	3	12:21 PM	2	3	5	12:51 PM	1	1	2
11:22 AM	2	2	4	11:52 AM	2	0	2	12:22 PM	3	2	5	12:52 PM	2	2	4
11:23 AM	2	2	4	11:53 AM	1	0	1	12:23 PM	3	1	4	12:53 PM	2	1	3
11:24 AM	2	1	3	11:54 AM	1	1	2	12:24 PM	3	3	6	12:54 PM	2	0	2
11:25 AM	2	1	3	11:55 AM	1	0	1	12:25 PM	4	2	6	12:55 PM	1	0	1
11:26 AM	1	2	3	11:56 AM	1	1	2	12:26 PM	2	2	4	12:56 PM	1	0	1
11:27 AM	1	2	3	11:57 AM	2	1	3	12:27 PM	2	1	3	12:57 PM	0	0	0
11:28 AM	2	1	3	11:58 AM	2	0	2	12:28 PM	2	0	2	12:58 PM	0	1	1
11:29 AM	3	2	5	11:59 AM	1	1	2	12:29 PM	0	2	2	12:59 PM	1	1	2

**Starbucks**  
**1224 N Harbor Blvd**  
**Anaheim, CA 92801**  
**10/1/2021 Friday PM Pk Hr**

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
4:00 PM	2	2	4	4:30 PM	3	1	4	5:00 PM	1	0	1	5:30 PM	2	1	3
4:01 PM	2	3	5	4:31 PM	3	0	3	5:01 PM	1	0	1	5:31 PM	2	1	3
4:02 PM	1	3	4	4:32 PM	3	1	4	5:02 PM	1	0	1	5:32 PM	2	0	2
4:03 PM	2	3	5	4:33 PM	3	2	5	5:03 PM	1	1	2	5:33 PM	1	1	2
4:04 PM	3	2	5	4:34 PM	3	1	4	5:04 PM	2	1	3	5:34 PM	1	1	2
4:05 PM	4	2	6	4:35 PM	3	1	4	5:05 PM	2	1	3	5:35 PM	1	0	1
4:06 PM	4	0	4	4:36 PM	4	1	5	5:06 PM	2	0	2	5:36 PM	1	3	4
4:07 PM	2	0	2	4:37 PM	4	3	7	5:07 PM	1	0	1	5:37 PM	2	5	7
4:08 PM	1	0	1	4:38 PM	3	3	6	5:08 PM	1	1	2	5:38 PM	2	3	5
4:09 PM	0	0	0	4:39 PM	3	3	6	5:09 PM	1	1	2	5:39 PM	2	2	4
4:10 PM	0	1	1	4:40 PM	3	3	6	5:10 PM	1	1	2	5:40 PM	3	1	4
4:11 PM	1	0	1	4:41 PM	4	3	7	5:11 PM	1	4	5	5:41 PM	4	2	6
4:12 PM	1	0	1	4:42 PM	4	2	6	5:12 PM	0	4	4	5:42 PM	3	1	4
4:13 PM	1	1	2	4:43 PM	4	3	7	5:13 PM	0	5	5	5:43 PM	3	1	4
4:14 PM	1	2	3	4:44 PM	4	2	6	5:14 PM	1	5	6	5:44 PM	2	0	2
4:15 PM	1	1	2	4:45 PM	4	1	5	5:15 PM	2	5	7	5:45 PM	0	0	0
4:16 PM	2	1	3	4:46 PM	4	1	5	5:16 PM	2	4	6	5:46 PM	0	0	0
4:17 PM	3	1	4	4:47 PM	3	0	3	5:17 PM	2	4	6	5:47 PM	0	1	1
4:18 PM	3	1	4	4:48 PM	2	1	3	5:18 PM	3	3	6	5:48 PM	1	0	1
4:19 PM	2	2	4	4:49 PM	1	1	2	5:19 PM	3	2	5	5:49 PM	1	0	1
4:20 PM	3	2	5	4:50 PM	2	1	3	5:20 PM	3	1	4	5:50 PM	1	0	1
4:21 PM	3	1	4	4:51 PM	2	0	2	5:21 PM	3	0	3	5:51 PM	0	0	0
4:22 PM	3	1	4	4:52 PM	3	1	4	5:22 PM	3	0	3	5:52 PM	0	2	2
4:23 PM	2	0	2	4:53 PM	4	2	6	5:23 PM	3	0	3	5:53 PM	1	2	3
4:24 PM	2	1	3	4:54 PM	3	2	5	5:24 PM	2	0	2	5:54 PM	2	3	5
4:25 PM	2	1	3	4:55 PM	2	0	2	5:25 PM	2	0	2	5:55 PM	3	3	6
4:26 PM	2	1	3	4:56 PM	2	0	2	5:26 PM	0	1	1	5:56 PM	3	5	8
4:27 PM	2	3	5	4:57 PM	2	0	2	5:27 PM	1	2	3	5:57 PM	3	5	8
4:28 PM	2	3	5	4:58 PM	0	1	1	5:28 PM	2	2	4	5:58 PM	3	5	8
4:29 PM	2	2	4	4:59 PM	1	1	2	5:29 PM	2	2	4	5:59 PM	3	5	8

**Starbucks**  
**1224 N Harbor Blvd**  
**Anaheim, CA 92801**  
**10/1/2021 Friday PM Pk Hr**

Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue	Time	Queue Pick-up to Order Board	Queue Order Board Back	Total Queue
6:00 PM	3	4	7	6:30 PM	1	1	2
6:01 PM	1	4	5	6:31 PM	2	1	3
6:02 PM	1	4	5	6:32 PM	2	0	2
6:03 PM	2	3	5	6:33 PM	2	2	4
6:04 PM	3	2	5	6:34 PM	2	2	4
6:05 PM	3	2	5	6:35 PM	1	2	3
6:06 PM	4	1	5	6:36 PM	2	2	4
6:07 PM	3	1	4	6:37 PM	3	1	4
6:08 PM	3	0	3	6:38 PM	3	1	4
6:09 PM	3	0	3	6:39 PM	3	1	4
6:10 PM	3	1	4	6:40 PM	4	0	4
6:11 PM	4	1	5	6:41 PM	4	1	5
6:12 PM	3	1	4	6:42 PM	2	1	3
6:13 PM	3	1	4	6:43 PM	1	1	2
6:14 PM	3	1	4	6:44 PM	0	1	1
6:15 PM	2	1	3	6:45 PM	1	1	2
6:16 PM	2	1	3	6:46 PM	1	0	1
6:17 PM	2	1	3	6:47 PM	1	1	2
6:18 PM	3	1	4	6:48 PM	1	1	2
6:19 PM	3	1	4	6:49 PM	1	0	1
6:20 PM	2	3	5	6:50 PM	1	1	2
6:21 PM	2	2	4	6:51 PM	1	0	1
6:22 PM	3	1	4	6:52 PM	0	2	2
6:23 PM	3	0	3	6:53 PM	0	2	2
6:24 PM	3	1	4	6:54 PM	1	2	3
6:25 PM	3	0	3	6:55 PM	1	1	2
6:26 PM	2	0	2	6:56 PM	1	1	2
6:27 PM	2	1	3	6:57 PM	1	0	1
6:28 PM	1	0	1	6:58 PM	1	0	1
6:29 PM	1	1	2	6:59 PM	0	1	1

## APPENDIX D

### EXISTING TRAFFIC COUNT DATA

# INTERSECTION TURNING MOVEMENT COUNTS

PREPARED BY: AimTD LLC. tel: 714 253 7888 cs@aimtd.com

T218

DATE:  
Tue, Oct 12, 21

LOCATION: Newport Beach  
NORTH & SOUTH: Burger King Dwy  
EAST & WEST: Bristol

PROJECT #: SC  
LOCATION #: 1  
CONTROL: NO CONTROL

NOTES:					AM PM		▲ N	
No queue EB to Burger King. EB through/right turn lane (BD17)					MD	◀ W	S	E ▶
					OTHER		▼	

☒ Add U-Turns to Left Turns

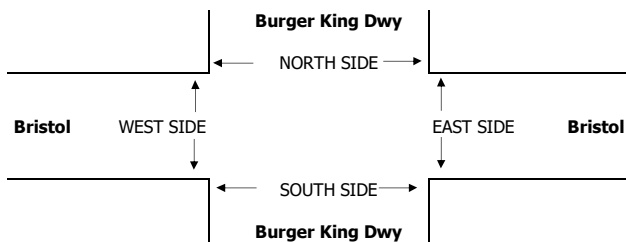
	NORTHBOUND Burger King Dwy			SOUTHBOUND Burger King Dwy			EASTBOUND Bristol			WESTBOUND Bristol			
LANES:	NL X	NT X	NR 0	SL X	ST X	SR X	EL X	ET 4	ER 0	WL X	WT X	WR X	TOTAL
7:00 AM	0	0	1	0	0	0	0	177	1	0	0	0	179
7:15 AM	0	0	3	0	0	0	0	212	1	0	0	0	216
7:30 AM	0	0	0	0	0	0	0	259	0	0	0	0	259
7:45 AM	0	0	4	0	0	0	0	282	1	0	0	0	287
8:00 AM	0	0	3	0	0	0	0	309	0	0	0	0	312
8:15 AM	0	0	3	0	0	0	0	321	2	0	0	0	326
8:30 AM	0	0	0	0	0	0	0	340	1	0	0	0	341
8:45 AM	0	0	2	0	0	0	0	305	3	0	0	0	310
VOLUMES	0	0	16	0	0	0	0	2,205	9	0	0	0	2,230
APPROACH %	0%	0%	100%	0%	0%	0%	0%	100%	0%	0%	0%	0%	
APP/DEPART	16	/	0	0	/	9	2,214	/	2,221	0	/	0	0
BEGIN PEAK HR	8:00 AM												
VOLUMES	0	0	8	0	0	0	0	1,275	6	0	0	0	1,289
APPROACH %	0%	0%	100%	0%	0%	0%	0%	100%	0%	0%	0%	0%	
PEAK HR FACTOR	0.667			0.000			0.939			0.000			0.945
APP/DEPART	8	/	0	0	/	6	1,281	/	1,283	0	/	0	0
4:00 PM	0	0	1	0	0	0	0	337	0	0	0	0	338
4:15 PM	0	0	2	0	0	0	0	303	2	0	0	0	307
4:30 PM	0	0	2	0	0	0	0	311	3	0	0	0	316
4:45 PM	0	0	4	0	0	0	0	371	1	0	0	0	376
5:00 PM	0	0	4	0	0	0	0	387	3	0	0	0	394
5:15 PM	0	0	2	0	0	0	0	353	5	0	0	0	360
5:30 PM	0	0	7	0	0	0	0	368	5	0	0	0	380
5:45 PM	0	0	3	0	0	0	0	312	3	0	0	0	318
VOLUMES	0	0	25	0	0	0	0	2,742	22	0	0	0	2,789
APPROACH %	0%	0%	100%	0%	0%	0%	0%	99%	1%	0%	0%	0%	
APP/DEPART	25	/	0	0	/	22	2,764	/	2,767	0	/	0	0
BEGIN PEAK HR	4:45 PM												
VOLUMES	0	0	17	0	0	0	0	1,479	14	0	0	0	1,510
APPROACH %	0%	0%	100%	0%	0%	0%	0%	99%	1%	0%	0%	0%	
PEAK HR FACTOR	0.607			0.000			0.957			0.000			0.958
APP/DEPART	17	/	0	0	/	14	1,493	/	1,496	0	/	0	0

U-TURNS				
NB 0	SB 0	EB 0	WB 0	TTL 0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0

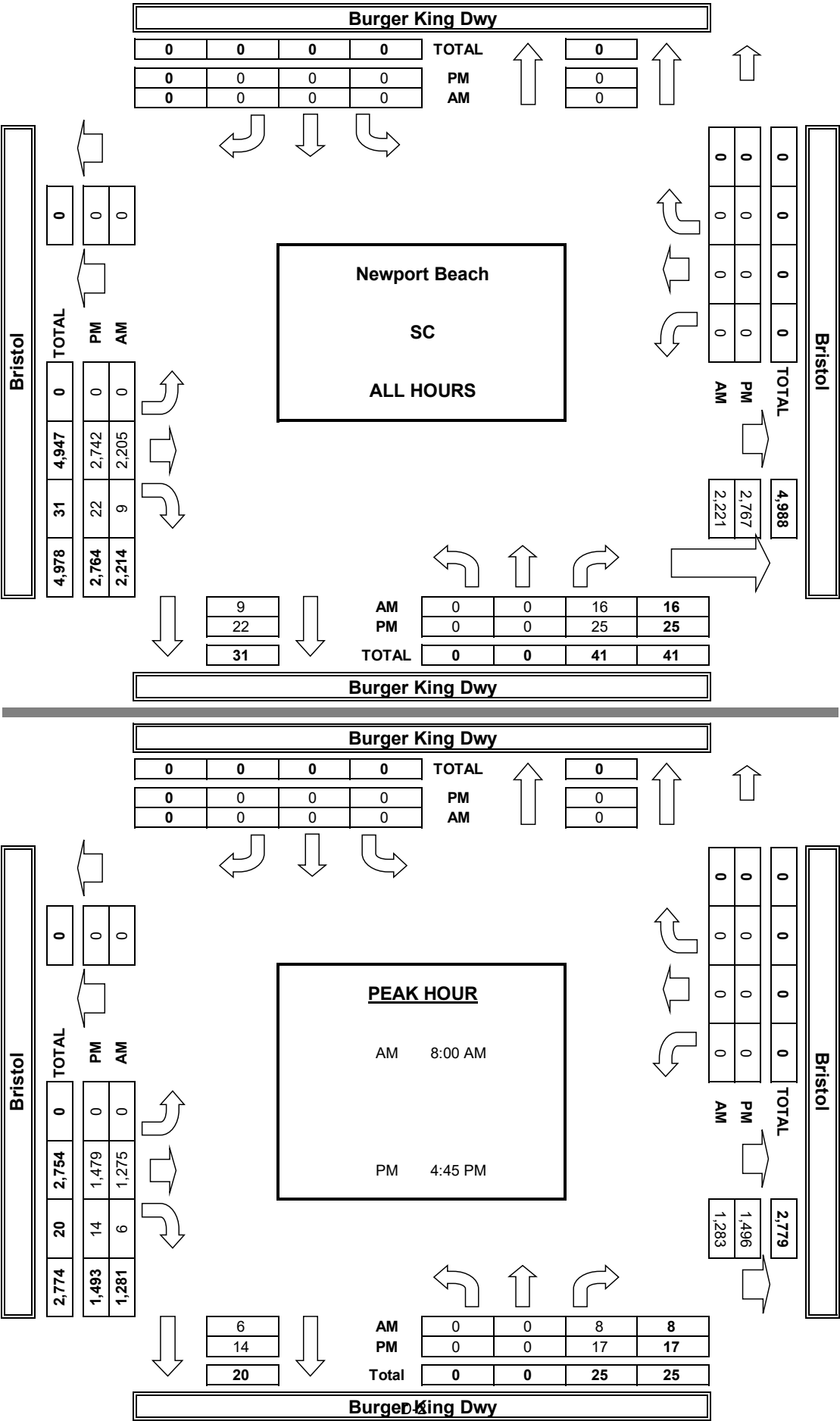
EB through/ right turn lane
31
40
45
31
49
49
40
36

0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0

47
37
41
53
53
42
51
41



AimTD LLC  
TURNING MOVEMENT COUNTS



**INTERSECTION TURNING MOVEMENT COUNTS**

PREPARED BY: AimTD LLC. tel: 714 253 7888 cs@aimtd.com

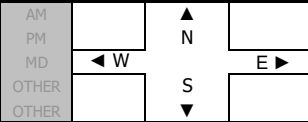
DATE:  
Tue, Oct 12, 21

LOCATION:  
NORTH & SOUTH: Newport Beach  
EAST & WEST: Birch  
Burger King Dwy

PROJECT #: SC  
LOCATION #: 2  
CONTROL: NO CONTROL

## NOTES:

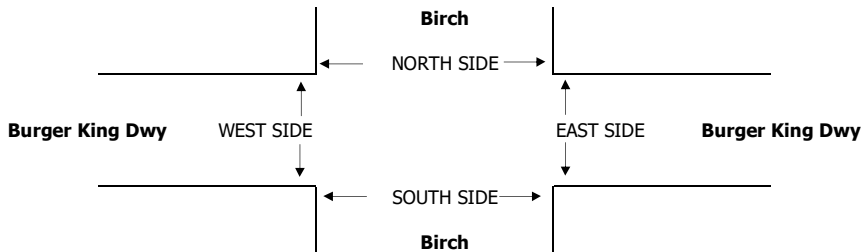
Queue NB till Burger King Dwy:  
16:16:10, 16:17:22, 16:26:11, 16:36:22, 17:02:16, 17:56:58

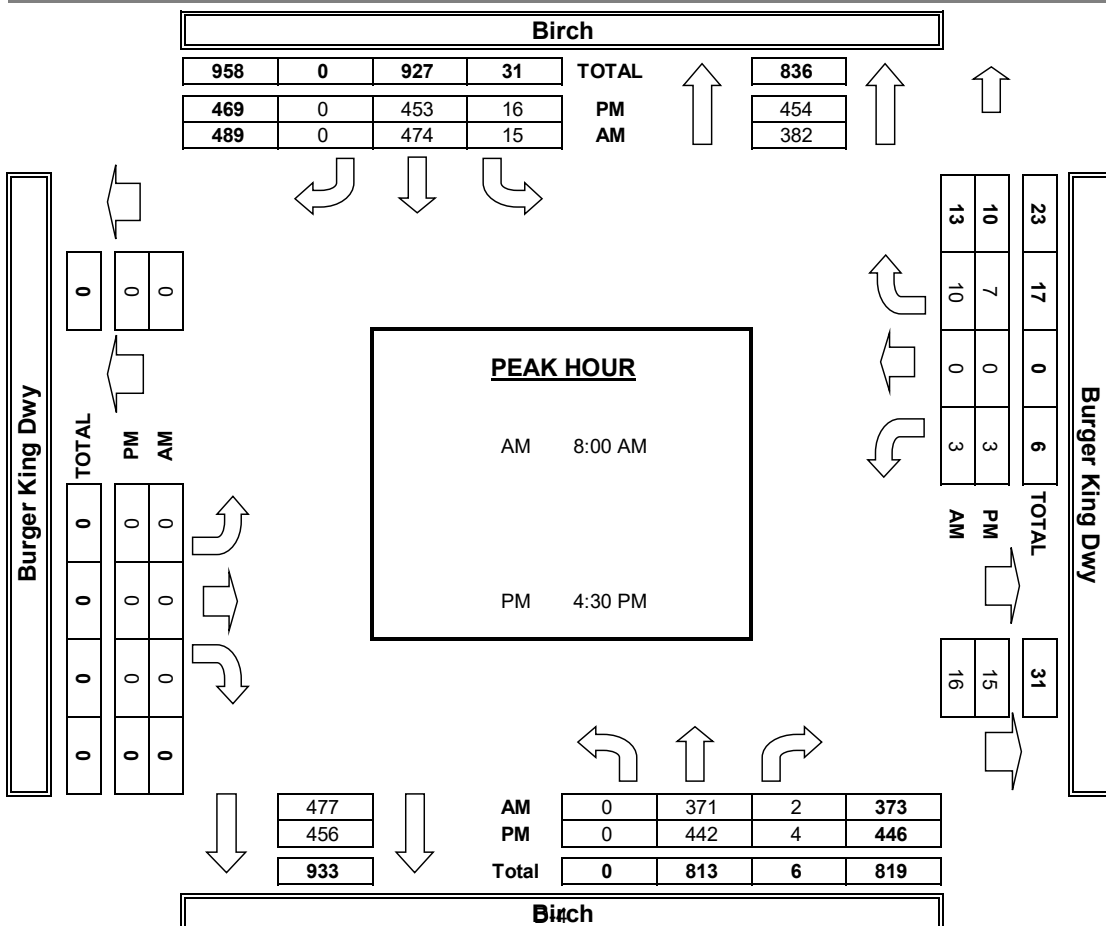
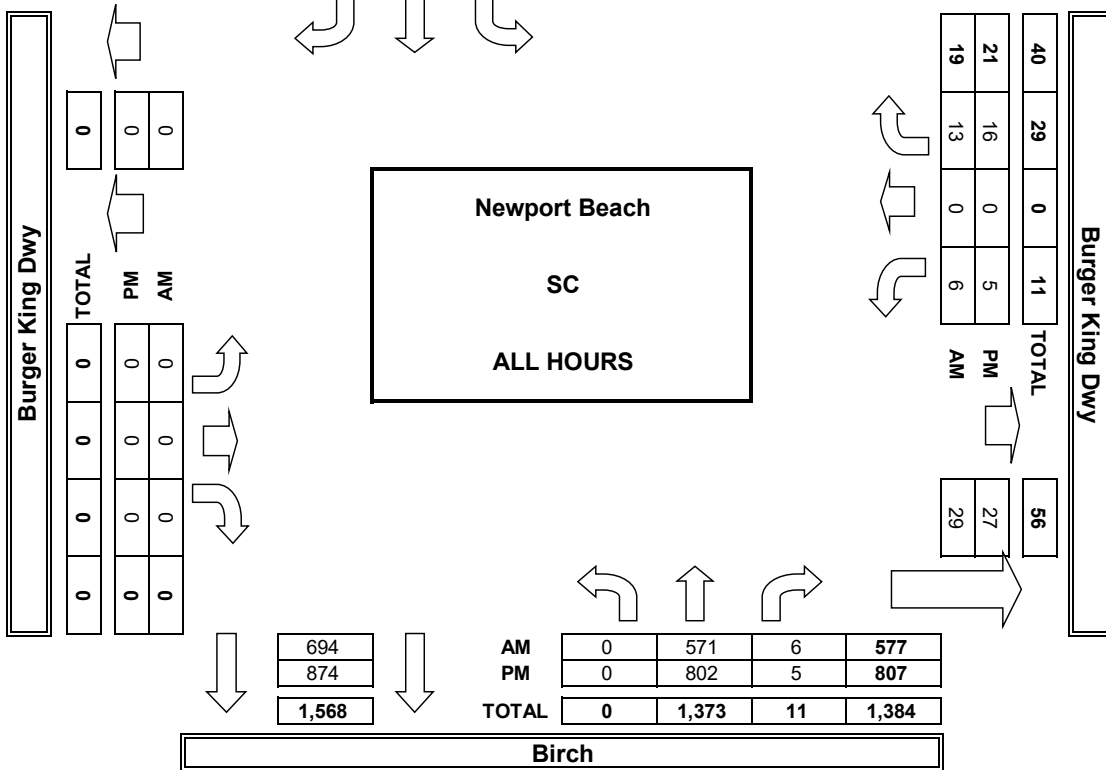

☒ Add U-Turns to Left Turns

	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
	Birch			Birch			Burger King Dwy			Burger King Dwy			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	X	4	0	0	2	X	X	X	X	0	X	0	
7:00 AM	0	32	0	2	34	0	0	0	0	1	0	1	70
7:15 AM	0	31	1	2	40	0	0	0	0	0	0	0	74
7:30 AM	0	58	1	3	62	0	0	0	0	2	0	1	127
7:45 AM	0	79	2	3	78	0	0	0	0	0	0	1	163
8:00 AM	0	79	1	4	125	0	0	0	0	2	0	1	212
8:15 AM	0	85	0	4	102	0	0	0	0	0	0	3	194
8:30 AM	0	102	1	3	135	0	0	0	0	1	0	2	244
8:45 AM	0	105	0	4	112	0	0	0	0	0	0	4	225
VOLUMES	0	571	6	25	688	0	0	0	0	6	0	13	1,309
APPROACH %	0%	99%	1%	4%	96%	0%	0%	0%	0%	32%	0%	68%	
APP/DEPART	577	/	586	713	/	694	0	/	29	19	/	0	0
BEGIN PEAK HR	8:00 AM												
VOLUMES	0	371	2	15	474	0	0	0	0	3	0	10	875
APPROACH %	0%	99%	1%	3%	97%	0%	0%	0%	0%	23%	0%	77%	
PEAK HR FACTOR	0.888			0.886			0.000			0.813			0.897
APP/DEPART	373	/	382	489	/	477	0	/	16	13	/	0	0
4:00 PM	0	97	0	3	92	0	0	0	0	1	0	2	195
4:15 PM	0	103	0	3	102	0	0	0	0	0	0	1	209
4:30 PM	0	101	3	3	107	0	0	0	0	0	0	3	217
4:45 PM	0	98	0	5	119	0	0	0	0	0	0	2	224
5:00 PM	0	123	0	4	107	0	0	0	0	2	0	2	238
5:15 PM	0	120	1	4	120	0	0	0	0	1	0	0	246
5:30 PM	0	96	1	3	115	0	0	0	0	1	0	0	216
5:45 PM	0	64	0	3	107	0	0	0	0	0	0	6	180
VOLUMES	0	802	5	28	869	0	0	0	0	5	0	16	1,725
APPROACH %	0%	99%	1%	3%	97%	0%	0%	0%	0%	24%	0%	76%	
APP/DEPART	807	/	824	897	/	874	0	/	27	21	/	0	0
BEGIN PEAK HR	4:30 PM												
VOLUMES	0	442	4	16	453	0	0	0	0	3	0	7	925
APPROACH %	0%	99%	1%	3%	97%	0%	0%	0%	0%	30%	0%	70%	
PEAK HR FACTOR	0.907			0.946			0.000			0.625			0.940
APP/DEPART	446	/	454	469	/	456	0	/	15	10	/	0	0

U-TURNS				
NB	SB	EB	WB	TTL
0	0	0	0	0
0	0	0	0	0
0	1	0	0	1
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	1	0	0	1
0	2	0	0	2

0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	1	0	0	1
0	1	0	0	1
0	3	0	0	3
0	1	0	0	1
0	0	0	0	0
0	6	0	0	6







## APPENDIX E

### LEVEL OF SERVICE CALCULATION WORKSHEETS



*APPENDIX E-1*

**EXISTING TRAFFIC CONDITIONS**

**Intersection Level Of Service Report**  
**Intersection 1: Project Driveway No. 1 at Bristol Street**

Control Type:	Two-way stop	Delay (sec / veh):	15.9
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.024

**Intersection Setup**

Name	Project Driveway No. 1		Bristol Street		Bristol Street	
Approach	Northbound		Eastbound		Westbound	
Lane Configuration						
Turning Movement	Left	Right	Thru	Right	Left	Thru
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Entry Pocket	0	0	0	0	0	0
Entry Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
No. of Lanes in Exit Pocket	0	0	0	0	0	0
Exit Pocket Length [ft]	0.00	0.00	0.00	0.00	0.00	0.00
Speed [mph]	30.00		45.00		30.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

**Volumes**

Name	Project Driveway No. 1		Bristol Street		Bristol Street	
Base Volume Input [veh/h]	0	8	1275	6	0	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	8	1275	6	0	0
Peak Hour Factor	0.9500	0.9450	0.9450	0.9450	0.9500	0.9500
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	2	337	2	0	0
Total Analysis Volume [veh/h]	0	8	1349	6	0	0
Pedestrian Volume [ped/h]	0		0		0	

**Intersection Settings**

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0




**Movement, Approach, & Intersection Results**

V/C, Movement V/C Ratio	0.00	0.02	0.01	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	15.89	0.00	0.00	0.00	0.00
Movement LOS		C	A	A		
95th-Percentile Queue Length [veh/ln]	0.00	0.07	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft/ln]	0.00	1.81	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	15.89		0.00		0.00	
Approach LOS	C		A		A	
d_I, Intersection Delay [s/veh]	0.09					
Intersection LOS	C					

**Intersection Level Of Service Report**  
**Intersection 2: Birch Street at Project Driveway No. 2**

Control Type:	Two-way stop	Delay (sec / veh):	11.8
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.006

**Intersection Setup**

Name	Birch Street		Birch Street		Project Driveway No. 2	
Approach	Northbound		Southbound		Westbound	
Lane Configuration						
Turning Movement	Thru	Right	Left	Thru	Left	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Entry Pocket	0	0	0	0	0	0
Entry Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
No. of Lanes in Exit Pocket	0	0	0	0	0	0
Exit Pocket Length [ft]	0.00	0.00	0.00	0.00	0.00	0.00
Speed [mph]	45.00		45.00		30.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

**Volumes**

Name	Birch Street		Birch Street		Project Driveway No. 2	
Base Volume Input [veh/h]	371	2	15	474	3	10
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	371	2	15	474	3	10
Peak Hour Factor	0.8970	0.8970	0.8970	0.8970	0.8970	0.8970
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	103	1	4	132	1	3
Total Analysis Volume [veh/h]	414	2	17	528	3	11
Pedestrian Volume [ped/h]	0		0		0	

**Intersection Settings**

Priority Scheme	Free	Free	Stop
Flared Lane			No
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance			Yes
Number of Storage Spaces in Median	0	0	2

**Movement, Approach, & Intersection Results**



V/C, Movement V/C Ratio	0.00	0.00	0.01	0.01	0.01	0.01
d_M, Delay for Movement [s/veh]	0.00	0.00	8.21	0.00	11.80	9.61
Movement LOS	A	A	A	A	B	A
95th-Percentile Queue Length [veh/ln]	0.00	0.00	0.05	0.00	0.06	0.06
95th-Percentile Queue Length [ft/ln]	0.00	0.00	1.14	0.00	1.48	1.48
d_A, Approach Delay [s/veh]	0.00		0.26		10.08	
Approach LOS	A		A		B	
d_I, Intersection Delay [s/veh]	0.29					
Intersection LOS	B					



**Intersection Level Of Service Report**  
**Intersection 1: Project Driveway No. 1 at Bristol Street**

Control Type:	Two-way stop	Delay (sec / veh):	18.2
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.062

**Intersection Setup**

Name	Project Driveway No. 1		Bristol Street		Bristol Street	
Approach	Northbound		Eastbound		Westbound	
Lane Configuration						
Turning Movement	Left	Right	Thru	Right	Left	Thru
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Entry Pocket	0	0	0	0	0	0
Entry Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
No. of Lanes in Exit Pocket	0	0	0	0	0	0
Exit Pocket Length [ft]	0.00	0.00	0.00	0.00	0.00	0.00
Speed [mph]	30.00		45.00		30.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

**Volumes**

Name	Project Driveway No. 1		Bristol Street		Bristol Street	
Base Volume Input [veh/h]	0	17	1479	14	0	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	17	1479	14	0	0
Peak Hour Factor	0.9500	0.9580	0.9580	0.9580	0.9500	0.9500
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	4	386	4	0	0
Total Analysis Volume [veh/h]	0	18	1544	15	0	0
Pedestrian Volume [ped/h]	0		0		0	

**Intersection Settings**

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0




**Movement, Approach, & Intersection Results**

V/C, Movement V/C Ratio	0.00	0.06	0.02	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	18.22	0.00	0.00	0.00	0.00
Movement LOS		C	A	A		
95th-Percentile Queue Length [veh/ln]	0.00	0.20	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft/ln]	0.00	4.93	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	18.22		0.00		0.00	
Approach LOS	C		A		A	
d_I, Intersection Delay [s/veh]	0.21					
Intersection LOS	C					

**Intersection Level Of Service Report**  
**Intersection 2: Birch Street at Project Driveway No. 2**

Control Type:	Two-way stop	Delay (sec / veh):	12.1
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.006

**Intersection Setup**

Name	Birch Street		Birch Street		Project Driveway No. 2	
Approach	Northbound		Southbound		Westbound	
Lane Configuration						
Turning Movement	Thru	Right	Left	Thru	Left	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Entry Pocket	0	0	0	0	0	0
Entry Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
No. of Lanes in Exit Pocket	0	0	0	0	0	0
Exit Pocket Length [ft]	0.00	0.00	0.00	0.00	0.00	0.00
Speed [mph]	45.00		45.00		30.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

**Volumes**

Name	Birch Street		Birch Street		Project Driveway No. 2	
Base Volume Input [veh/h]	442	4	16	453	3	7
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	442	4	16	453	3	7
Peak Hour Factor	0.9400	0.9400	0.9400	0.9400	0.9400	0.9400
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	118	1	4	120	1	2
Total Analysis Volume [veh/h]	470	4	17	482	3	7
Pedestrian Volume [ped/h]	0		0		0	

**Intersection Settings**

Priority Scheme	Free	Free	Stop
Flared Lane			No
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance			Yes
Number of Storage Spaces in Median	0	0	2

**Movement, Approach, & Intersection Results**

V/C, Movement V/C Ratio	0.00	0.00	0.02	0.00	0.01	0.01
d_M, Delay for Movement [s/veh]	0.00	0.00	8.37	0.00	12.06	9.79
Movement LOS	A	A	A	A	B	A
95th-Percentile Queue Length [veh/ln]	0.00	0.00	0.05	0.00	0.05	0.05
95th-Percentile Queue Length [ft/ln]	0.00	0.00	1.19	0.00	1.14	1.14
d_A, Approach Delay [s/veh]	0.00		0.29		10.47	
Approach LOS	A		A		B	
d_I, Intersection Delay [s/veh]	0.25					
Intersection LOS	B					



*APPENDIX E-II*

**EXISTING (WITH STARBUCKS)  
TRAFFIC CONDITIONS**

**Intersection Level Of Service Report**  
**Intersection 1: Project Driveway No. 1 at Bristol Street**

Control Type:	Two-way stop	Delay (sec / veh):	17.2
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.104

**Intersection Setup**

Name	Project Driveway No. 1		Bristol Street		Bristol Street	
Approach	Northbound		Eastbound		Westbound	
Lane Configuration						
Turning Movement	Left	Right	Thru	Right	Left	Thru
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Entry Pocket	0	0	0	0	0	0
Entry Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
No. of Lanes in Exit Pocket	0	0	0	0	0	0
Exit Pocket Length [ft]	0.00	0.00	0.00	0.00	0.00	0.00
Speed [mph]	30.00		45.00		30.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

**Volumes**

Name	Project Driveway No. 1		Bristol Street		Bristol Street	
Base Volume Input [veh/h]	0	32	1275	45	0	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	32	1275	45	0	0
Peak Hour Factor	0.9500	0.9450	0.9450	0.9450	0.9500	0.9500
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	8	337	12	0	0
Total Analysis Volume [veh/h]	0	34	1349	48	0	0
Pedestrian Volume [ped/h]	0		0		0	



**Intersection Settings**

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0




**Movement, Approach, & Intersection Results**

V/C, Movement V/C Ratio	0.00	0.10	0.01	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	17.23	0.00	0.00	0.00	0.00
Movement LOS		C	A	A		
95th-Percentile Queue Length [veh/ln]	0.00	0.34	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft/ln]	0.00	8.59	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	17.23		0.00		0.00	
Approach LOS	C		A		A	
d_I, Intersection Delay [s/veh]	0.41					
Intersection LOS	C					

**Intersection Level Of Service Report**  
**Intersection 2: Birch Street at Project Driveway No. 2**

Control Type:	Two-way stop	Delay (sec / veh):	13.0
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.050

**Intersection Setup**

Name	Birch Street		Birch Street		Project Driveway No. 2	
Approach	Northbound		Southbound		Westbound	
Lane Configuration						
Turning Movement	Thru	Right	Left	Thru	Left	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Entry Pocket	0	0	0	0	0	0
Entry Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
No. of Lanes in Exit Pocket	0	0	0	0	0	0
Exit Pocket Length [ft]	0.00	0.00	0.00	0.00	0.00	0.00
Speed [mph]	45.00		45.00		30.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

**Volumes**

Name	Birch Street		Birch Street		Project Driveway No. 2	
Base Volume Input [veh/h]	371	22	45	474	22	54
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	371	22	45	474	22	54
Peak Hour Factor	0.8970	0.8970	0.8970	0.8970	0.8970	0.8970
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	103	6	13	132	6	15
Total Analysis Volume [veh/h]	414	25	50	528	25	60
Pedestrian Volume [ped/h]	0		0		0	

**Intersection Settings**

Priority Scheme	Free	Free	Stop
Flared Lane			No
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance			Yes
Number of Storage Spaces in Median	0	0	2



**Movement, Approach, & Intersection Results**

V/C, Movement V/C Ratio	0.00	0.00	0.04	0.01	0.05	0.08
d_M, Delay for Movement [s/veh]	0.00	0.00	8.37	0.00	12.97	10.36
Movement LOS	A	A	A	A	B	B
95th-Percentile Queue Length [veh/ln]	0.00	0.00	0.14	0.00	0.43	0.43
95th-Percentile Queue Length [ft/ln]	0.00	0.00	3.51	0.00	10.79	10.79
d_A, Approach Delay [s/veh]	0.00		0.72		11.13	
Approach LOS	A		A		B	
d_I, Intersection Delay [s/veh]	1.24					
Intersection LOS	B					

**Intersection Level Of Service Report**  
**Intersection 1: Project Driveway No. 1 at Bristol Street**

Control Type:	Two-way stop	Delay (sec / veh):	18.2
Analysis Method:	HCM 6th Edition	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.055

**Intersection Setup**

Name	Project Driveway No. 1		Bristol Street		Bristol Street	
Approach	Northbound		Eastbound		Westbound	
Lane Configuration						
Turning Movement	Left	Right	Thru	Right	Left	Thru
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Entry Pocket	0	0	0	0	0	0
Entry Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
No. of Lanes in Exit Pocket	0	0	0	0	0	0
Exit Pocket Length [ft]	0.00	0.00	0.00	0.00	0.00	0.00
Speed [mph]	30.00		45.00		30.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

**Volumes**

Name	Project Driveway No. 1		Bristol Street		Bristol Street	
Base Volume Input [veh/h]	0	15	1479	20	0	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	0	15	1479	20	0	0
Peak Hour Factor	0.9500	0.9580	0.9580	0.9580	0.9500	0.9500
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	4	386	5	0	0
Total Analysis Volume [veh/h]	0	16	1544	21	0	0
Pedestrian Volume [ped/h]	0		0		0	

**Intersection Settings**

Priority Scheme	Stop	Free	Free
Flared Lane			
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0




**Movement, Approach, & Intersection Results**

V/C, Movement V/C Ratio	0.00	0.06	0.02	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	0.00	18.18	0.00	0.00	0.00	0.00
Movement LOS		C	A	A		
95th-Percentile Queue Length [veh/ln]	0.00	0.17	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft/ln]	0.00	4.37	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	18.18		0.00		0.00	
Approach LOS	C		A		A	
d_I, Intersection Delay [s/veh]	0.18					
Intersection LOS	C					

**Intersection Level Of Service Report**  
**Intersection 2: Birch Street at Project Driveway No. 2**

Control Type:	Two-way stop	Delay (sec / veh):	12.4
Analysis Method:	HCM 6th Edition	Level Of Service:	B
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.022

**Intersection Setup**

Name	Birch Street		Birch Street		Project Driveway No. 2	
Approach	Northbound		Southbound		Westbound	
Lane Configuration						
Turning Movement	Thru	Right	Left	Thru	Left	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Entry Pocket	0	0	0	0	0	0
Entry Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00
No. of Lanes in Exit Pocket	0	0	0	0	0	0
Exit Pocket Length [ft]	0.00	0.00	0.00	0.00	0.00	0.00
Speed [mph]	45.00		45.00		30.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

**Volumes**

Name	Birch Street		Birch Street		Project Driveway No. 2	
Base Volume Input [veh/h]	442	10	20	453	10	25
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	442	10	20	453	10	25
Peak Hour Factor	0.9400	0.9400	0.9400	0.9400	0.9400	0.9400
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	118	3	5	120	3	7
Total Analysis Volume [veh/h]	470	11	21	482	11	27
Pedestrian Volume [ped/h]	0		0		0	



**Intersection Settings**

Priority Scheme	Free	Free	Stop
Flared Lane			No
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance			Yes
Number of Storage Spaces in Median	0	0	2

**Movement, Approach, & Intersection Results**

V/C, Movement V/C Ratio	0.00	0.00	0.02	0.00	0.02	0.04
d_M, Delay for Movement [s/veh]	0.00	0.00	8.41	0.00	12.37	10.06
Movement LOS	A	A	A	A	B	B
95th-Percentile Queue Length [veh/ln]	0.00	0.00	0.06	0.00	0.18	0.18
95th-Percentile Queue Length [ft/ln]	0.00	0.00	1.49	0.00	4.52	4.52
d_A, Approach Delay [s/veh]	0.00		0.35		10.73	
Approach LOS	A		A		B	
d_I, Intersection Delay [s/veh]	0.57					
Intersection LOS	B					

# **Attachment No. CD 4**

Project Plans



# STARBUCKS COFFEE

## NEWPORT BEACH, CA

### PROJECT INFORMATION

#### PROJECT LOCATION

4501 JAMBOREE RD  
NEWPORT BEACH, CA

#### PROJECT DESCRIPTION

PROPOSED IS A STARBUCKS TAKEOVER OF THE EXISTING BUILDING AND SITE. THE EXISTING BUILDING WILL BE REDUCED IN SIZE TO APPROXIMATELY 2,117 SF AND REBRANDED TO A STARBUCKS UTILIZING NEW PAINT COLORS, MATERIALS, AND SIGNAGE. STARBUCKS WILL HAVE APPROXIMATELY 25 INTERIOR SEATS AND APPROXIMATELY 16 OUTDOOR SEATS.

ONCE THE REQUIRED ACCESSIBILITY REQUIREMENTS AND CIRCULATION MODIFICATIONS ARE IMPLEMENTED INTO THE SITE THE TOTAL PARKING COUNT WILL REMAIN 30 PARKING SPACES AND 9 CARS IN THE DRIVE-THRU QUEUE.

THE PROPOSED BUSINESS HOURS OF THE STARBUCKS ARE 5:00AM TO 11:00PM SUNDAY THROUGH SATURDAY.

STARBUCKS MAY HAVE APPROXIMATELY 8 TO 9 EMPLOYEES DURING PEAK TIMES IN THE STORE AND UP TO APPROXIMATELY 30 TOTAL EMPLOYEE COUNT.

#### ASSESSOR'S PARCEL NUMBER

APN 405-161-03

#### LEGAL DESCRIPTION

REAL PROPERTY IN THE CITY OF NEWPORT BEACH, COUNTY OF ORANGE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

PARCEL 2, IN THE CITY OF NEWPORT BEACH, COUNTY OF ORANGE, STATE OF CALIFORNIA, AS SHOWN ON A MAP FILED IN BOOK 108, PAGES 27 AND 28 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF ORANGE COUNTY.

EXCEPTING THEREFROM ALL MINERAL RIGHTS, INCLUDING WITHOUT LIMITED, ALL OIL, GAS, HYDROCARBON AND SIMILAR RIGHTS AND ALL WATER, WATER RIGHTS, GEOTHERMAL STEAM AND STEAM POWER WITHIN OR UNDERLYING SAID LAND, TOGETHER WITH THE PERPETUAL RIGHT OF DEVELOPMENT THEREOF, WITHOUT HOWEVER, THE RIGHT TO ENTER UPON THE SURFACE AND THE TOP 500 FEET OF THE SUBSURFACE OF SAID LAND, AS RESERVED IN A DEED RECORDED FEBRUARY 14, 1976, IN BOOK 12562, PAGE 1899 OF OFFICIAL RECORDS.

NOTE: THE COMPANY IS PROHIBITED FROM INSURING THE AREA OR QUANTITY OF THE LAND DESCRIBED HEREIN. ANY STATEMENT IN THE ABOVE LEGAL DESCRIPTION OF THE AREA OR QUANTITY OF LAND IS NOT A REPRESENTATION THAT SUCH AREA OR QUANTITY IS CORRECT, BUT IS MADE ONLY FOR INFORMATIONAL AND / OR IDENTIFICATION PURPOSES

#### ZONING

JURISDICTION: CITY OF NEWPORT BEACH , CA

ZONING: P-C (KOLL CENTER)

### CONTACTS

#### APPLICANT

STARBUCKS COFFEE COMPANY  
555 ANTON BLVD, SUITE 300  
COSTA MESA, CA 92626  
t: 714.477.5861  
CONTACT: KAREEM ALI, kaali@starbucks.com

#### APPLICANT REPRESENTATIVE

GREENBERGFARROW  
4695 MACARTHUR COURT  
NEWPORT BEACH, CA 92660  
t: 949.296.0450 f: 949.296.0437  
CONTACT: DOUG COUPER, dcouper@greenbergfarrow.com

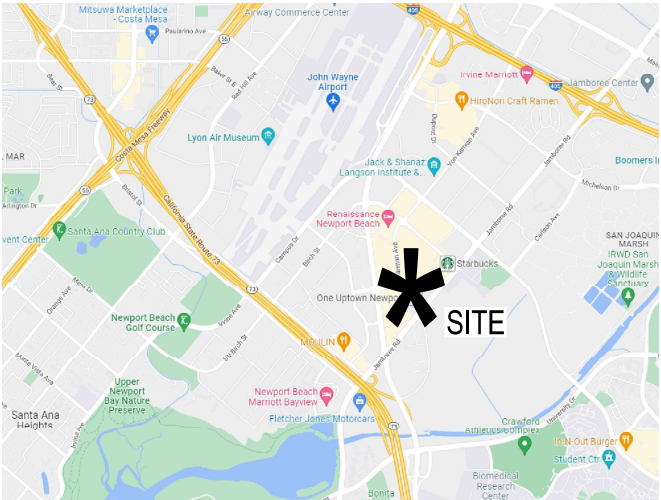
#### ARCHITECT

GREENBERGFARROW  
4695 MACARTHUR COURT  
NEWPORT BEACH, CA 92660  
t: 949.296.0450 f: 949.296.0437  
CONTACT: DOUG COUPER, dcouper@greenbergfarrow.com

#### LANDSCAPE ARCHITECT

WOOD ARCHITECTURE  
1512 W MINERAL KING AVENUE  
VISALIA, CA 93291  
t: 559.786.9600 f: 559.636.3277  
CONTACT: KURT KOETHER, KURT@IWOODARCHITECTURE.COM

### SITE VICINITY MAP



VICINITY MAP  
NOT TO SCALE



### DRAWING INDEX

<input type="checkbox"/> SHEET 00	COVER SHEET
<input type="checkbox"/> SHEETS 01-04	ALTA SURVEY (4 SHEETS)
<input type="checkbox"/> SHEET 05	SITE PLAN
<input type="checkbox"/> SHEET 06	EXISTING FLOOR PLAN
<input type="checkbox"/> SHEET 07	PROPOSED FLOOR PLAN
<input type="checkbox"/> SHEET 08	EXISTING ELEVATIONS
<input type="checkbox"/> SHEET 09	PROPOSED EXTERIOR ELEVATIONS - B&W
<input type="checkbox"/> SHEET 10	PROPOSED EXTERIOR ELEVATIONS - COLOR
<input type="checkbox"/> SHEET 11	CONCEPTUAL GRADING PLAN
<input type="checkbox"/> SHEET 12	SECTIONS & PRECISE GRADING DETAILS
<input type="checkbox"/> SHEET 13	NOT USED
<input type="checkbox"/> SHEET 14	LANDSCAPE CONCEPT PLAN
<input type="checkbox"/> SHEET 15	IRRIGATION CONCEPT PLAN
<input type="checkbox"/> SHEET 16	PHOTOMETRIC PLAN
<input type="checkbox"/> SHEET 17	PHOTO SIMULATIONS

<b>Notes:</b> 1. Signs will be under a separate review and approval of a comprehensive sign program.  2. All signs shall be in compliance wit PC15 Koll Center or an approved sign program and require separate review and permits. Signs under separate reviews and permits.  3. Outdoor light fixtures will be designed, shielded, aimed, located, and maintained to shield adjacent properties and to not produce glare onto adjacent properties or roadways. Parking lot light fixtures and light fixtures on buildings shall be full cut-off fixtures.  4. Light spillover will not exceed one foot-candle at the subject property line.  5. Parking area light standards shall be the minimum height required to effectively illuminate the parking area and eliminate spillover of light and glare onto adjoining properties and roadways.
--



DATE OF FIELD SURVEY

MARCH, 2022

TAX PARCEL NO.

445-161-03

TITLE INFORMATION

THE TITLE INFORMATION SHOWN HEREON IS PER PRELIMINARY REPORT FOR TITLE INSURANCE NO. NCS-1111580-H0U1 DATED FEBRUARY 5, 2022 AS PREPARED BY FIRST AMERICAN TITLE COMPANY, DALLAS, TEXAS [TITLE OFFICER: SHARON MARK, TELEPHONE: (713) 402-4399] NO RESPONSIBILITY OF CONTENT, COMPLETENESS OR ACCURACY OF SAID PRELIMINARY REPORT IS ASSUMED BY THIS MAP OR THE SURVEYOR.

RECORD OWNER

TODD I. SCHIFFMAN, TRUSTEE AND SUCCESSOR TRUSTEE OF THE TODD I. SCHIFFMAN LIVING TRUST DATED DECEMBER 23, 1999

LEGAL DESCRIPTION

REAL PROPERTY IN THE CITY OF NEWPORT BEACH, COUNTY OF ORANGE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

PARCEL 2, IN THE CITY OF NEWPORT BEACH, COUNTY OF ORANGE, STATE OF CALIFORNIA, AS SHOWN ON A MAP FILED IN BOOK 108, PAGES 27 AND 28 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF ORANGE COUNTY.

EXCEPTING THEREFROM ALL MINERAL RIGHTS, INCLUDING WITHOUT LIMITED, ALL OIL, GAS, HYDROCARBON AND SIMILAR RIGHTS AND ALL WATER, WATER RIGHTS, GEOTHERMAL STEAM AND STEAM POWER WITHIN OR UNDERLYING SAID LAND, TOGETHER WITH THE PERPETUAL RIGHT OF DEVELOPMENT THEREOF, WITHOUT HOWEVER, THE RIGHT TO ENTER UPON THE SURFACE AND THE TOP 500 FEET OF THE SUBSURFACE OF SAID LAND, AS RESERVED IN A DEED RECORDED FEBRUARY 14, 1976, IN BOOK 12562, PAGE 1899 OF OFFICIAL RECORDS.

NOTE: THE COMPANY IS PROHIBITED FROM INSURING THE AREA OR QUANTITY OF THE LAND DESCRIBED HEREIN. ANY STATEMENT IN THE ABOVE LEGAL DESCRIPTION OF THE AREA OR QUANTITY OF LAND IS NOT A REPRESENTATION THAT SUCH AREA OR QUANTITY IS CORRECT, BUT IS MADE ONLY FOR INFORMATIONAL AND/OR IDENTIFICATION PURPOSES.

TITLE EXCEPTIONS AND EASEMENTS

1-3 TAXES.

4 AN EASEMENT IN FAVOR OF SOUTHERN CALIFORNIA EDISON COMPANY FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED AS BOOK 1522, PAGE 527 OF OFFICIAL RECORDS.

(DOCUMENT DOES NOT AFFECT)

5 AN EASEMENT FOR PERPETUAL AIR OR FLIGHT EASEMENT, ALSO REFERRED TO AS "NAVIGATION RIGHTS" AND INCIDENTAL PURPOSES IN THE DOCUMENT RECORDED MARCH 17, 1964 AS BOOK 6965, PAGE 721 OF OFFICIAL RECORDS.

(DOCUMENT AFFECTS - BLANKET IN NATURE)

6 AN EASEMENT FOR PRIVATE DRIVE AND INCIDENTAL PURPOSES SHOWN OR DEDICATED ON THE MAP OF TRACT NO. 7953 RECORDED OCTOBER 31, 1972 AND ON FILE IN BOOK 310, PAGE 7-11, OF TRACT MAPS.

(DOCUMENT AFFECTS - CENTERLINE OF 30' PRIVATE DRIVEWAY PLOTTED AS [A] )

7 AN EASEMENT IN FAVOR OF SOUTHERN CALIFORNIA EDISON COMPANY FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 11, 1974 AS BOOK 11073, PAGE 218 OF OFFICIAL RECORDS.

(DOCUMENT AFFECTS - PLOTTED HEREON AS [B] )

8 ABUTTER'S RIGHTS OF INGRESS AND EGRESS TO OR FROM JAMBOREE BOULEVARD EXCEPT AT PRIVATE DRIVE, HAVE BEEN DEDICATED OR RELINQUISHED ON THE MAP OF PARCEL MAP ON FILE IN BOOK 108, PAGE 27-28, OF PARCEL MAPS.

(DOCUMENT AFFECTS - PLOTTED HEREON AS [C] )

9 AN EASEMENT FOR BICYCLE AND PEDESTRIAN PURPOSES AND INCIDENTAL PURPOSES SHOWN OR DEDICATED ON THE MAP OF PARCEL MAP RECORDED DECEMBER 15, 1977 AND ON FILE IN BOOK 108, PAGE 27-28, OF PARCEL MAPS.

(DOCUMENT AFFECTS - PLOTTED HEREON AS [C] )

10 COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED JANUARY 03, 1978 AS BOOK 12516, PAGE 1162 OF OFFICIAL RECORDS.

(DOCUMENT AFFECTS - BLANKET IN NATURE)

11 AN EASEMENT IN FAVOR OF SOUTHERN CALIFORNIA EDISON COMPANY, A CORPORATION, ITS SUCCESSORS AND ASSIGNS FOR RIGHT OF WAY TO CONSTRUCT, USE, MAINTAIN, OPERATE, ALTER, ADD TO, REPAIR, REPLACE, RECONSTRUCT, INSPECT AND REMOVE AT ANY TIME AND FROM TIME TO TIME UNDERGROUND ELECTRICAL SUPPLY SYSTEMS AND COMMUNICATION SYSTEMS (HEREINAFTER REFERRED TO AS "SYSTEMS") CONSISTING OF WIRES, UNDERGROUND CONDUITS, CABLES, VAULTS, MANHOLES, HANDHOLES AND INCLUDING ABOVE GROUND ENCLOSURES, MARKERS AND CONCRETE PADS AND OTHER APPURTENANT FIXTURES AND EQUIPMENT NECESSARY OR USEFUL FOR DISTRIBUTING ELECTRICAL ENERGY AND FOR TRANSMITTING INTELLIGENCE BY ELECTRICAL MEANS AND INCIDENTAL PURPOSES, RECORDED MAY 05, 1978 AS INSTRUMENT NO. 9042 AS BOOK 12663, PAGE 1499 OF OFFICIAL RECORDS.

(DOCUMENT AFFECTS - PLOTTED HEREON AS [D] )

12 AN EASEMENT FOR EXCLUSIVE EASEMENT FOR PASSAGE AND INCIDENTAL PURPOSES IN THE DOCUMENT RECORDED MAY 26, 1978 AS INSTRUMENT NO. 38246 AS BOOK 12690, PAGE 854 OF OFFICIAL RECORDS.

(DOCUMENT DOES NOT AFFECT)

13 A DEED OF TRUST TO SECURE AN ORIGINAL INDEBTEDNESS OF \$1,050,000.00 RECORDED JANUARY 29, 2013 AS INSTRUMENT NO. 2013000057021 OF OFFICIAL RECORDS, A DOCUMENT ENTITLED "ASSIGNMENT OF LEASES AND RENTS" RECORDED JANUARY 29, 2013 AS INSTRUMENT NO. 2013000057022 OF OFFICIAL RECORDS, AS ADDITIONAL SECURITY FOR THE PAYMENT OF THE INDEBTEDNESS SECURED BY THE DEED OF TRUST, A DOCUMENT RECORDED OCTOBER 27, 2021 AS INSTRUMENT NO. 2021000654355 OF OFFICIAL RECORDS PROVIDES THAT CITIZENS BUSINESS BANK WAS SUBSTITUTED AS TRUSTEE UNDER THE DEED OF TRUST. THE EFFECT OF A DOCUMENT ENTITLED "SUBSTITUTION OF TRUSTEE AND FULL RECONVEYANCE", RECORDED OCTOBER 27, 2021 AS INSTRUMENT NO. 2021000654355 OF OFFICIAL RECORDS.

(NOT SURVEY MATTERS)

14 WATER RIGHTS, CLAIMS OR TITLE TO WATER, WHETHER OR NOT SHOWN BY THE PUBLIC RECORDS.

15 RIGHTS OF PARTIES IN POSSESSION.

BASIS OF BEARINGS

THE BEARINGS SHOWN HEREON ARE BASED ON THE CENTERLINE OF JAMBOREE ROAD AS SHOWN ON PARCEL MAP FILED IN PARCEL MAP BOOK 108, PAGES 27-28, BEING NORTH 40°38'26" EAST.

LINE LEGEND

PROPERTY LINE  
CENTER LINE  
RIGHT-OF-WAY LINE  
UNDERLYING LOT LINE  
TIE LINE  
EASEMENT  
SETBACK LINES

PROPERTY LINE  
CENTER LINE  
RIGHT-OF-WAY LINE  
UNDERLYING LOT LINE  
TIE LINE  
EASEMENT  
SETBACK LINES

BIRCH STREET  
(PUBLIC STREET)

REFERENCES

R1 PARCEL MAP - P.M.B. 108/27-28.

(...) INDICATES RECORD OR CALCULATED FROM RECORD DATA AS NOTED.

M&R INDICATES MEASURE DATA AND RECORD DATA ARE THE SAME.

MONUMENT AND ESTABLISHMENT NOTES

● INDICATES FOUND MONUMENT AS NOTED BELOW.

① INDICATES FOUND 2" BRASS CAP PUNCHED "LS 3109" IN O.C.S. WELL MONUMENT, PER CORNER RECORD 2013-3386, DOWN 1.2'.

② INDICATES FOUND 1" IRON PIPE TAGGED "L.S. 3109", PER R1, DOWN 0.7'.

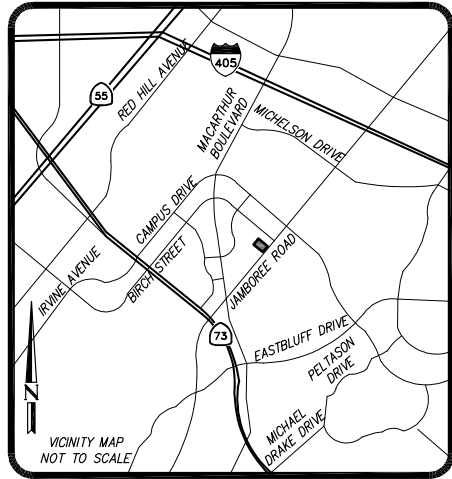
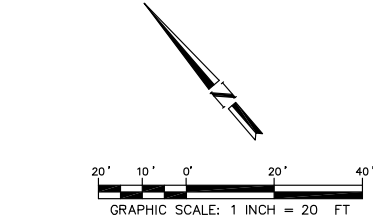
③ INDICATES FOUND 2" IRON PIPE WITH NAIL, NO TAG, NO REFERENCE, DOWN 1.0'. SAID POINT IS LOCATED NORTH 49°21'34" WEST, 0.26' FROM PROPERTY CORNER AS ESTABLISHED BY HOLDING R1 DATA.

④ INDICATES SEARCHED, NOTHING FOUND. ESTABLISHED BY HOLDING RECORD DATA PER R1.

⑤ INDICATES FOUND 1" IRON PIPE WITH ILLEGIBLE TAG, NO REFERENCE, DOWN 1.0'. ACCEPTED AS WEST CORNER OF PARCEL 1 OF R1.

⑥ INDICATES FOUND LEAD, TACK AND TAG STAMPED "LS 7223", NO REFERENCE, FLUSH. SAID MONUMENT IS LOCATED NORTH 49°21'34" WEST, 0.15' FROM PROPERTY CORNER AS ESTABLISHED BY HOLDING R1 DATA.

⑦ INDICATES FOUND MAG NAIL AND WASHER STAMPED "LS 5411", PER CORNER RECORD 2013-3383, FLUSH.



NO.: REVISION:

ALTA/NSPS TABLE "A" ITEM NOTES

ITEM 2 THE SITE ADDRESS OBSERVED WHILE CONDUCTING THE SURVEY IS: 4501 JAMBOREE ROAD, NEWPORT BEACH 92660

ITEM 3 THE LAND SHOWN ON THIS SURVEY LIES ENTIRELY WITHIN FLOOD ZONE "X" (UN-SHADED), BEING DESCRIBED AS "AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN" PER FLOOD INSURANCE RATE MAP (FIRM) - COMMUNITY PANEL NUMBER 06059C0286J DATED DECEMBER 3, 2009.

ITEM 4 THE GROSS LAND AREA IS: 30,000 SF / 0.689 ACRES

ITEM 5 THE CONTOURS AND ELEVATIONS SHOWN HEREON ARE BASED ON THE FOLLOWING BENCHMARK:

BM No.: 3J-13-70 ELEV.: 54.540' (COUNTY OF ORANGE BENCHMARK)  
DATUM: NAVD 88  
DESCRIPTION: DESCRIBED BY OCS 2001 - FOUND 3 3/4" OCS ALUMINUM BENCHMARK DISK STAMPED "3J-13-70", SET IN THE TOP OF A 4 IN. BY 4 IN. CONCRETE POST. MONUMENT IS LOCATED IN THE SOUTHERLY CORNER OF THE INTERSECTION OF JAMBOREE ROAD AND FAIRCHILD, 72.3 FT. WESTERLY OF THE CENTERLINE OF FAIRCHILD AND 57.2 FT. SOUTHERLY OF THE CENTER OF THE CENTER MEDIAN ALONG MACARTHUR, 2.8 FT. WESTERLY OF THE WEST END OF THE SOUTHWEST CURB RETURN. MONUMENT IS DOWN 0.1 FT. FROM THE TOP OF CURB.  
CONTOUR INTERVAL=1'

ITEM 6(a) THE CURRENT ZONING CLASSIFICATION IS \_\_\_\_\_ PER THE CITY OF ...

(INSERT ZONING INFORMATION HERE - INDICATE SOURCE AND DATE OF INFO)

ITEM 6(b) SEE THE SURVEY PLAT FOR THE LOCATION OF SETBACK LINES.

ALTA/NSPS TABLE "A" ITEM NOTES

ITEM 7(a) SEE THE SURVEY PLAT FOR EXTERIOR DIMENSIONS OF ALL BUILDINGS AT GROUND LEVEL.

ITEM 7(b)(1) SEE THE SURVEY PLAT FOR THE SQUARE FOOTAGE OF THE EXTERIOR FOOTPRINT OF ALL BUILDINGS AT GROUND LEVEL.

ITEM 7(c) SEE THE SURVEY PLAT FOR THE MEASURED HEIGHT ABOVE GRADE OF ALL BUILDINGS.

ITEM 8 SEE THE SURVEY PLAT FOR ANY SUBSTANTIAL FEATURES OBSERVED IN THE PROCESS OF CONDUCTING THE SURVEY.

ITEM 9 SEE THE SURVEY PLAT FOR ANY PARKING STRIPING AND TYPE OF PARKING SPACE. THE PARKING COUNT IS AS FOLLOWS:

REGULAR SPACES 28  
HANDICAP SPACES 2  
TOTAL SPACES 30

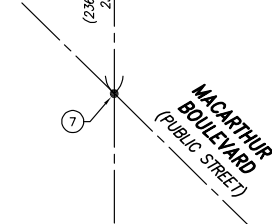
ITEM 13 SEE THE SURVEY PLAT FOR THE NAMES OF ADJOINING OWNERS.

ITEM 14 SEE THE SURVEY PLAT FOR THE DISTANCE TO THE NEAREST INTERSECTING STREET.

ITEM 16 THERE IS NO OBSERVABLE EVIDENCE OF EARTH MOVING WORK WITHIN RECENT MONTHS.

ITEM 17 THE SURVEYOR IS NOT AWARE OF OR HAS NOT BEEN PROVIDED EVIDENCE REGARDING PROPOSED CHANGES IN STREET RIGHT OF WAY LINES, OR OBSERVED EVIDENCE OF RECENT STREET OR SIDEWALK CONSTRUCTION OR REPAIRS.

ITEM 18 THE COMMITMENT DID NOT LIST ANY OFFSITE EASEMENTS OR SERVITUDES BENEFITING THE SURVEYED PROPERTY.



SURVEYOR'S CERTIFICATE

TO: STARBUCKS CORPORATION, A WASHINGTON CORPORATION, AND ITS AFFILIATES; AND FIRST AMERICAN TITLE INSURANCE COMPANY:

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2021 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 2, 3, 4, 5, 6(a), 6(b), 7(a), 7(b)(1), 7(c), 8, 9, 11, 13, 14, 16, 17, 18, 19 AND 20 OF TABLE "A" THEREOF. THE FIELD WORK WAS COMPLETED ON MAY 11, 2021.

PRELIMINARY

JAKE W. LAPPERT  
PLS 9303  
EMAIL: jlappert@drc-eng.com

DATE: 03/25/2022



THE LEASE AREA IS 30,000 SF/0.689 ACRES

ALTA/NSPS LAND TITLE SURVEY



160 S. Old Springs Road, Ste. 210  
Anaheim Hills, California 92808

(714) 685-6860

DRC  
PROJECT NO.: 22-318

PREPARED FOR: STARBUCKS CORPORATION



LOCATED AT: 4501 JAMBOREE ROAD  
NEWPORT BEACH, CALIFORNIA

STARBUCKS PROJECT NO.: RE-

SCALE:  
1" = 20'

DRAWN BY:  
JWL

SHEET NUMBER:

1

DATE:  
03/25/2022

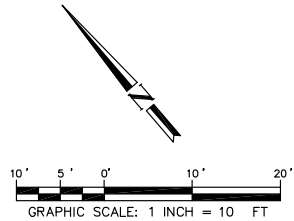
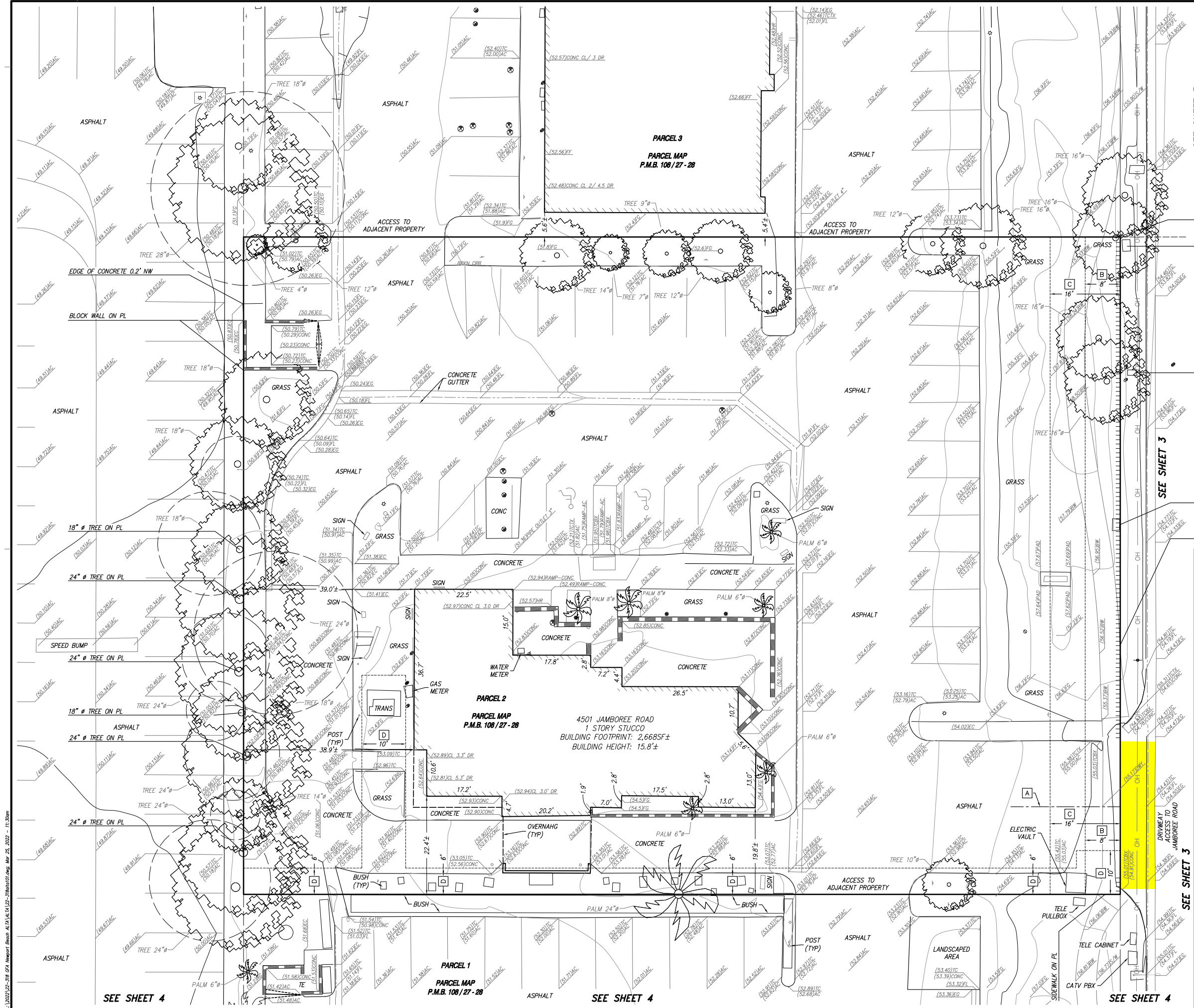
CHECKED BY:  
JWL

OF 4 SHEETS

PA2024-0035



L:\2023\23-318 GFA Newport Beach ALTA\ALTA\23-318\Sheet01.dwg, Mar 25, 2023 - 11:30am



**NOTE**  
SEE SHEET 4 FOR ABBREVIATIONS AND SYMBOL LEGEND

LINE LEGEND	
	PROPERTY LINE
	CENTER LINE
	RIGHT-OF-WAY LINE
	UNDERLYING LOT LINE
	EASEMENT
	OVERHEAD LINES
	BLOCK WALL
	BUILDING FOOTPRINT
	DRIVEWAY

NO.	REVISION

THE LEASE AREA IS 30,000 SF/0.689 ACRES

**ALTA/NSPS LAND TITLE SURVEY**

**DRC Engineering, Inc.**  
Civil Engineering/Land Surveying/Land Planning

160 S. Old Springs Road, Ste. 210  
Anaheim Hills, California 92808 (714) 685-6860 DRC PROJECT NO.: 22-318

PREPARED FOR: **STARBUCKS CORPORATION**

LOCATED AT: **4501 JAMBOREE ROAD  
NEWPORT BEACH, CALIFORNIA**

STARBUCKS PROJECT NO.: **RE-**

SCALE: 1" = 10'	DRAWN BY: JWL	SHEET NUMBER: <b>2</b> OF 4 SHEETS
DATE: 03/25/2022	CHECKED BY: JWL	

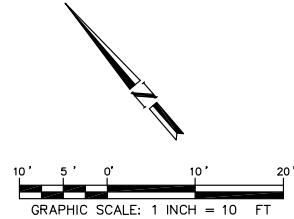
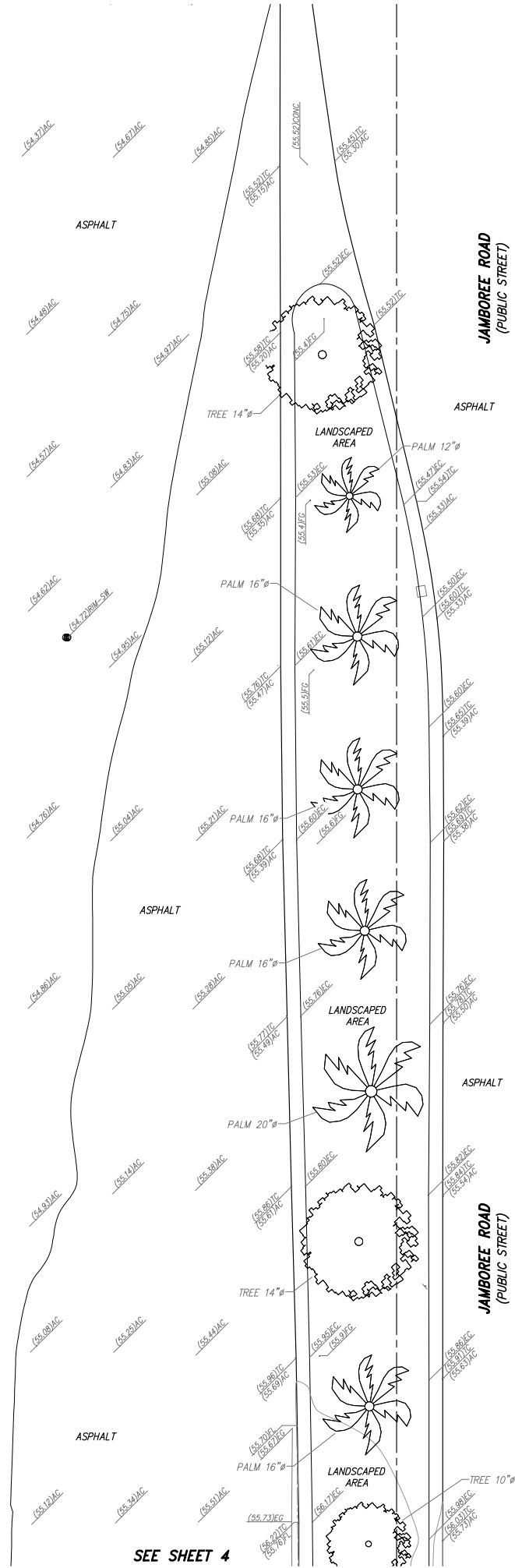
L:\2022\22-318 GFA Newport Beach ALTA\ALTA\22-318\altdwg.dwg, Mar 25, 2022 - 11:30am

SEE SHEET 2

SEE SHEET 2

SEE SHEET 2

SEE SHEET 4



**NOTE**  
SEE SHEET 4 FOR ABBREVIATIONS AND SYMBOL LEGEND

NO.	REVISION

THE LEASE AREA IS 30,000 SF/0.689 ACRES

ALTA/NSPS LAND TITLE SURVEY



160 S. Old Springs Road, Ste. 210  
Anaheim Hills, California 92808

(714) 685-6860

DRC  
PROJECT NO.: 22-318

PREPARED FOR: **STARBUCKS CORPORATION**



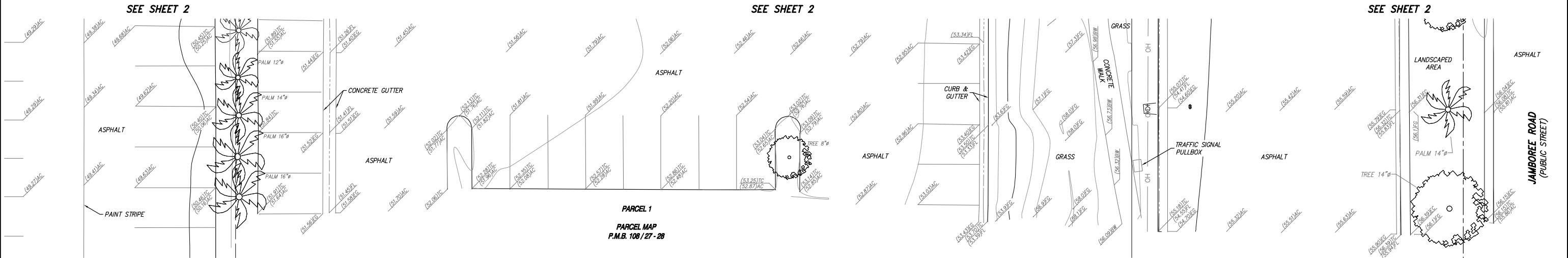
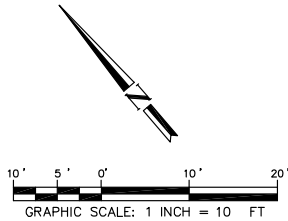
LOCATED AT: **4501 JAMBOREE ROAD  
NEWPORT BEACH, CALIFORNIA**

STARBUCKS  
PROJECT NO.: **RE-**

SCALE: 1" = 10'	DRAWN BY: JWL	SHEET NUMBER:  <b>3</b>  OF 4 SHEETS
DATE: 03/25/2022	CHECKED BY: JWL	

PA2024-0035





NO.:	REVISION:

ABBREVIATIONS

ASPH	ASPHALT	L/S	LANDSCAPING VEGETATION
BW	BACK OF WALK	MH	MANHOLE
CATV	CABLE TELEVISION	NG	NATURAL GROUND
CL	CENTERLINE	PL	PROPERTY LINE
CONC	CONCRETE	SD	STORM DRAIN
EG	EDGE OF CONCRETE	SF	SQUARE FEET
EG	EDGE OF GUTTER	S/W	SIDEWALK
EP	EDGE OF PAVEMENT	TC	TOP OF CURB
FF	FINISHED FLOOR	TE	TRASH ENCLOSURE
FG	FINISHED GRADE	TELE	TELEPHONE
FL	FLOWLINE	TXBX	TOP OF CURB - BOTTOM OF "X"
HR	HANDICAP RAMP	TCTX	TOP OF CURB - TOP OF "X"
INV	INVERT	TYP	TYPICAL

SYMBOL LEGEND

	BUSH
	FIRE HYDRANT
	GATE
	GUY ANCHOR
	GUY ANCHOR POLE
	HOSE BIB
	LANDSCAPE LIGHT
	LIGHT POLE
	PALM TREE
	PEDESTRIAN CROSSING SIGNAL
	POWER POLE
	SIGN
	STREET LIGHT
	TREE
	TRAFFIC SIGNAL WITH ARM
	TRAFFIC SIGNAL ON POLE
	UTILITY PAINT MARK
	OBSERVATION MONITORING WELL
METERS	
	ELECTRIC
	GAS
	WATER

MANHOLES	
	CABLE TELEVISION
	ELECTRIC
	GREASE INTERCEPTOR
	SEWER
	SEWER CLEANOUT
	STORM DRAIN
	STORM DRAIN CLEANOUT
	TELEPHONE
	UNKNOWN UTILITY

VALVES	
	BACKFLOW PREVENTER
	BLOW-OFF/AIR VAC VENT
	FIRE DEPARTMENT CONNECTOR
	GAS
	IRRIGATION CONTROL VALVE
	POST INDICATOR VALVE
	SEWER
	UNKNOWN UTILITY
	WATER

THE LEASE AREA IS 30,000 SF/0.689 ACRES

ALTA/NSPS LAND TITLE SURVEY



160 S. Old Springs Road, Ste. 210  
Anaheim Hills, California 92808

(714) 685-6860

DRC  
PROJECT NO.: 22-318

PREPARED FOR: STARBUCKS CORPORATION



LOCATED AT: 4501 JAMBOREE ROAD  
NEWPORT BEACH, CALIFORNIA

STARBUCKS  
PROJECT NO.: RE-

SCALE:  
1" = 10'

DRAWN BY:  
JWL

SHEET NUMBER:

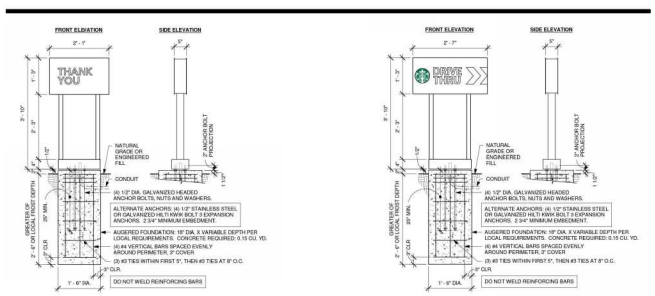
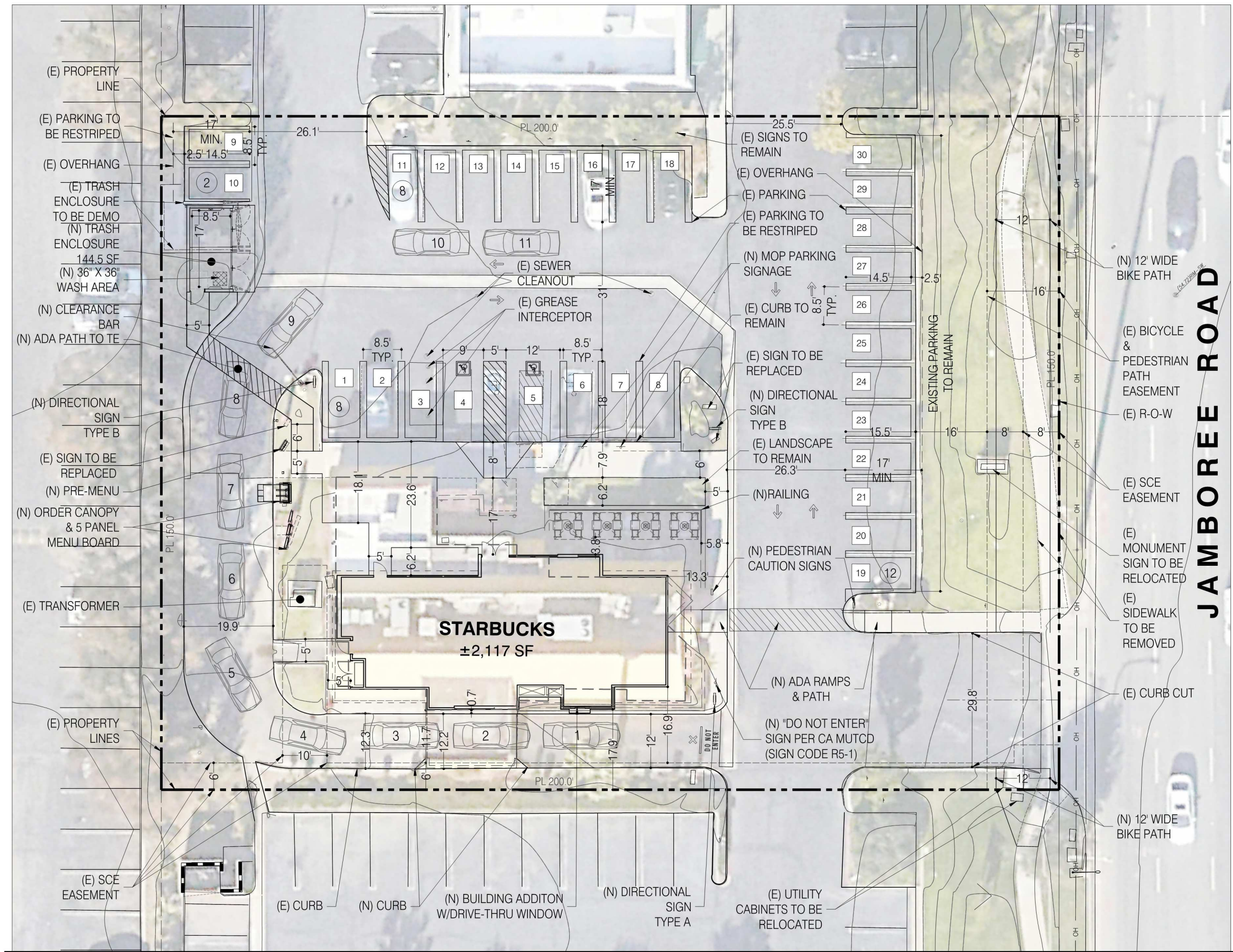
4

DATE:  
03/25/2022

CHECKED BY:  
JWL

OF 4 SHEETS





SIGN TYPE A (NTS) SIGN TYPE B (NTS)

PROJECT INFORMATION

ZONING CLASSIFICATION	
JURISDICTION	CITY OF NEWPORT BEACH, CA
EXISTING ZONE	PC15 - PLANNED COMMUNITY 15 KOLL CENTER NEWPORT

SITE AREA	
NET SITE AREA:	± 0.689 AC (± 30,000 SF)
SITE COVERAGE:	± 7.05% (± 3,073 SF/AC)

HOURS OF OPERATIONS	
5:00 AM - 11:00 AM 7 DAYS A WEEK	

BUILDING INFORMATION	
EXISTING	
EXISTING FAST FOOD	± 2,668 SF

PROPOSED	
STARBUCKS	± 2,117 SF
INDOOR SEATING	± 25 SEATS
OUTDOOR SEATING/PATIO	± 16 SEATS

PARKING SUMMARY	
EXISTING PARKING	
USER	SPACES PROVIDED
EXISTING BUILDING	
STANDARD	28
ACCESSIBLE	2
TOTAL	30
VEHICLES ACCOMMODATED BY THE DRIVE-THRU LANE	
	8

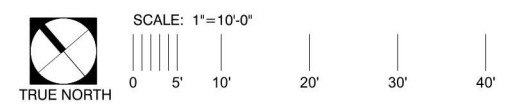
PROPOSED PARKING			
USER	RATIO REQUIRED	SPACES REQUIRED	SPACES PROVIDED
STARBUCKS			
incl OUTDOOR SEATING			
STANDARD			28
ACCESSIBLE			2
TOTAL			30
VEHICLES ACCOMMODATED BY THE DRIVE-THRU LANE			
			± 9

\* PROPOSED LAYOUT MAINTAINS SAME NUMBER OF PARKING OF STALLS AS EXISTING CONDITIONS.

- PROJECT NOTES
- THIS CONCEPTUAL SITE PLAN IS FOR PLANNING SUBMITTAL PURPOSES ONLY.
  - THIS SITE PLAN IS BASED ON A CAD AND PDF FILE OF AN ALTA SURVEY PREPARED BY DRC ENGINEERING, INC AND DATED 03/25/2022, AND AN AERIAL.
  - ALL SIGNS SHALL COMPLY WITH CHAPTER 20.42 OF THE ZONING CODE OR AN APPROVED SIGN PROGRAM. ALL SIGNS REQUIRE SEPARATE REVIEW AND PERMITS.
  - ANY LANDSCAPE WITHIN SIGHT TRIANGLES SHALL BE MAINTAINED BELOW 24".

DRAWING ISSUE/REVISION RECORD		
DATE	NARRATIVE	INITIALS
12.13.2022	PREP SP-8 FOR SUBMITTAL	II
02.05.2024	PREP SP-9	AG
03.22.2024	REV SP-9	AG

GREENBERG FARROW CONTACTS	
PROJECT MANAGER	I. IBRAHIMBEGOVIC
SITE DEV. COORDINATOR	D. COUPER







DEMOLITION LEGEND

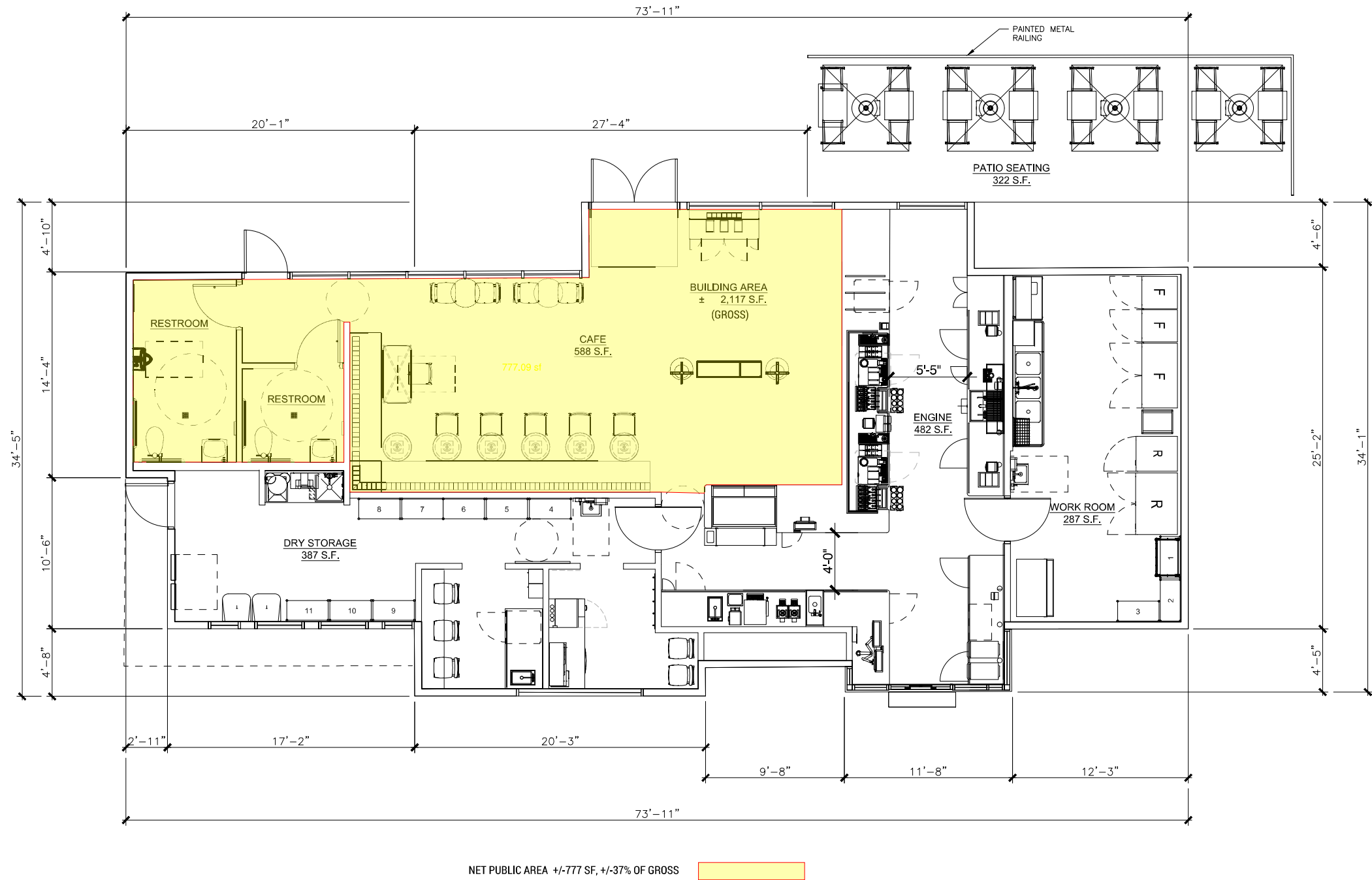
- EXISTING BUILDING WALL TO BE DEMOLISHED
- EXISTING LOW WALL TO BE DEMOLISHED
- EXISTING DOOR TO BE DEMOLISHED
- NET PUBLIC AREA +/-928 SF, +/-35% OF GROSS

1 EXISTING FLOOR PLAN  
SCALE: 1/4" = 1'-0"

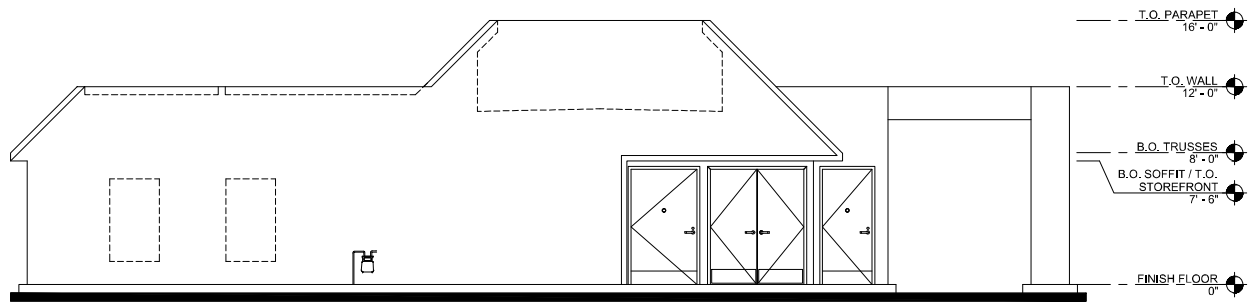
BASED ON AS-BUILT SURVEY DATED 8/23/2022



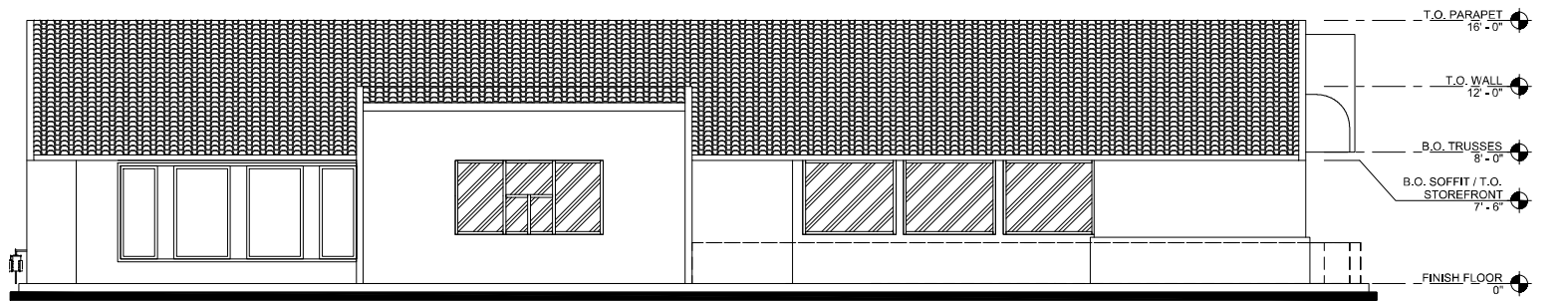
TRUE NORTH



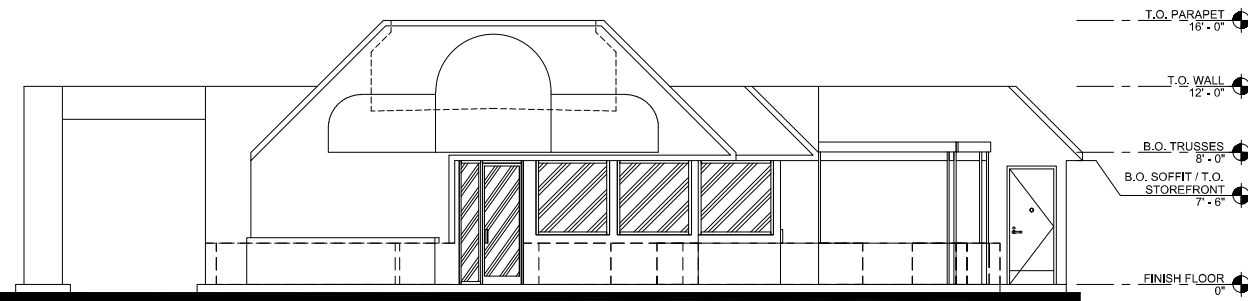
1 PROPOSED FLOOR PLAN  
SCALE: 1/4" = 1'-0"



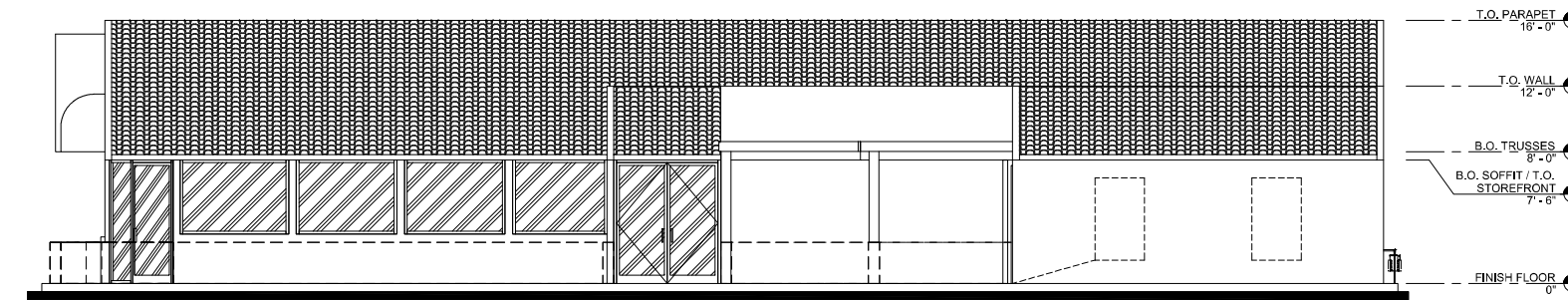
① NORTH-WEST ELEVATION  
SCALE: 3/16" = 1'-0"



② SOUTH-WEST ELEVATION  
SCALE: 3/16" = 1'-0"



③ SOUTH-EAST ELEVATION  
SCALE: 3/16" = 1'-0"



④ NORTH-EAST ELEVATION  
SCALE: 3/16" = 1'-0"



4695 MacArthur Court  
Suite 1400N  
Newport Beach, CA 92660  
T:949 296 0450



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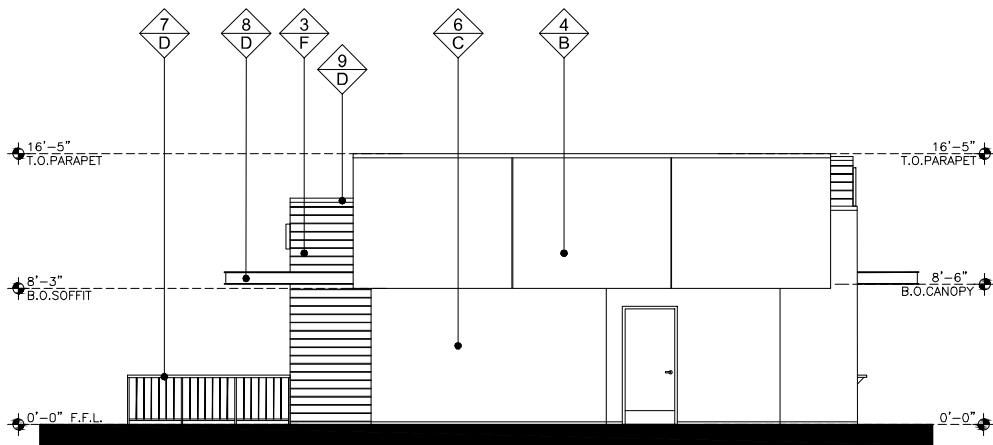
**STARBUCKS**  
BIRCH & JAMBOREE  
NEWPORT BEACH, CA

20220083.0

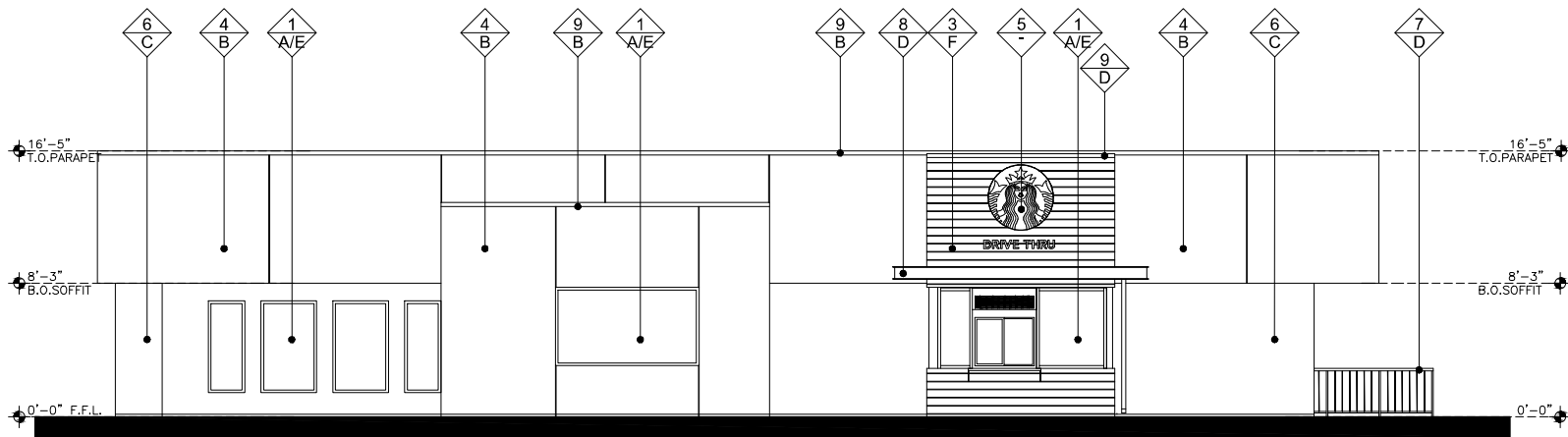
**EXISTING ELEVATIONS**  
**SHEET 08**

12.15.2022

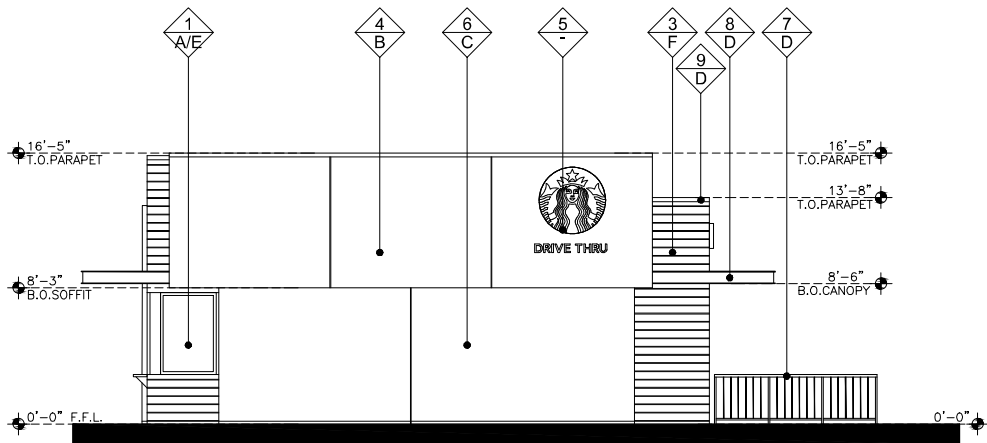
**PA2024-0035**



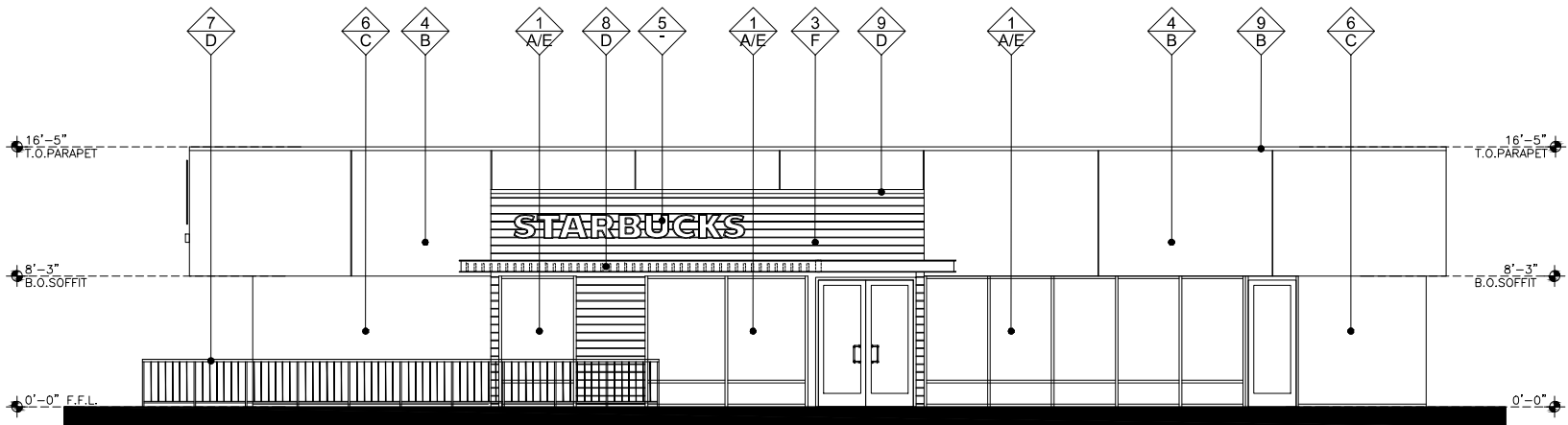
① NORTH WEST ELEVATION  
SCALE: 3/16" = 1'-0"



② SOUTH WEST ELEVATION  
SCALE: 3/16" = 1'-0"



③ SOUTH EAST ELEVATION  
SCALE: 3/16" = 1'-0"



④ NORTH EAST ELEVATION  
SCALE: 3/16" = 1'-0"

FINISH SCHEDULE			
▲	FINISH MATERIAL	▼	FINISH COLOR
1	ALUMINUM STOREFRONT DOORS / WINDOWS SYSTEM	A	PRE-FINISHED - ANODIZED DARK BRONZE
2	HOLLOW METAL DOOR AND FRAME	B	TO MATCH SW 6260 UNIQUE GRAY
3	WOOD SHIP LAP HORIZONTAL SIDING	C	TO MATCH SW 7076 CYBERSPACE
4	CONCRETE LOOK STUCCO FINISH SYSTEM WITH INTEGRAL COLOR	D	TO MATCH SW 6992 INK WELL
5	SIGNAGE (UNDER SEPARATE PERMIT)	E	DOUBLE PANE CLEAR GLASS
6	STUCCO SYSTEM SMOOTH FINISH PAINTED	F	RUSTIC
7	METAL RAILING - PAINTED		
8	METAL CANOPY - PAINTED		
9	METAL COPING - PRE FINISHED TO MATCH		
		Note: Colors shown on these elevations are for illustration purposes only. For actual colors, refer to manufacturer's samples.	



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**STARBUCKS**  
BIRCH & JAMBOREE  
NEWPORT BEACH, CA

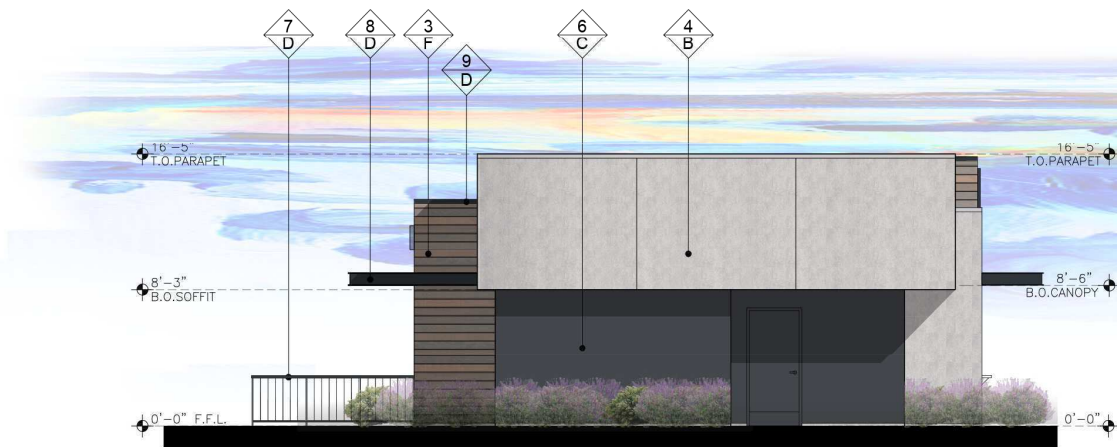
20220083.0

**EXTERIOR ELEVATIONS - B&W**  
**SHEET 09**

12.15.2022

PA2024-0035





① NORTH WEST ELEVATION  
SCALE: 3/16" = 1'-0"



② SOUTH WEST ELEVATION  
SCALE: 3/16" = 1'-0"



③ SOUTH EAST ELEVATION  
SCALE: 3/16" = 1'-0"



④ NORTH EAST ELEVATION  
SCALE: 3/16" = 1'-0"

FINISH SCHEDULE			
FINISH MATERIAL		FINISH COLOR	
1	ALUMINUM STOREFRONT DOORS / WINDOWS SYSTEM	A	PRE-FINISHED - ANODIZED DARK BRONZE
2	HOLLOW METAL DOOR AND FRAME	B	TO MATCH SW 6260 UNIQUE GRAY
3	WOOD SHIPLAP HORIZONTAL SIDING	C	TO MATCH SW 7076 CYBERSPACE
4	CONCRETE LOOK STUCCO FINISH SYSTEM WITH INTEGRAL COLOR	D	TO MATCH SW 6992 INKWELL
5	SIGNAGE (UNDER SEPARATE PERMIT)	E	DOUBLE PANE CLEAR GLASS
6	STUCCO SYSTEM SMOOTH FINISH PAINTED	F	RUSTIC
7	METAL RAILING - PAINTED		
8	METAL CANOPY - PAINTED		
9	METAL COPING - PRE FINISHED TO MATCH		
		Note: Colors shown on these elevations are for illustration purposes only. For actual colors, refer to manufacturer's samples.	



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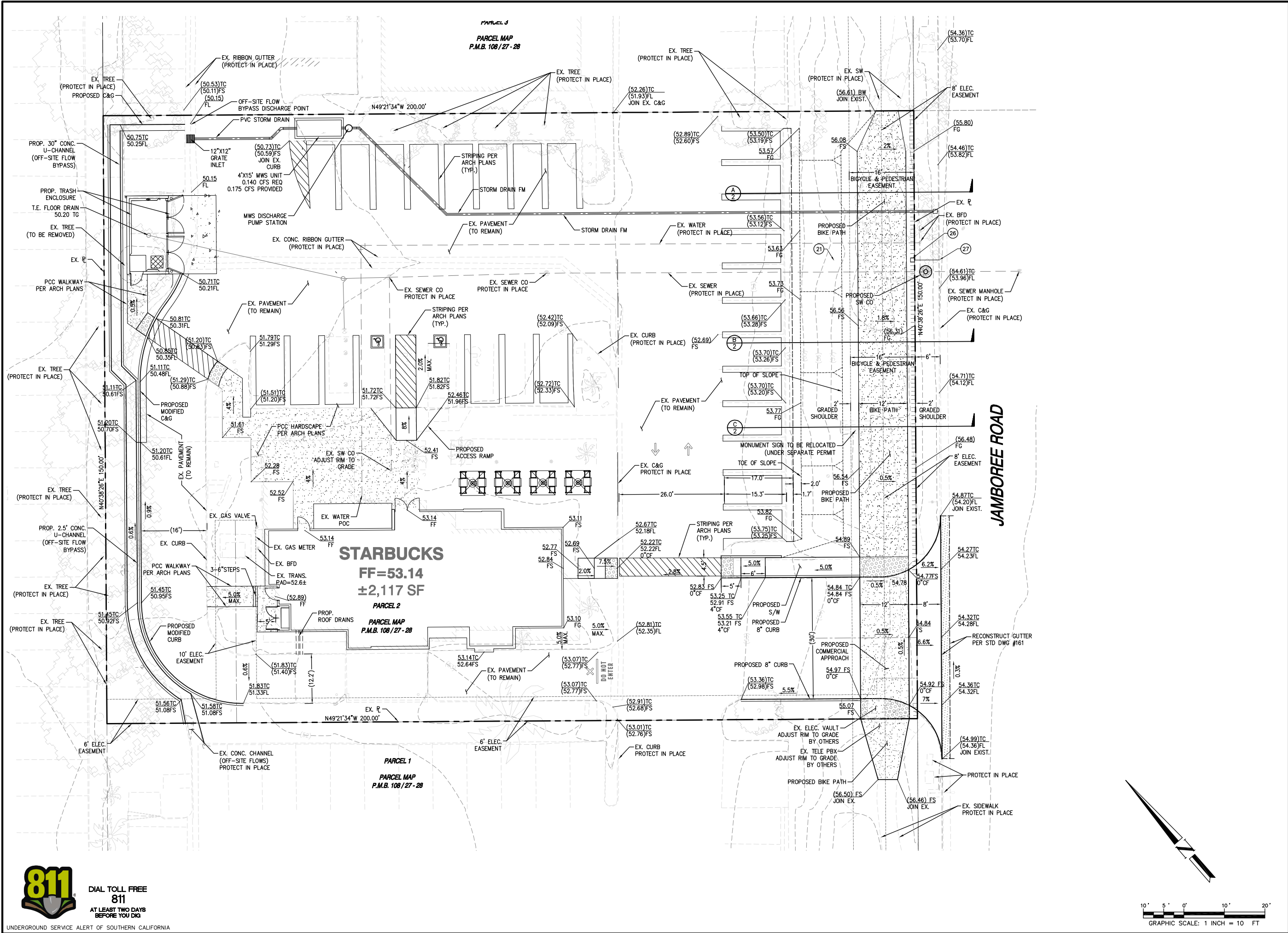
**STARBUCKS**  
BIRCH & JAMBOREE  
NEWPORT BEACH, CA

**EXTERIOR ELEVATIONS - COLOR**  
**SHEET 10**

20220083.0 12.15.2022

PA2024-0035





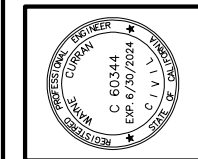
DIAL TOLL FREE  
811  
AT LEAST TWO DAYS  
BEFORE YOU DIG

UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA

160 S. Old Springs Road  
Suite 210  
Anheim Hills, CA 92808  
714-685-6860

**PREPARED BY:**  
**dlrc** Engineering, Inc.  
Civil Engineering/Land Surveying/Land Planning

**NOT FOR CONSTRUCTION**  
WAYNE CURRAN  
R.C.E. 60344  
DATE



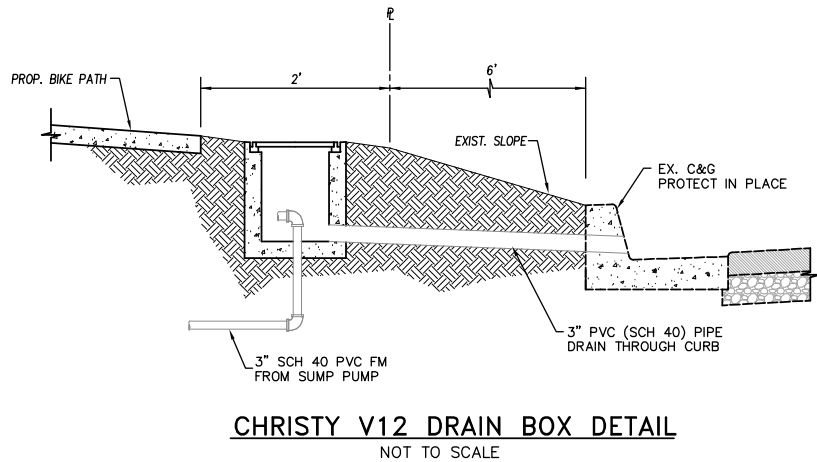
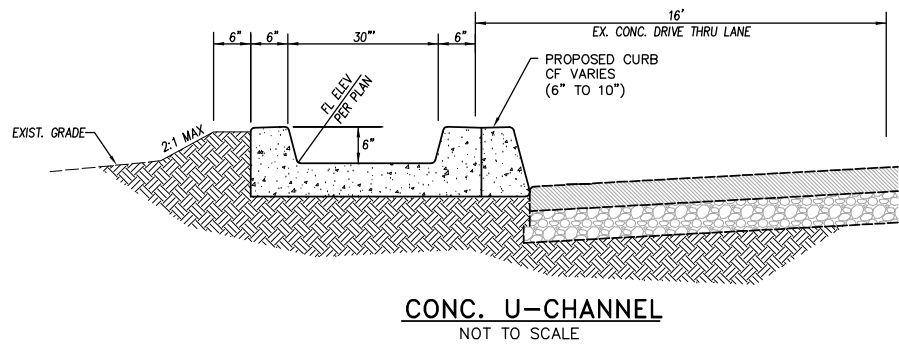
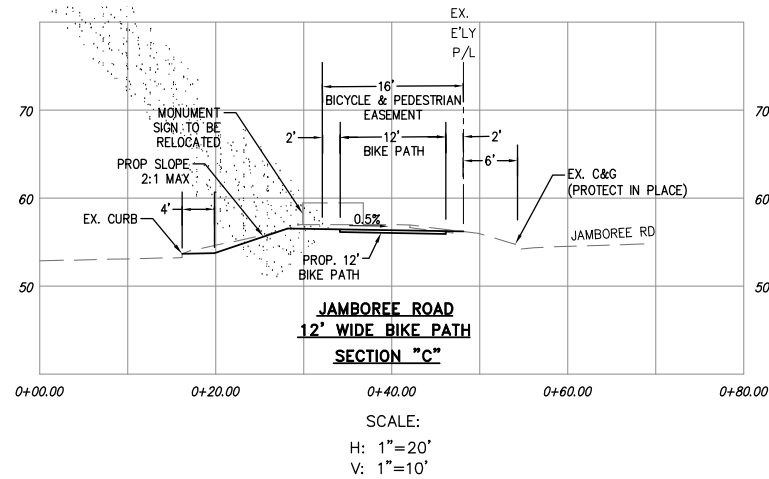
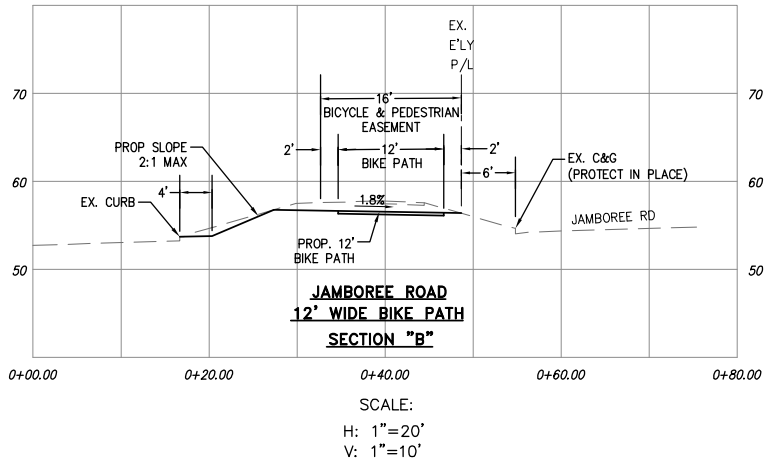
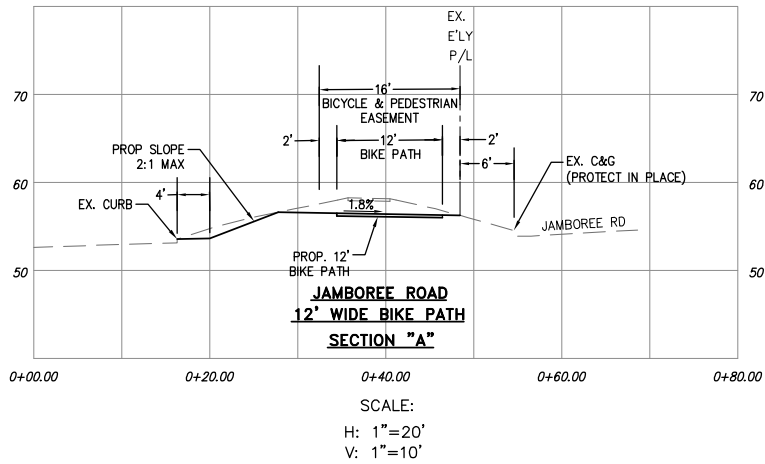
NO.	REVISION	APP.	DATE

**STARBUCKS PROJECT**  
**4501 JAMBOREE ROAD**  
**NEWPORT BEACH, CA**

**DRAWING NAME:**  
**CONCEPTUAL GRADING**

ISSUE:	SBUX CHECK
DATE:	02/20/2024
CHECKED: WC	DRAWN: HA
DRAWING FILE:	22-318 PG
PROJECT NO.:	22-318
SHEET NUMBER:	1
OF 2 SHEETS	
SCALE:	AS SHOWN

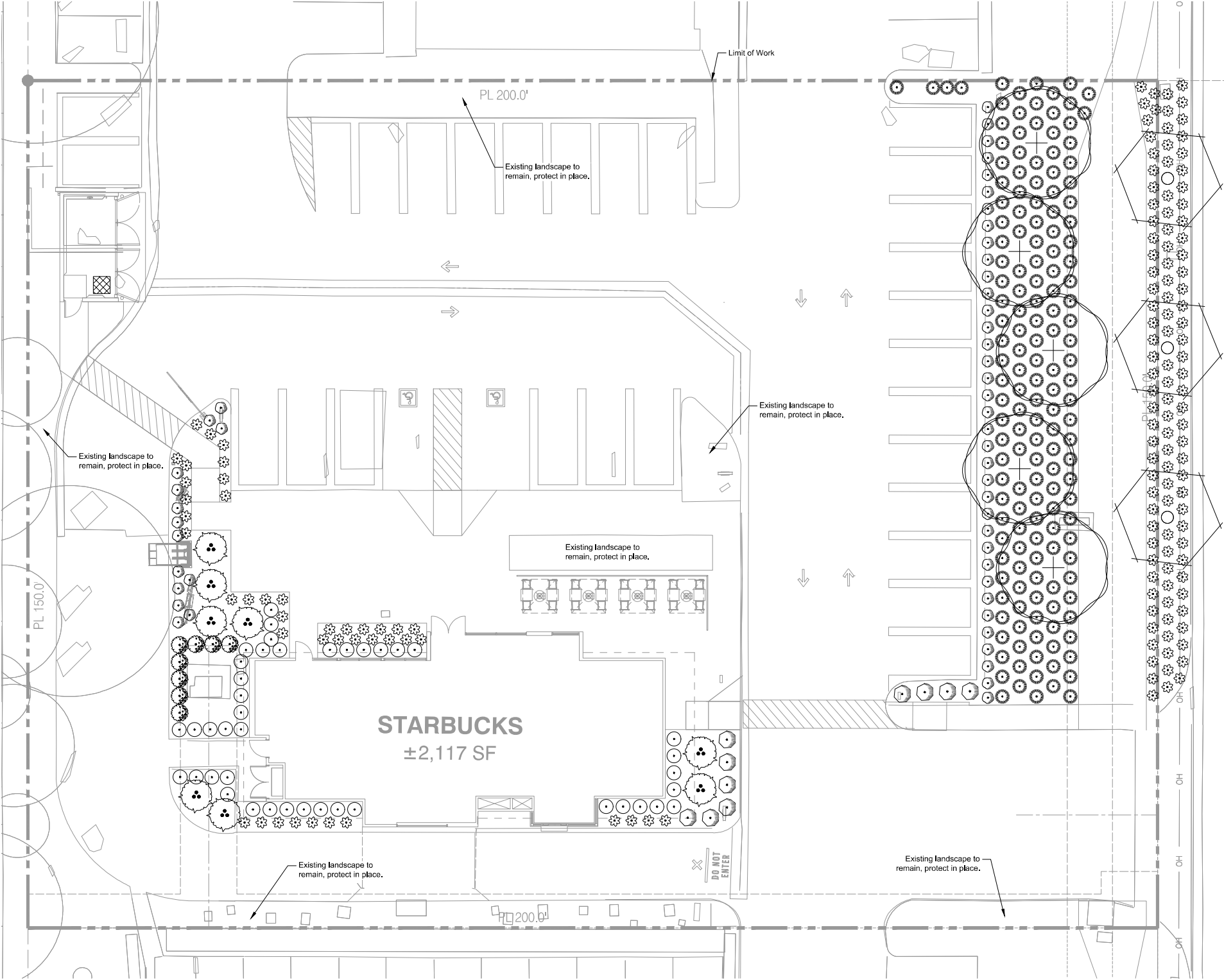
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NO.	REVISION	APP.	DATE

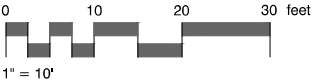
PROJECT:	<b>STARBUCKS PROJECT</b>
	<b>4501 JAMBOREE ROAD</b>
	<b>NEWPORT BEACH, CA</b>
DRAWING NAME:	<b>SECTIONS &amp; PRECISE GRADING DETAILS</b>

ISSUE:	SBUX CHECK
DATE:	02/20/2024
CHECKED: WC	DRAWN: HA
DRAWING FILE:	22-318 PG
PROJECT NO.:	22-018
SHEET NUMBER:	2
OF 2 SHEETS	
SCALE:	AS SHOWN

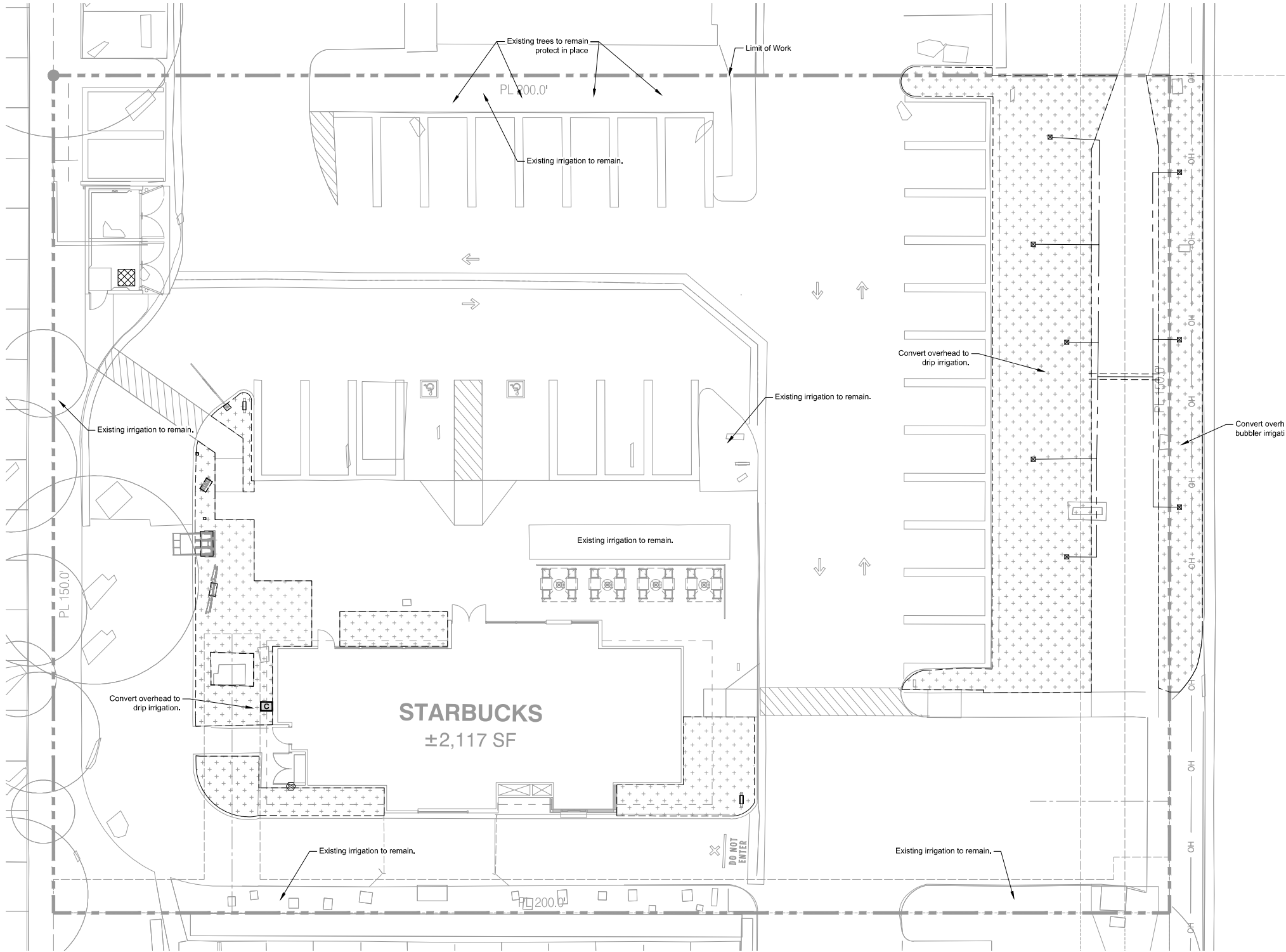


Plant Legend

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	TYPE	USE	WUCOLS	STYLE	CA NATIVE	QTY
TREES									
	Acer buergerianum	Trident Maple	24"box	Deciduous	Accent Tree	Medium	Standard	No	5
	Lagerstroemia indica x fauriei 'Natchez'	Natchez Crape Myrtle	24"box	Deciduous	Street Tree	Medium	Standard	No	3
SHRUBS									
	Cistus x pulverulentus 'Sunset'	Sunset Rockrose	5 gal	Evergreen	Low	No			8
	Dianella revoluta 'DR5000'	Little Rev™ Flax Lily	1 gal	Evergreen	Low	No			150
	Muhlenbergia rigens	Deer Grass	1 gal	Evergreen	Low	Yes			159
	Myrtus communis 'Compacta'	Dwarf Common Myrtle	5 gal	Evergreen	Low	No			42
	Nepeta x 'Walker's Low'	Walker's Low Catmint	5 gal	Evergreen	Low	No			10
	Teucrium cossonii majoricum	Germander	5 gal	Evergreen	Low	No			50
	Westringia fruticosa 'WES03'	Blue Gem Coast Rosemary	5 gal	Evergreen	Low	No			8







Irrigation Legend

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	PSI
	Hunter PCB Flood Bubbler, 1/2in. FIPT.	8	15
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	
	Area to Receive Drip Emitters Salco Irrigation SLV-PSTM-CV Threaded pressure compensating drip emitter with 1/2in. FIPT inlet and Diffuser Cap Outlet in Mulch Camo Color, TruCheck Valve Technology. Brown = 0.5 GPH; Black = 1.0 GPH; Green = 2.0 GPH; Red = 4.0 GPH. Emitter Notes: 2.0 GPH emitters (1 assigned to each 1 gal plant) 2.0 GPH emitters (1 assigned to each 5 gal plant)	4,219 s.f. 309 118	
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	
	Hunter A2C-1200-M 12-Station controller in an outdoor gray steel wall mount enclosure,	1	
	Hunter WSS Wireless Solar, rain freeze sensor with outdoor interface, connects to Hunter PCC, Pro-C, and I-Core Controllers, install as noted. Includes 10 year lithium battery and rubber module cover, and gutter mount bracket.	1	
	Irrigation Lateral Line: PVC Class 200 SDR 21 (Trees) See irrigation detail.	201.7 l.f.	
	Pipe Sleeve: PVC Schedule 40 Standard pipe sleeve for irrigation pipe, Pipe sleeve shall be two times the diameter of pipe being sleeved. Irrigation mainline, lateral lines, and wire shall all be in separate sleeves.	12.6 l.f.	
	Valve Callout Valve Number Valve Flow Valve Size		

Preliminary MWELC Calculations

Newport Beach Eto: 43.4  
Total Landscape Area: 6,637 SF

Plant Factor: 0.03 Low water use plants  
4,279 square feet  
Irrigation Efficiency: 0.81 Drip Irrigation

Plant Factor: 0.03 Low water use plants  
1,843 square feet  
Irrigation Efficiency: 0.75 Overhead

Plant Factor: 0.09 High water use plants  
515 square feet  
Irrigation Efficiency: 0.75 Overhead

Estimated Annual Water use: **75,415 gallons**

Maximum allowed water Allowance: 80,365 gallons

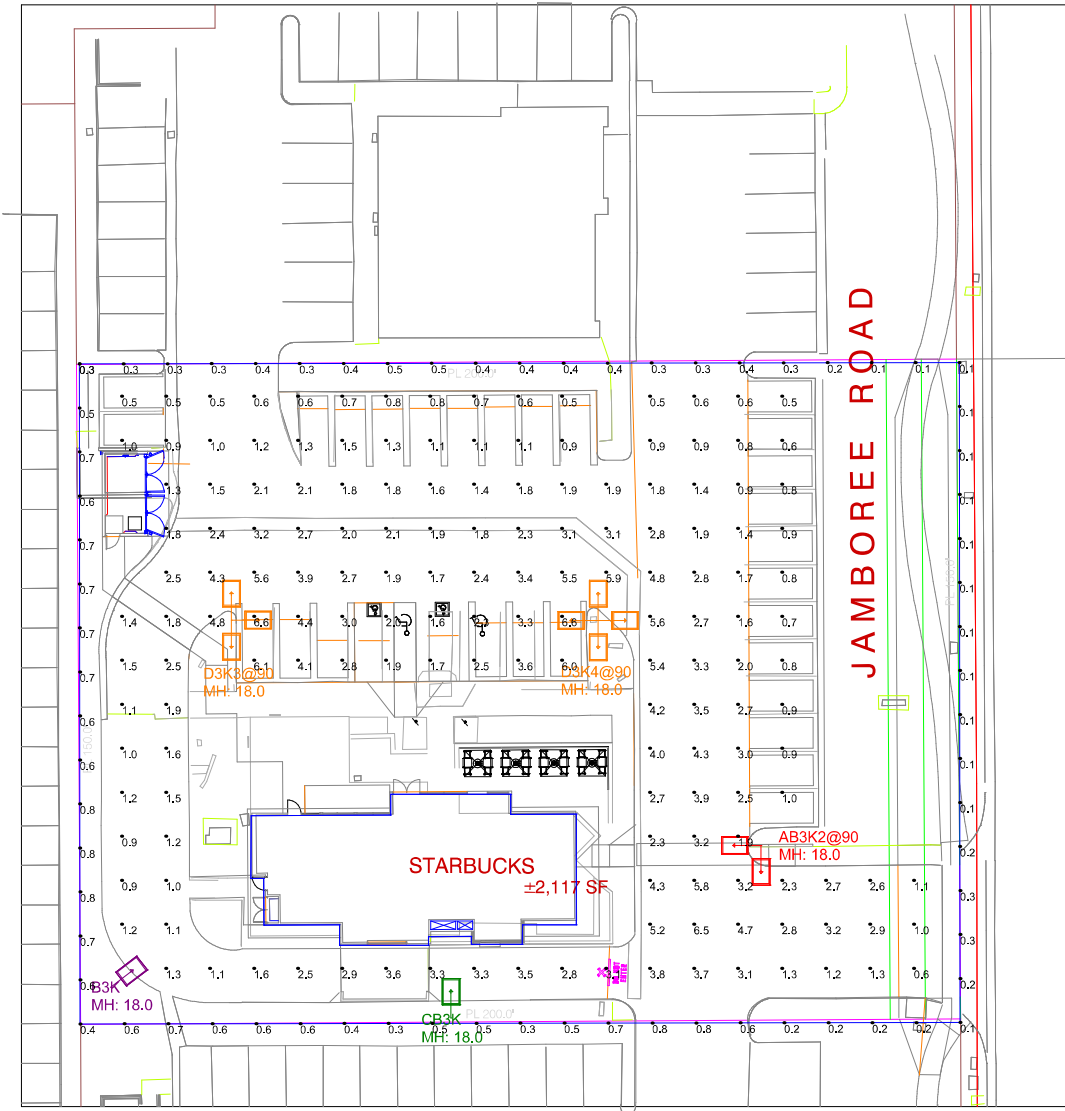
- Notes:
- 1- All landscape plans and installations shall comply with the City of Newport Beach design guidelines, standards, codes and regulations.
  - 2- All landscape areas shall receive permanent irrigation.
  - 3- All landscape installations shall be permanently maintained.
  - 4- All landscape plans shall comply with the Model Water Efficient Landscape Ordinance (MWELC) or the local jurisdictions water ordinance, whichever is more stringent.
  - 5- All plants shall be of quality as prescribed in the details and specifications of the landscape construction plans.
  - 6- All utilities, perimeter walls and trash enclosures shall be screened with hedges, vines, or other approved treatments.



NOTES:

1. THE FOOTCANDLE LEVELS AS SHOWN ARE BASED ON THE FOLLOWING CRITERIA. ANY SUBSTITUTIONS IN SPECIFIED FIXTURES OR CHANGES TO LAYOUT WILL AFFECT LIGHTING LEVELS SHOWN AND WILL NOT BE THE RESPONSIBILITY OF SECURITY LIGHTING.

2. DISTANCE BETWEEN READINGS 10'

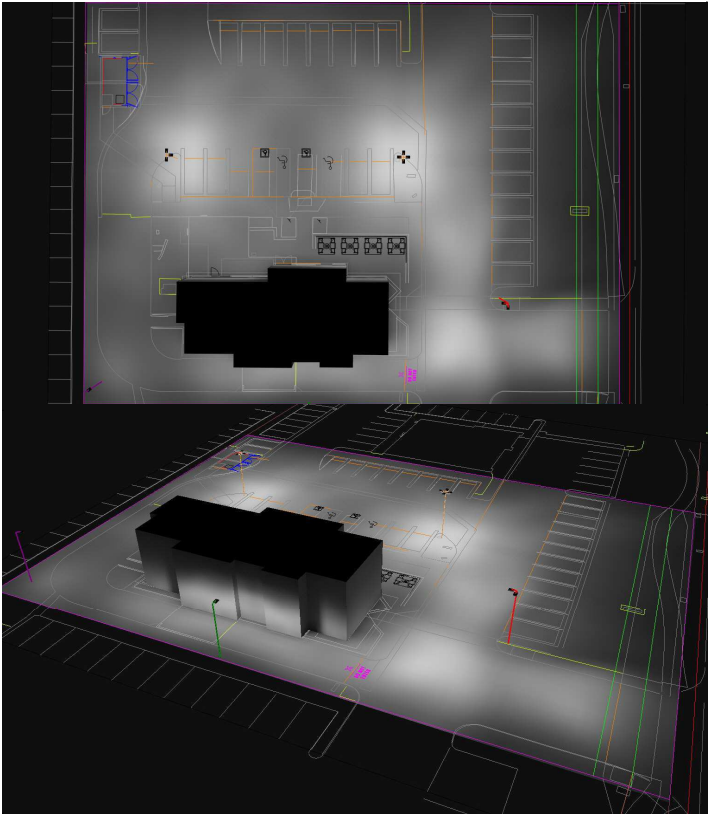


Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
PAVED SURFACE READINGS	Illuminance	Fc	2.29	6.6	0.5	4.58	13.20
PROPERTY LINE READINGS	Illuminance	Fc	0.40	0.8	0.1	4.00	8.00

Luminaire Schedule										
Symbol	Qty	Label	Arrangement	LLF	Description	Lum. Watts	EPA	Mtg Height	Pole Type	BUG Rating
	1	AB3K2@90	2 @ 90 degrees	0.900	RAR-1-160L-70-3K7-4W-BC-SCP-40F	68.4	1.214	18	SES-15-40-1-TA-GL-xx (4")	B0-U0-G1
	1	B3K	Single	0.900	RAR1-80L-25-3K-4F-SCP-40F	25	.607	18	SES-15-40-1-TA-GL-xx (4")	B1-U0-G1
	1	CB3K	Single	0.900	RAR-1-160L-70-3K7-3-BC-SCP-40F	68.4	.607	18	SES-15-40-1-TA-GL-xx (4")	B0-U0-G2
	1	D3K3@90	3 @ 90 Degrees	0.900	RAR1-80L-50-3K-4F-SCP-40F	39	1.486	18	SES-15-40-1-TA-GL-xx (4")	B1-U0-G2
	1	D3K4@90	4 @ 90 Degrees	0.900	RAR1-80L-50-3K-4F-SCP-40F	39	1.486	18	SES-15-40-1-TA-GL-xx (4")	B1-U0-G2

LOCAL REQUIREMENTS:  
1fc max on PL  
Minimize height of poles  
Title 24  
3000K for Starbucks

Pole Fixtures Are Full Cutoff  
Tilt=0  
Calculation Grids Are At Grade  
Pole Light Mounting Height=18ft  
(15' Pole + 3' Base)



PROJECT WIND LOAD CRITERIA BASED ON:  
ASCE 7-10 WIND SPEEDS (3-SEC PEAK GUST MPH)  
50 YEAR MEAN RECURRENCE INTERVAL  
ALLOWED EPA 21.1 @ WIND LOAD 85 MPH

2105 Gulf Road, Suite 400, Rolling Meadows, IL 60008  
1-800-544-8888

UNLESS OTHERWISE SPECIFIED, ALL DIMENSIONS ARE IN INCHES

SCALE 1"=20' (0")

DRAWN BY CLB

POINT-BY-POINT FOOTCANDLE PLOT FOR  
STARBUCKS  
4501 JAMBOREE RD  
NEWPORT BEACH, CA

DATE 2/13/2024	DRAWING NUMBER EX50422 B.AGI
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1. THIS LIGHTING DESIGN IS BASED ON INFORMATION SUPPLIED BY OTHERS TO SECURITY LIGHTING SYSTEMS. SITE DETAILS PROVIDED HEREON ARE REPRODUCED ONLY AS A VISUALIZATION AD. FIELD DEVIATIONS MAY SIGNIFICANTLY AFFECT PREDICTED PERFORMANCE. PRIOR TO INSTALLATION, CRITICAL SITE INFORMATION (POLE LOCATIONS, ORIENTATION, MOUNTING HEIGHT, ETC.) SHOULD BE COORDINATED WITH THE CONTRACTOR AND/OR SPECIFIER RESPONSIBLE FOR THE PROJECT.

2. LUMINAIRE DATA IS TESTED TO INDUSTRY STANDARDS UNDER LABORATORY CONDITIONS. OPERATING VOLTAGE AND NOMINAL MANUFACTURING TOLERANCES OF LAMP, BALLAST, AND LUMINAIRE MAY AFFECT FIELD RESULTS.

3. CONFORMANCE TO FACILITY CODE AND OTHER LOCAL REQUIREMENTS IS THE RESPONSIBILITY OF THE OWNER AND/OR THE OWNER'S REPRESENTATIVE.

4. THIS LAYOUT MAY NOT MEET TITLE 24 OR LOCAL ENERGY REQUIREMENTS. IF THIS LAYOUT NEEDS TO BE COMPLIANT WITH TITLE 24 OR OTHER ENERGY REQUIREMENTS, PLEASE CONSULT FACTORY WITH SPECIFIC DETAILS REGARDING PROJECT REQUIREMENTS SO THAT REVISIONS MAY BE MADE TO THE DRAWING.

THIS DRAWING MEETS OR EXCEEDS MCNEMARS CURRENT ILLUMINATION SPECIFICATIONS OF A 3-4 FOOTCANDLE AVERAGE, UNLESS SUPERSEDED BY OTHER REQUIREMENTS.





4695 MacArthur Court  
Suite 1400N  
Newport Beach, CA 92660  
T:949 296 0450



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**STARBUCKS**  
4501 JAMBOREE RD  
NEWPORT BEACH, CA

20220083.0

**PHOTO SIMULATIONS**  
**SHEET 17**

PA2024-0035





ANODIZED DARK BRONZE



CLEAR INSULATED GLASS WINDOWS



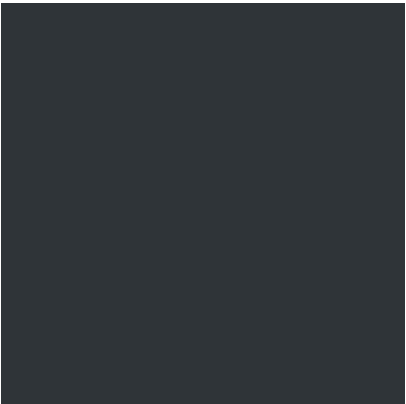
CONCRETE LOOK STUCCO WITH  
INTEGRAL COLOR TO MATCH  
SW 6260 UNIQUE GRAY



SW 6260 UNIQUE GRAY



STUCCO FINISH – PAINTED TO MATCH  
SW 7076 CYBERSPACE



METAL CANOPY/RAILING PAINTED TO  
MATCH SW 6992 INKWELL



WOOD APPEARANCE SIDING – NICHIBA  
CEDAR FINISH



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T:949 296 0450

**STARBUCKS**  
BIRCH & JAMBOREE  
NEWPORT BEACH, CA  
20220083.0

**COLOR & MATERIAL BOARD**

12.12.2022